

1.0 INTRODUCTION

1.1 PURPOSE AND INTENT OF THE SPECIFIC PLAN

The Lytle Creek Ranch Specific Plan, hereafter referred to as “Lytle Creek Ranch Specific Plan” or “Specific Plan,” provides a detailed description of the proposed land uses and infrastructure requirements for the Lytle Creek Ranch project, which will be processed through the City of Rialto, California. The design and development standards contained in this document will assist in creating architectural themes and landscape character for development within Lytle Creek Ranch. The Specific Plan is expected to be adopted by Resolution with the exception of Chapter 5.0, Development Standards, which will be adopted by Ordinance and serve as the zoning for the Lytle Creek Ranch Specific Plan area.

This Specific Plan is intended to serve the following purposes:

- Promote quality development consistent with the goals and policies of the City of Rialto General Plan.
- Provide for comprehensive planning that assures the orderly development of the project site in relation to surrounding existing development.
- Assure appropriate phasing and financing for community facilities, including circulation and streetscape improvements, domestic water, urban runoff and flood control facilities, sewage disposal facilities, educational facilities, and parks.
- Establish development regulations permitting a wide variety of detached and attached residential products.
- Develop a plan that is economically feasible and capable of being implemented based on existing and anticipated future economic conditions such that no economic burden to the City occurs.
- Provide for the creation of a compact, walkable community that concentrates development, accommodates residential and commercial/retail development, and establishes a strong “sense of place.”

1.2 AUTHORITY AND FORMAT OF THE SPECIFIC PLAN

The State of California Legislature has established the authority and scope to prepare and implement specific plans. The State requires that all cities and counties in California prepare and adopt a comprehensive General Plan for the physical development of their areas of jurisdiction. To implement the policies described in the General Plan, regulating programs need to be adopted (i.e.,

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zoning ordinances, subdivision ordinances, building and housing codes, etc.). California State law authorizes cities with complete General Plans to prepare and adopt specific plans (Government Code Section 65450 – 65457). Local planning agencies or their legislative bodies may designate areas within their jurisdiction as areas for which a specific plan is “necessary or convenient” (Government Code Section 65451).

Specific plans are intended to serve as bridges between the local General Plan and individual development proposals. Specific plans contain both planning policies and regulations, and may combine zoning regulations, capital improvement programs, detailed development standards, and other regulatory requirements into one document, which are designed to meet the needs of a specific area.

The Lytle Creek Ranch Specific Plan has been created through the authority granted to the City of Rialto by the California Government Code, Sections 65450 through 65453. This Specific Plan has been prepared in accordance with the provisions of the California Government Code, which stipulate that a specific plan contain text and diagrams that specify the following:

Land Use

The specific plan must specify the distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.

Public Facilities

The specific plan must show the proposed distribution, location, extent, and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities located within the area covered by the plan, and needed to support the land uses described in the plan.

Development Standards

The specific plan must include standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.

Implementation Measures

The specific plan must include a program of implementation measures, including regulation, programs, public works projects, and financing measures.

General Plan Consistency

The specific plan must include a statement of the relationship of the specific plan to the General Plan.

Optional Contents

The specific plan may address any other subject that, in the judgment of the planning agency, is necessary or desirable for implementation of the General Plan.

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All future development plans, tentative parcel and/or tract map(s), and/or other similar entitlements for the Lytle Creek Ranch Specific Plan area shall be consistent with the regulations set forth in this Specific Plan and with all other applicable City of Rialto regulations. Furthermore, all regulations, conditions, and programs contained herein shall be deemed separate, distinct, and independent provisions of the Lytle Creek Ranch Specific Plan. In the event that any such provision, standard, or clause is held invalid or unconstitutional, the validity of all remaining provisions, standards, and clauses of this Specific Plan shall not be affected.

1.3 PROJECT LOCATION

The Lytle Creek Ranch Specific Plan project site is located partially within the city limits of Rialto and mostly within the City's sphere of influence in unincorporated San Bernardino County. The site is bisected partially by both the Interstate 15 (I-15) Freeway and Lytle Creek Wash, an intermittent stream. The location of the Lytle Creek Ranch Specific Plan in relation to the local and regional setting is displayed in Figure 1-1, Regional Map, and Figure 1-2, Local Vicinity Map.

Regionally, the City of Rialto is located approximately 60 miles east of downtown Los Angeles and 103 miles north of San Diego, in the western portion of the San Bernardino Valley, in the center of the Inland Empire. The primary regional transportation linkages include the Foothill Freeway (State Route 210), which traverses through the central portion of the City in an east-west direction, and the Ontario Freeway (Interstate 15), which borders the City to the north, providing regional access to the project area. Secondary regional transportation linkages include the Interstate 215 Freeway and U.S. Highway 66 to the northeast and, further south, Interstate. From the I-15, direct access to the project site is provided by Sierra and Riverside Avenues, which run along the southwestern boundary of the site. Access to the site from State Route 210 is available via an interchange at Riverside Avenue.

1.4 PROJECT OBJECTIVES

The Lytle Creek Specific Plan is designed to implement a series of project-related objectives that have been carefully crafted to ensure that the project develops as a high-quality master planned community that meets realistic and achievable objectives. These objectives, which are identified below, have been refined throughout the planning and design process for Lytle Creek Ranch:

- Build upon the platform of high-quality design, architecture, and landscaping established by neighboring residential communities to provide a northern gateway to the City of Rialto that offers new and exciting amenities to residents.
- Establish open space preservation areas that will provide functioning habitats for sensitive, threatened, and endangered species, preserve Lytle Creek Wash and minimize impacts to its riparian and alluvial fan sage scrub habitats, while providing other wildlife benefits and accommodating growth and development opportunities within the City.

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- Locate and integrate the design of native habitat open space areas into the community by providing and promoting connectivity with significant blocks of wildlife habitat off-site and habitat linkages and wildlife movement corridors in the region.
- Maximize opportunities for using native plant material/species in the project landscaping, especially in areas where such landscaping is located in proximity to areas of preserved native habitat.
- Develop freeway-oriented commercial areas to serve regional needs and stimulate job and revenue growth in the City.
- Concentrate development within neighborhoods to promote greater efficiency of land use and promote walking and bicycling.
- Respond to the unmet need for ~~Active_~~ Adult communities and conventional single-family homes in the Rialto area by providing residents with a ~~golf course-oriented community and a~~ variety of conveniently located on-site amenities.
- Provide the City and surrounding community with ~~a redesigned public golf course and clubhouse,~~ recreation and open space areas, parks, and trails to meet the City's General Plan goals to provide such facilities to maintain and enhance the City's quality of life.
- Address the City of Rialto's current and projected housing needs for all segments of the community by providing a range of family-oriented single- and multi-family residences, as well as an Active Adult ~~golf course-~~community.
- Establish a mix of land uses and local-serving activities that meet the General Plan's objectives concerning community character and pedestrian-friendly design.
- Implement the City's General Plan Land Use Element goal to facilitate annexation of large areas of land that are governed by a specific plan, which provides for compatibility of land uses, fiscal balance, recreation, and resource protection.
- Create a transportation network that will fulfill the policies of the Rialto General Plan's Circulation Element by allowing residents to live within proximity to schools, recreational opportunities, retail centers, and commercial development, and by minimizing vehicle trips through utilizing access to a variety of transportation opportunities, including pedestrian pathways, bikeways, regional freeways, transit, and trains/Metrolink.
- Provide a network of pleasant, safe, and convenient pedestrian trails and bike lanes.

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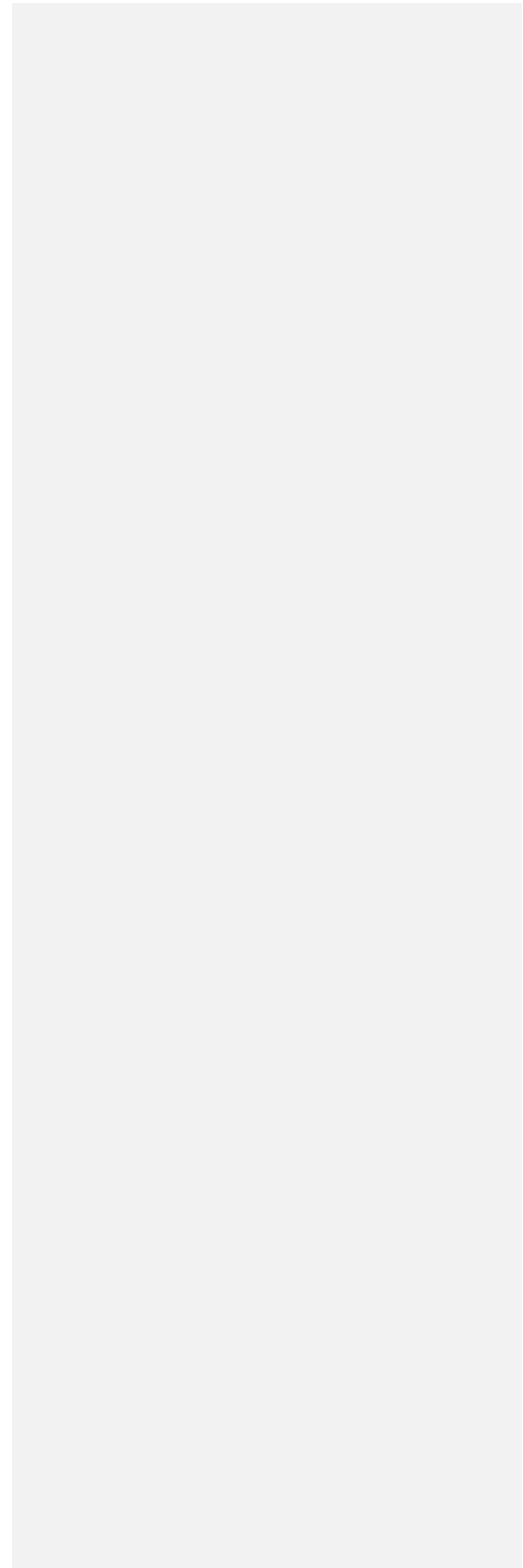
- Address regional infrastructure concerns by locating development in areas where opportunities for groundwater recharge are maintained and the life of groundwater aquifers are protected.
- Incorporate “Green” and sustainable practices, as practicable, in developing buildings and infrastructure in Lytle Creek Ranch.
- Identify and address safety hazards, such as wildfire and flooding dangers, through implementation of design safety features and levee improvements.
- Undertake development of the project site in a manner that is economically feasible and balanced to address both the Applicant’s and the City’s economic concerns.

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Figure 1-1
Regional Location

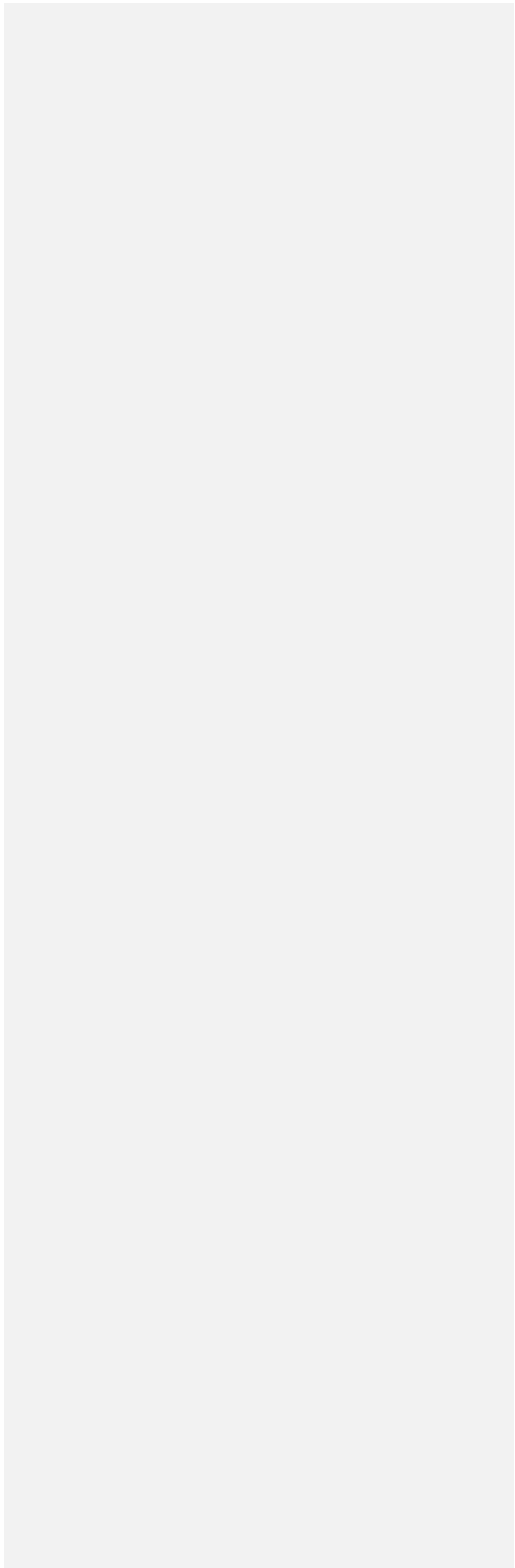


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Figure 1-2
Local Vicinity Map



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1.5 PROJECT OVERVIEW

Lytle Creek Ranch is the result of years of intensive planning and careful design to create one of the foremost master-planned communities in the Inland Empire and, indeed, in all of Southern California. The project site has been owned and protected by one family for several decades. Now that Rialto is nearing build-out, this family has decided the timing is right to develop portions of the last large remaining vacant land in the City with a beautiful, new master-planned community on approximately 2,447,030 acres. Portions of the site are located within the city limits of Rialto, while remaining areas of the site are located within the City's sphere of influence in unincorporated San Bernardino County.

The Lytle Creek Ranch community is designed as ~~four~~three separate and unique neighborhoods:

- ~~▪ **Neighborhood I** – includes approximately 417 acres of land. A portion of this land (“Sycamore Flats East” and “Sycamore Flats West”) is located within the boundaries of the 3,400-acre Glen Helen Specific Plan. The remaining land in Neighborhood I includes acreage located within the boundaries of the Lytle Creek North Planned Development. The Lytle Creek North Planned Development encompassed parts of Sycamore Flats East and Sycamore Flats West, including the community of Rosena Ranch. Once approved, the “Lytle Creek Ranch Specific Plan” will supersede portions of the “Glen Helen Specific Plan” (County of San Bernardino) and the “Lytle Creek North Preliminary Development Plan” (County of San Bernardino). Areas to be removed from these adopted plans include Planning Areas 1 through 15 of the Lytle Creek Ranch Specific Plan.~~
- **Neighborhood II** – is planned as a ~~gated conventional single-family neighborhood or~~ Active Adult ~~golf course~~ community on approximately 802 acres and includes the entire 221-acre El Rancho Verde Specific Plan area. Once approved, the “Lytle Creek Ranch Specific Plan” will supersede the City-approved “El Rancho Verde Specific Plan.” Areas to be removed from the adopted El Rancho Verde Specific Plan include a portion of Planning Area 95, and all of Planning Areas 96 through 103 of the Lytle Creek Ranch Specific Plan.
- **Neighborhood III** – is located south of the I-15 and is planned to appeal to young families and families with children and will include a mix of single-family detached and attached homes, as well as Village Center Commercial development on approximately 969 acres.
- **Neighborhood IV** – includes multi-family residential and Village Center Commercial development on approximately 259 acres located north of the I-15.

Each of the neighborhoods will have a separate and unique identity based on its physical features and public amenities. Three of the neighborhoods will be built-out with housing targeted at a variety of family sizes, couples, and singles, while the fourth neighborhood will be built as a gated, age-qualified community for residents age 55 and older. In all, a maximum of 8,407,039 dwelling units

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may be constructed in Lytle Creek Ranch. The community will build-out at an overall gross density of approximately 3.5 dwelling units per acre.

Approximately 95.6 acres of Village Commercial Center uses are planned on-site. These areas will develop with retail, commercial, office, business park, and medical/dental uses. One of the Village Center Commercial areas, located at the juncture of Sierra Avenue and Riverside Avenue, is expected to build-out as a major retail shopping center.

Lytle Creek Ranch will include a wide variety of housing types in community settings that reflect the aesthetic charm and neighborhood structure reminiscent of traditional Southern California towns. The community is designed as a mix of family-oriented ~~and-potential~~ Active Adult homes clustered into ~~four~~three distinct neighborhoods. Each neighborhood will have its own unique identity and character. This will be accomplished by promoting authentic architecture and creating iconic streets with consistent design elements and a unified landscape palette to create a readily identifiable streetscape.

Like most areas, the baby boomer segment of the San Bernardino County population is quickly approaching retirement age. The southern portion of Lytle Creek Ranch (Neighborhood II) is planned ~~for conventional single-family homes with the potential for as-~~ a lifestyle community targeted at households within the expanding active adult (age 55 and older) population. Active adult communities such as Lytle Creek Ranch ~~could~~ offer residents of similar ages and interests a place to come together to enjoy an active lifestyle and sense of community. Lytle Creek Ranch will focus on the health, wellness, and fitness of its residents. The project will include an extensive network of sidewalks, which will link together the ~~Active Adult~~ neighborhood. ~~In addition, there will be a public 18-hole public golf course.~~ The age-qualified community ~~is-could be~~ designed to accommodate housing without burdening parks and local schools.

~~An Active Adult~~ recreation center is planned in Neighborhood II especially for those residents. The recreation center will be beautifully landscaped and designed to serve as a community focal and gathering point. It is anticipated that the Neighborhood II recreation center will include a community center building that may contain such amenities as meeting and game/craft rooms, exercise facilities, locker rooms, restrooms, and other facilities. There will also be a swimming pool with a spa, and an outdoor area with barbecues for picnics and special events.

Of the ~~2,4472.030~~ acres comprising the project site, half of the property will be preserved as open space by clustering development along Riverside Avenue, Lytle Creek Road, Glen Helen Parkway, Clearwater Parkway, and the I-15 corridor. Lytle Creek Wash bisects a portion of the project site. A minimum of ~~829.2769~~ acres will be preserved as undisturbed open space in its natural condition for habitat and wildlife potential, including the areas located along and within Lytle Creek Wash ~~and portions of the hillsides adjacent to Glen Helen Regional Park and the San Bernardino National Forest.~~

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Another ~~296~~ 299.5 acres will be devoted to open space, neighborhood parks, ~~golf~~, and recreation areas. The project proposes an extensive system of green spaces, such as neighborhood parks, paseos, and recreation areas linked together by a network of trails and paseos.

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A comprehensive trail system is planned throughout Lytle Creek Ranch. This system includes multi-purpose trails that run adjacent to Lytle Creek Wash in Neighborhoods II, III, and IV. Other trails include a pedestrian walkway along the length of Riverside Avenue in Neighborhoods II, III, and IV, and a variable width "Grand Paseo" that runs the length of Neighborhood III. A multi-purpose trail in the Grand Paseo will be a minimum of eight feet in width and will accommodate both bicycle and pedestrian traffic. ~~In addition, a trail system will be provided in Neighborhood I that will link up to the pedestrian trail system planned in the adjacent Rosena Ranch community (formerly known as "Lytle Creek North").~~

Lytle Creek Ranch also includes three public neighborhood parks that will include a mix of passive uses including, but not limited to, picnicking areas, shade structure(s), playgrounds, gardens, seating areas, informal turf play areas, and attractive landscaping. Each of the neighborhood parks in Neighborhood III will contain private recreation facilities designed ~~especiall~~ specially to serve the recreational needs of Lytle Creek Ranch residents of Neighborhood III. In addition, there will be two joint-use parks located adjacent to the two schools, which will include playgrounds and/or sports fields.

~~In addition to the above recreational amenities, the project will include a re-designed and reconfigured 18-hole public golf course. The golf course will include a new 19,000-square-foot minimum clubhouse facility with pro shop, locker rooms, offices, bar, restaurant, and banquet facilities. Other features include a tournament lawn, driving range, and carts storage barn. Although the golf course will be surrounded by active adult housing, the course and clubhouse will be open for use by the general public.~~

A key feature of Lytle Creek Ranch is the establishment of a new northern gateway into the City of Rialto. At present, there is no clearly defined edge to the northern portion of the City. Lytle Creek Ranch is designed as the gateway into the City from the north. A dramatic entry featuring an interpretation of the City's symbol, the Rialto Bridge, will be constructed on Riverside Avenue, near the I-15. This gateway will become a community landmark and will announce to both residents and visitors that they are entering Rialto. ~~The project will also include two "Welcome to Rialto" monument signs, one each in Neighborhoods I and IV.~~

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1.6 DISCRETIONARY ACTIONS AND APPROVALS

The City of Rialto is the Lead Agency for purposes of California Environmental Quality Act (CEQA) compliance and has prepared an Environmental Impact Report (EIR) to consider the following discretionary actions, for which applications have been submitted to the City. These actions are required to implement this Specific Plan:

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- **Approval of the General Plan Amendment:** A General Plan Amendment will be necessary to change the entire property from the current General Plan land use designations of “Special Study Areas,” “Edison Easement,” “Residential – Low Density (0-3),” and “Residential – Medium Density (3-6)/Recreation-Golf Course” to “Specific Plan Area” on the City’s General Plan Land Use Map.
- **Approval of the Specific Plan:** The Lytle Creek Ranch Specific Plan has been prepared to realize the objectives of the proposed project as defined here in this Specific Plan. The Specific Plan will be adopted by resolution by the City of Rialto City Council, with the Development Standards chapter adopted by ordinance. The existing “El Rancho Verde Specific Plan,” a portion of the existing “Glen Helen Specific Plan,” and a portion of the “Lytle Creek North Planned Development” will be superseded by the Lytle Creek Ranch Specific Plan, once the Lytle Creek Ranch Specific Plan is approved.
- **Approval of Prezoning:** Once the Specific Plan is approved by the City, the Specific Plan will serve as the “pre-zoning” for the project site. The Specific Plan will pre-zone the property from the mix of current Rialto and San Bernardino County zoning designations to “Specific Plan Zone.”
- **Approval of Tentative Tract Maps (TTM):** Concurrently with the General Plan Amendment, Specific Plan, and other entitlement requests, the master developer intends to process Tentative Tract Maps for portions of the Specific Plan area. The Tentative Tract Maps will be prepared and processed through the City in accordance with Section 17.16 of the City of Rialto Municipal Code and in accordance with the Subdivision Map Act of the California Government Code.
- **Approval of Grading Plans:** In conjunction with the Tentative Tract Maps, the City will process the corresponding grading plans based on the grading permit process established by the City’s Building Codes. Grading permits will be required prior to commencement of on-site grading activities.
- **Certification of the Environmental Impact Report (EIR):** The City of Rialto has determined that an EIR is required to analyze the potential environmental impacts of the project and include mitigation measures, as appropriate, to reduce potential environmental impacts. The EIR has been prepared in accordance with CEQA and the CEQA Guidelines. The City of Rialto will consider certification of the EIR prior to taking action on the requested approvals.
- **Adoption of Mitigation Monitoring Program:** The City will evaluate and adopt a Mitigation Monitoring Program (MMP), which will be considered by the City related to the changes made to the project or conditions of project approval that were adopted in order to mitigate or avoid significant effects on the environment.

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- **Approval of a Development Agreement/Pre-Annexation Development Agreement:** A Development Agreement/Pre-Annexation Development Agreement will be negotiated between the City of Rialto and the Project Applicant that will establish vesting of development rights and entitlements, identify project improvements, timing of improvements, as well as the responsibilities and rights of both the City and the project Applicant applying to development of the Lytle Creek Ranch project.

- **Annexation Determination:** All of the above land use entitlements will be acted on by the City prior to annexation of the unincorporated areas into the City. The above entitlements, including the Pre-Annexation Development Agreement, are premised upon “pre-annexation” approvals that will become “in effect” upon completion of the annexation process. Cities are permitted to process pre-annexation General Plan amendments, zone changes, and specific plans prior to Local Agency Formation Commission (LAFCO) action on the proposed annexation; however, these land use entitlements are not considered in effect for the portions of the property located outside the city limits until the property is actually incorporated into the City. The annexation determination will involve the filing of a petition by the landowner(s) with the San Bernardino County LAFCO to annex the unincorporated portions of Lytle Creek Ranch into the City of Rialto. At the time of approval by the City Council, the land use entitlements for those portions of the project site located within the city limits will become effective immediately or as provided for by state law.

The approximately ~~2,447.32,030.1~~-acre Lytle Creek Ranch project site is located partly within the City of Rialto (approximately 694.2 acres) and partly within an unincorporated portion of southwestern San Bernardino County (approximately ~~4,753.41,335.9~~ acres). The jurisdictional boundaries are depicted in Figure 1-3, Annexation Areas. As part of project entitlements for Lytle Creek Ranch, the following annexations/boundary adjustments will need to occur:

- Annexation of all unincorporated lands (approximately ~~4,753.41,335.9~~ acres) within the project area into the City of Rialto;
- ~~Removal of Neighborhood I from the San Bernardino County GH-70 Service District for Fire and Sewer Service;~~
- ~~Adjustments between the Rialto Unified School District/San Bernardino Unified School District service boundaries in Neighborhood I; and~~
- Annexation of ~~these~~ portions of the project site located within the Sphere of Influence (i.e., portions of Neighborhoods II, III, and IV) into the West Valley Water District.

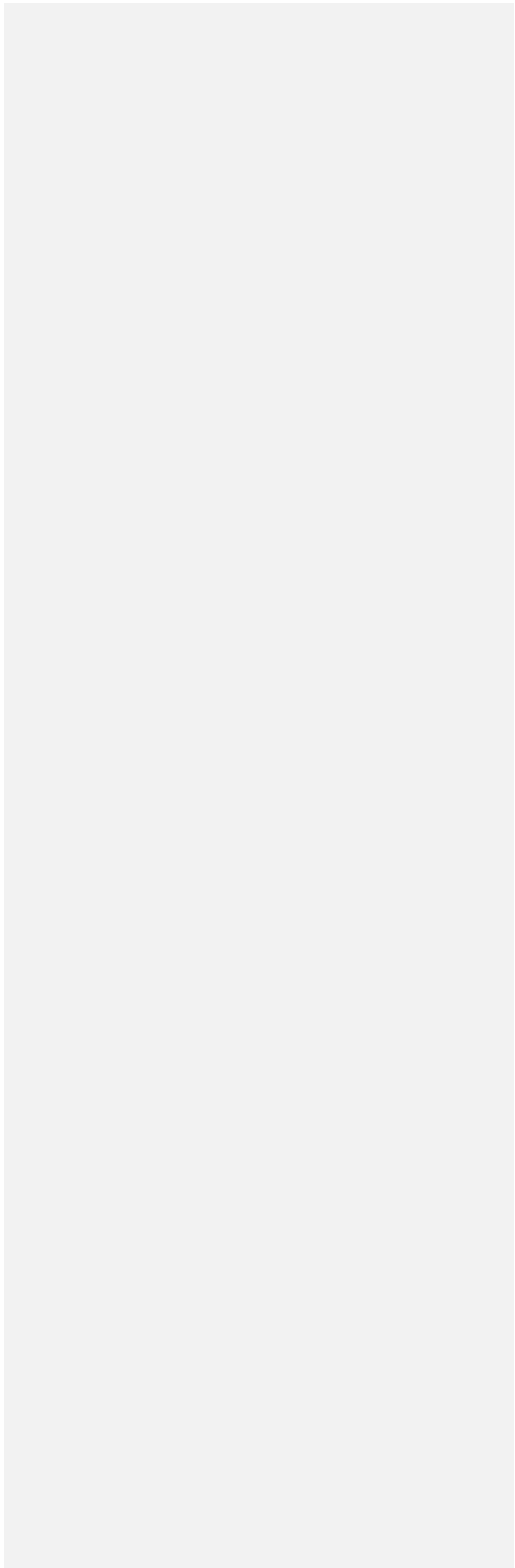
All entitlements will require approval by the Rialto City Council. The annexation request will require approval by LAFCO, as well.

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Figure 1-3
Annexation Areas



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2.0 PLANNING CONTEXT

2.1 RELATIONSHIP TO THE CITY OF RIALTO GENERAL PLAN/ZONING CODE

The Lytle Creek Ranch Specific Plan serves as a planning guide to implement the intent of the City's General Plan. The Specific Plan development context has been prepared to be consistent with all applicable goals and programs contained within the General Plan and to effectively implement the policies of the General Plan, and the applicable Zoning requirements and guidelines. The issues and goals identified within each element of the General Plan have been evaluated, and a statement of compliance with the General Plan has been included as Appendix A (General Plan Consistency Analysis).

2.1.1 GENERAL PLAN LAND USES

The City of Rialto General Plan Land Use Element has the broadest scope of any of the required components and serves as the framework for the goals and policies contained in the other elements. The primary implementation mechanism for this element is the Zoning Ordinance.

The majority of the project site is designated as "Special Project areas" on the City's General Plan Land Use Map. A narrow swath of land is designated as "Edison Easement." The southernmost portion of the project site is designated as "Residential – Medium Density (3-6)/Recreation – Golf Course" on the Land Use Map. There are also a few small scattered parcels designated as "Residential – Low Density (0-3)." Figure 2-1, Existing City of Rialto General Plan Land Use Designations, depicts the existing City of Rialto General Plan Land Use designations for the project site and delineates those areas of Lytle Creek Ranch that are currently in the City from those which are in the County.

The Lytle Creek Ranch Specific Plan proposes to amend the Rialto General Plan to:

- Add to Section 2.0, Land Use Designations a "Specific Plan Area" designation that denotes that the land uses in those areas are to be governed by a Specific Plan prepared in accordance with Government Code Section 65450 *et seq.*
- Amend the City's General Plan Land Use Map to (1) reflect proposed annexation of those areas of Lytle Creek Ranch currently in the County into the City; and (2) change the land use designations for the Lytle Creek Ranch project site from "Special Project areas," "Recreation-Golf Course," and "Residential-Medium Density" to "Specific Plan Area." (See Figure 2-2, Proposed City of Rialto General Plan Land Use Map.)
- Adopt "Specific Plan Area" as the land use designation for the Lytle Creek Ranch area, including changing the land use designation for the El Rancho Verde Specific Plan area to "Specific Plan Area."

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Figure 2-1
Existing City of Rialto General Plan Land Uses

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Figure 2-2
Proposed City of Rialto General Plan Land Uses

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2.1.2 ZONING

Approximately 694.2 acres within the Specific Plan area are located within the Rialto city limits and are designated as follows:

- R-1A Single Family Residential
- CC-R Golf Course Residential
- R-OS Recreation Open Space

Approximately ~~4,753.4~~1,335.9 acres within the Specific Plan area are located within unincorporated San Bernardino County and are designated by the County as:

- GH/SP Glen Helen Specific Plan
- FW Floodway
- RS-20M Single Family Residential
- SD-RES Special Development – Residential

Figure 2-3, Existing City of Rialto Zoning, depicts the existing County and City zoning designations for the Lytle Creek Ranch project site.

The Lytle Creek Ranch Specific Plan will be adopted by the City Council by resolution, except for Chapter 5.0, Development Standards, which will be adopted by ordinance and serve as the zoning for Lytle Creek Ranch after the annexation is complete.

The Lytle Creek Ranch project proposes the following zoning amendments:

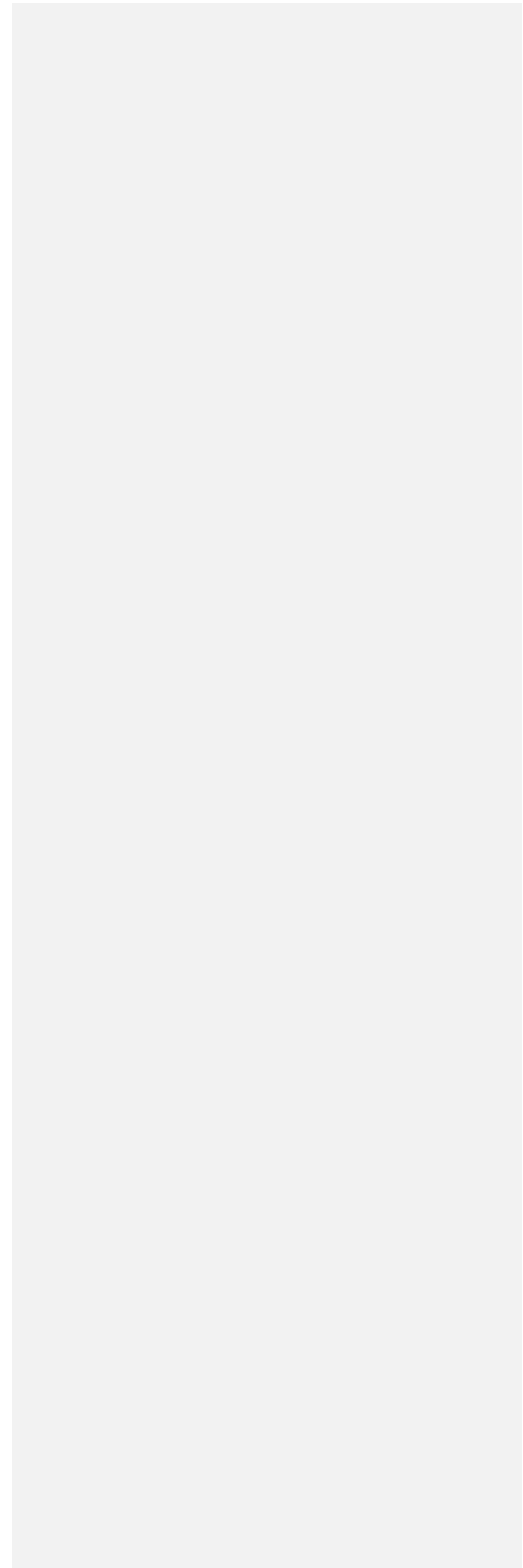
- Amend Chapter 18 of the Municipal Code to add a “Specific Plan Zone” which allows specific plans to serve as the zoning for property.
- Amend the City’s zoning map to (1) reflect proposed annexation of those areas of Lytle Creek Ranch currently in the County into the City; and (2) change the zoning for the Lytle Creek Ranch project site from its current designations to “Specific Plan Zone.” (See Figure 2-4, Zoning Proposed City of Rialto Zoning.)

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Figure 2-3
Existing City of Rialto Zoning

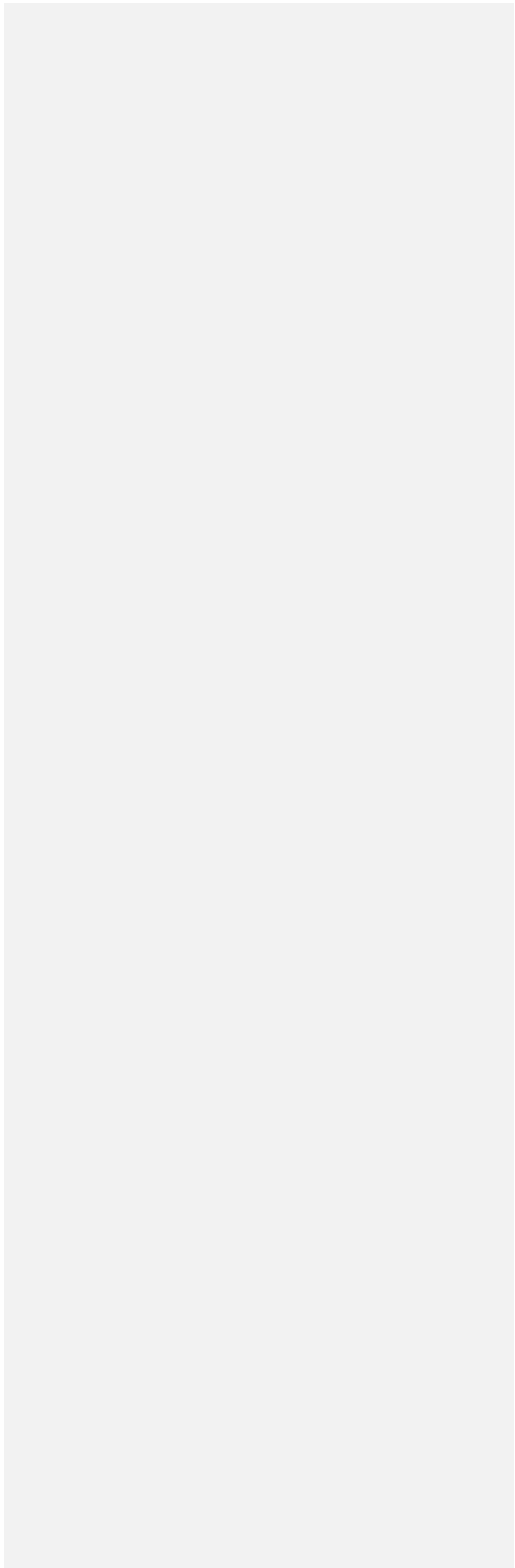


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Figure 2-4
Proposed City of Rialto Zoning



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2.2 EXISTING CONDITIONS

Approximately 694.2 acres of the project site (~~26-34~~ percent) are located within the jurisdictional limits of the City of Rialto. In addition, approximately ~~4,753.41, 335.9~~ acres of the project site (approximately ~~72-66~~ percent) are located within unincorporated San Bernardino County. ~~The project site is situated within the U.S. Geological Survey (USGS) 7.5-minute Devore topographic quadrangle map within Sections 4, 5, 17, 22, and an unsectioned portion, T,1 N., R. 5 W. Elevations within the portion of the Specific Plan area within Lytle Creek Wash range from approximately 2,180 feet above mean sea level (MSL) in the upstream portion of the wash in Neighborhood II. Elevations within Sycamore Flats range from 2,270 feet above MSL to 1,930 feet above MSL in the eastern portion of Neighborhood I.~~

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Lytle Creek is a wide stream that bisects the property. It is a tributary of the Santa Ana River. The river flows through the eastern San Gabriel Mountains and has three forks, the North, Middle, and South forks. The North Fork and Middle Fork meet just west of the unincorporated community of Lytle Creek, California and the South Fork joins them soon after. The last mile of the creek has been diverted into an artificial channel to prevent flooding. The creek merges with Warm Creek before joining with Cajon Creek and the Santa Ana River downstream of the property.

~~Interstate 15 crosses the northern portion of the site. Neighborhood I (including Sycamore Flats) exists northeast of Lytle Creek Wash and on either side of Interstate 15. Portions of Neighborhood I abut Glen Helen Regional Park and the San Bernardino National Forest. In addition to Glen Helen Regional Park, several other County of San Bernardino facilities are located just off site to the east, including Verdemon Ranch, Glen Helen Rehabilitation Facility, a sheriff's training facility, and an off-road vehicle recreation area. The approved Lytle Creek North Planned Development Project (now Rosena Ranch) borders portions of Neighborhood I and Lytle Creek Wash.~~

Neighborhood II abuts existing City and County residential areas and an existing sand and gravel operation. Neighborhood III is located across Riverside Avenue from the existing Las Colinas planned community, which is located within the City of Rialto. A roof tile manufacturing plant borders the south and southwestern portion of the Specific Plan area, and a sand and gravel extraction operation exists in the vicinity of Neighborhoods II and III. Neighborhood IV is located near some existing freeway-related retail uses (adjacent to the I-15 Freeway) and lower density residential uses along Lytle Creek Road.

The Specific Plan area encompasses several prescribed land uses, which are not subject to the activities proposed for the Lytle Creek Ranch Specific Plan, including dedicated open space areas, Edison and Caltrans easements, and land uses pre-approved under the Lytle Creek North Planned Development Project. Previously entitled areas include portions of Neighborhood I (the Glen Helen Specific Plan) and the El Rancho Verde Specific Plan (Neighborhood II).

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The project site is largely vacant with a series of existing roads, ~~including Glen Helen Parkway that providing-provides~~ access to portions of the property. ~~Streets that traverse through the project include Glen Helen Parkway and Clearwater Parkway.~~ Riverside Avenue, Sierra Avenue, and Lytle Creek Road border portions of the project, and Country Club Drive and Oakdale Avenue terminate at the project boundary. Refer to Figure 2-1, Existing Conditions.

2.3 SURROUNDING LAND USES

Much of the area surrounding the Lytle Creek Ranch project site is either developed or limited from development by other constraints. ~~For example, Glen Helen Regional Park, which is home to the Hyundai Pavilion, is located to the northeast of the project. The Pavilion has a capacity of 65,000 and is the nation's largest outdoor amphitheater. The park offers a myriad of recreational opportunities including fishing, swimming, pedal boat rentals, a playground, hiking, volleyball, and the Glen Helen OHV Park.~~

A portion of the San Bernardino National Forest, which contains a total of more than 671,000 acres, abuts the north/northwest portion of the project site. The National Forest offers a diversity of outdoor opportunities — all which are open to the public. Recreational activities include, but are not limited to, hiking trails, camping, picnicking, mountain biking, off-road vehicles, fishing, horseback riding, target shooting, and hunting. Hiking trails are accessible from a portion of the Lytle Creek Ranch property.

Other uses located in the vicinity of the project site include the San Bernardino County Sheriff's Regional Public Safety Training Center and the Glen Helen Rehabilitation Center, which is San Bernardino County's primary facility for housing both male and female inmates sentenced to County commitments.

The Rosena Ranch residential community (formerly Lytle Creek North), which is currently under construction, is located directly adjacent to the project site and shares access to the Lytle Creek Ranch property via Glen Helen Parkway ~~and Clearwater Parkway~~. When completed, the Rosena Ranch will offer a mix of single-family and multi-family residences on 386 acres. -The project is being developed by Lennar and SunCal and is located in unincorporated San Bernardino County.

An unincorporated area of County land is located immediately to the southwest of the project site between Riverside Avenue and the Lytle Creek Ranch project site. This existing neighborhood consists of single-family detached homes on lots ranging in size from 8,000 square feet to more than 10,000 square feet.

2.4 HISTORICAL BACKGROUND OF RIALTO

Ancient artifacts discovered by archaeologists suggest that the area now known as the City of Rialto was settled prior to the year 1500 AD. These artifacts indicate that the Serrano Indians probably lived in the Rialto area between 1500 and 1800 AD, although no trace remains to suggest their eventual relocation.

Records indicate that in 1769 the King of Spain awarded portions of the Rialto area to selected Spanish Dons. By 1854, many families began to move into the area and developed Rialto into a haven for vineyards, citrus groves and ranches. Early residents of Rialto prided themselves for the cultivation of muscat grapes, and this became an important contribution to the economy. Original cuttings of the muscat grapes were sent to neighboring counties.

In 1887, a group of Methodists seeking land on which to build a college arrived in the area. Although the college was never constructed, it was the Methodists who started the town of Rialto. It is not known how Rialto got its name; one story suggests that "Rialto" is a contraction of *rio* (river) and *alto* (high), and another argues that the area was named after the Rialto Bridge located in Venice, Italy, as the river that once crossed the city was reminiscent of the one in Italy. In either case, the bridge soon became a community symbol and became known as the "Bridge of Progress."

In 1887 a railroad connector line was built between San Bernardino and Pasadena by the Santa Fe Railroad. Along the line, townsites were located every 2,600 yards and, by the fall of that year, more than 25 new towns were built, including the community of Rialto. This same year the Semitropic Land and Water Company was formed to organize the purchase and selling of real estate, water and water rights and privileges. Rialto was incorporated as a city in 1911.

In the early part of the 20th century, the Rialto area was dominated by the citrus industry; however, it quickly developed as a bedroom community of Los Angeles following the Second World War. It has continued to evolve as part of the substantial urban development along Interstate 10 and the new Interstate 210 freeway. In 1989, the population of Rialto was 64,313. In 2005, the population of Rialto exceeded 101,000 persons and is expected to continue to increase. Rialto is now home to a variety of businesses and industry, including three major regional distribution centers: Staples, Inc., which serves stores across the entire West Coast of the United States, Toys "R" Us, and Target in the northern region of the City.

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2.5 ENVIRONMENTAL SETTING

2.5.1 SITE TOPOGRAPHY

The project area may best be characterized as being made up of a wide, gently sloping, stony alluvial plain at the base of the San Bernardino Mountains. The project area begins at the base of these mountains and gradually falls to the southeast with a slope averaging three percent.

Elevations within the Lytle Creek Wash portion of the project area range from approximately 2,180 feet above mean sea level (MSL) in the upstream portion to 1,450 feet above MSL in the downstream portion. Elevations within the northeast portion of the property range from 2,260 feet above MSL to 1,980 feet above MSL west of Interstate 15 and from 2,270 feet above MSL to 1,930 feet above MSL east of Interstate 15. (See Figure 2-5, Existing Site Topography.)

2.5.2 GEOLOGY AND SOILS

The soils within the project area are comprised predominantly of unconsolidated alluvium. The alluvium, consisting primarily of sands, gravels, cobbles and occasional boulders, is derived from granitic rock of the San Gabriel Mountains that has been deposited by Lytle Creek in the broad gently sloping alluvial plain on which the Cities of Rialto and Fontana are located.

The California Department of Conservation, Division of Mines and Geology, has designated most of Lytle Creek, including the project area, as a Mineral Resource Zone 2 (MRZ-2), which is defined as an area where adequate information indicates that significant mineral deposits are present. Additionally, according to the City of Rialto General Plan, the Department of Mines and Geology has further defined portions of the project area as containing important mineral resources. The mineral resources found are construction aggregate (sand and gravel) deposits. The site has not been used for mining in recent times.

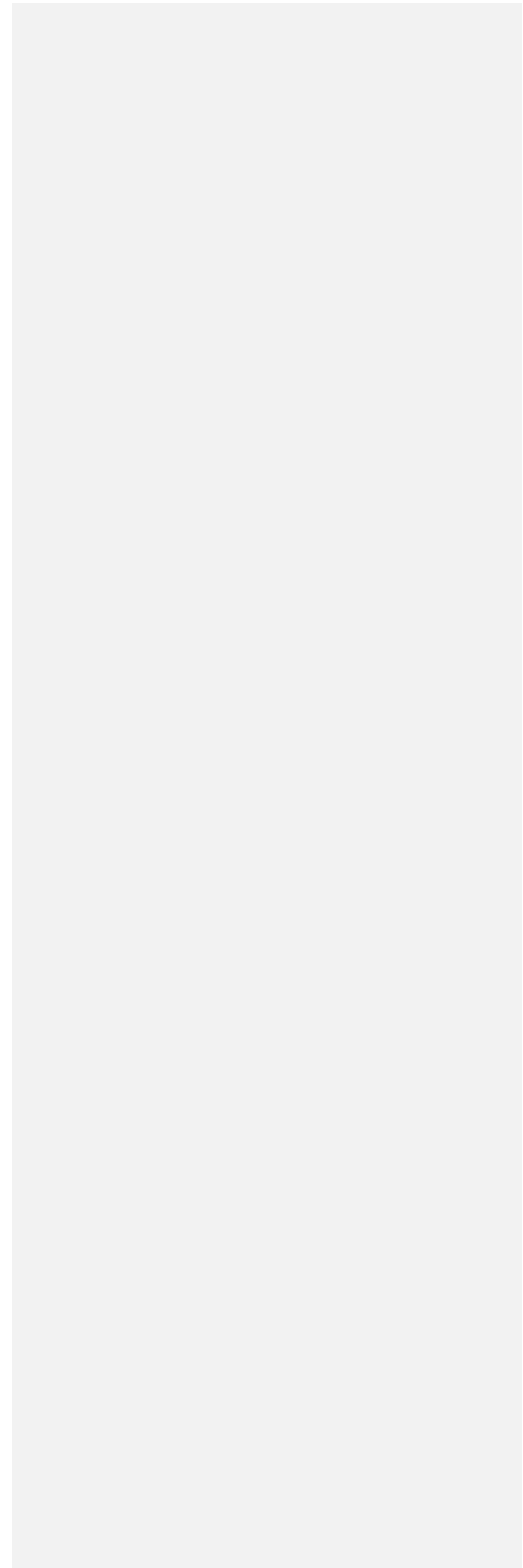
2.5.3 SEISMICITY AND FAULTING

The Rialto area, like most of Southern California, is located in a region of active faults. Active faults are defined by the California Department of Mines and Geology (1985) as those structures exhibiting displacement during Holocene times (i.e., within the last 11,000 years).

The project site contains several State-designated Alquist-Priolo Special Studies Zones. Seismic events have the potential to occur along these active fault zones.

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Figure 2-5
Site Topography



LYTLE CREEK RANCH
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2.5.4 HYDROLOGY

Alluvial fans are located at the base of mountain ranges where creeks exit from incised canyon channels at a steep gradient to the more gently sloping valley floor. The project area is located at the apex of the alluvial fan head of the Lytle Creek watershed which has a tributary drainage area from the San Gabriel Mountains of over 50 square miles.

Extensive hydrological technical analysis and evaluation has been performed for the Lytle Creek Specific Plan. The project area is significantly influenced by Lytle Creek since much of the project is located within the active flood plain. Lytle Creek, an intermittent stream, flows from the northwest to southeast through the central portion of the project area with the channel pattern changing with each flood.

A significant existing influence impacting the project on-site watershed is the existing Cemex gravel mining operation which has developed three large excavation pits across the floodplain. Three excavations have been constructed so the pits daylight on the downstream side of the project. In addition, earthen diversion channels or levees have been constructed around the upstream perimeter of the excavations to protect the mining operation from flooding and to convey flows between the excavations.

Discharge from the Lytle Creek watershed enters the project site at three main locations. The first are the tributaries to Lytle Creek upstream of the I-15 Freeway. These canyons drain directly into the Lytle Creek through Sierra Avenue adjacent to the project boundary. The second are those tributaries which drain to the I-15 Freeway culverts at Sycamore Flats. The third location consists of the foothill area along the northerly boundary of the San Bernardino County Sheriff complex (and the former Verdemont Ranch). This drainage area generates a flow pattern which generally follows an alignment along the entrenched channel adjacent to the northerly boundary and also intercepts the drainage from the Sycamore Flats area.

2.5.5 BIOLOGICAL RESOURCES

The Lytle Creek Ranch Specific Plan area has been substantially disturbed in the past for agricultural and mining purposes, though much of the native vegetation has grown back. Thorough investigations of biological resources have been ongoing for several years for the purpose of establishing an accurate baseline of existing biological resources.

The majority of the area has, at one time, been disturbed. Recent natural disturbance include extensive fire damage that occurred in October 2003 and 2007, and heavy rains in the winter of 2004 to 2005. However, a mix of non-native and native vegetation is reestablishing itself over most of the Specific Plan area. Today, a fairly widespread system of existing flood control dikes and diversion channels is found throughout Lytle Creek Wash in the project area. In addition, there are a number of unimproved roads and trails, several groundwater recharge areas, and numerous illegal dump sites within the project site.

In addition, as part of the Lytle Creek North Planned Development Project, an approximately 212.7-acre portion of the Lytle Creek Wash (referred to as the "SBKR Conservation Area") was set aside as mitigation for the San Bernardino kangaroo rat (*Dipodomys merriami parvus*). During the Section 7 consultation process for the Lytle Creek North Planned Development Project, a mitigation and monitoring program was developed for the San Bernardino kangaroo rat (SBKR) Conservation Area, which included restoration of San Bernardino kangaroo rat habitat on 40 acres of an upland "island" within the SBKR Conservation Area. Habitat manipulation, to improve the habitat quality for the San Bernardino kangaroo rat, has been implemented, which includes reducing the cover of non-native grasses and dense chamise. Subsequently, the SBKR Conservation Area was expanded by four acres just upstream of Interstate 15 to provide mitigation for the West Valley Water District. Therefore, the total acreage of the combined SBKR Conservation Area is 216.7 acres. The project area supports 38 distinct plant communities and/or associations including 1,136.9 acres of Riverside alluvial fan sage scrub, 1.2 acres of white sage scrub, 15.7 acres of southern willow scrub, 0.3-acre of California sycamore alliance, and 1.7 acres of southern cottonwood willow riparian that are considered sensitive by the CDFG due to their limited distribution or their potential to support sensitive wildlife species.

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The only sensitive plant species observed within the project site is the Plummer's mariposa lily (*Calochortus plummerae*), a CNPS List 1B.2 species. Over 127,300 Plummer's mariposa lilies were mapped primarily within the proposed on-site Habitat Mitigation Area portion of the project area. The project site also contains two CNPS List 3 or List 4 plant species, namely Parry's spineflower (*Chorizanthe parryi* var. *parryi*), a California Native Plant Society (CNPS) List 3.2 species, and southern California black walnut (*Juglans californica* var. *californica*), a CNPS List 4.2 species. Plants in these two categories are not considered to have the same level of sustainability as plants designated as CNPS Lists 1A, 1B, and 2.

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Although CNPS Lists 3 and 4 plants require more information before rarity can be determined, they are included in the EIR analysis in order to provide full disclosure during the CEQA review process. In addition, the CNPS recently updated their Lists to utilize "Threat Codes", represented by decimal ranks following the list number. Threat code .2 indicates the species is fairly endangered in California (20-80% of identified occurrences threatened). An estimated total of 501,280 Parry's spineflower individuals identified were mapped within the project area. In addition, one southern California black walnut was observed within the project area. Although slender-horned spineflower (*Dodecahema leptoceras*) was documented on-site by MBA in 1994, it has not been observed during consecutive focused surveys conducted by PCR and is considered absent from the project area today.

Sensitive wildlife species observed within the project area include coast (San Diego) horned lizard [(*Phrynosoma coronatum* ssp. *Blainvillei*), California Species of Special Concern (CSC)], golden eagle [(*Aquila chrysaetos*), State fully protected (SFP), CSC], northern harrier [(*Circus cyaneus*), CSC], American peregrine falcon [(*Falco peregrinus anatum*), state endangered, SFP], white-tailed kite [(*Elanus leucurus*), SFP], burrowing owl [(*Athene cunicularia*), CSC], loggerhead shrike [(*Lanius ludovicianus*), CSC], Vaux's swift [(*Chaetura vauxi*), CSC], willow flycatcher [(*Empidonax traillii*), state endangered], least Bell's vireo [(*Vireo bellii pusillus*), state and federally endangered], yellow warbler [(*Dendroica petechia brewsteri*), CSC], yellow-breasted chat [(*Icteria virens*), CSC], western mastiff bat [(*Eumops perotis*), CSC], pocketed free-tailed bat [(*Nyctinomops femorosaccus*), CSC], San Diego black-tailed jack rabbit [(*Lepus californicus bennettii*), CSC], northwestern San Diego pocket mouse [(*Chaetodipus fallax fallax*), CSC], Los Angeles pocket mouse [(*Perognathus longimembris brevinasus*), CSC], and San Bernardino kangaroo rat (federally endangered).

Two transient willow flycatcher (state-listed endangered) were observed within the project area in 2006, and another individual was observed in 2007, and another transient individual in 2008. Based on willow flycatcher migration patterns and periods, these birds are considered to be transient, as they were not observed on any previous or subsequent surveys for the state- and federally-listed endangered southwestern willow flycatcher. No state- and federally-listed endangered southwestern willow flycatcher are present in the Specific Plan area.

In 2006, ~~two pairs of least Bell's vireos were observed during the survey period within Sycamore Flat in Neighborhood I and~~ at least one least Bell's vireo was observed numerous times within a narrow riparian corridor along the western portion of Neighborhood II. During protocol breeding season surveys in 2007, no pairs of least Bell's vireo were observed within the project area boundaries, ~~but one pair was found within the Sycamore Flats riparian corridor just off site and north of the Neighborhood I boundary, within the County Parks parcel.~~ During protocol breeding season surveys in 2008, no least Bell's vireos were detected within the project area.

A total of approximately 620.3 acres are considered to be occupied by the San Bernardino kangaroo rat within Neighborhoods II and III, and an additional 76.5 acres are considered to be

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occupied by the species within Neighborhood IV. Los Angeles pocket mouse occupies 399.9 acres within Neighborhood III and Neighborhood IV. A total of 647.6 acres supporting Riversidean alluvial fan sage scrub (including mixed communities where this community is dominant) on-site within the 100-year floodplain are considered viable habitat for the San Bernardino kangaroo rat.

Wildlife species for which focused surveys of habitat assessments resulted in negative findings include the coastal California gnatcatcher. Focused surveys from 2005 through 2007 have not detected this species on-site. In addition, burrowing owl was only incidentally observed within the proposed on-site Habitat Mitigation Area and not within the development portion of the Specific Plan area.

The majority of the area has, at one time, been disturbed. Recent natural disturbance include extensive fire damage that occurred in October of 2003 and 2007, and heavy rains in the winter of 2004 to 2005. However, native vegetation is reestablishing itself over most of the project area. Today, a fairly widespread system of flood control dikes and diversion channels is found throughout Lytle Creek Wash in the Specific Plan area. In addition, there are a number of unimproved roads and trails, several groundwater recharge areas, and numerous illegal dump sites within the project area.

In addition, as part of the Lytle Creek North Planned Development Project, an approximately 212.7-acre portion of the Lytle Creek Wash (referred to as the "SBKR Conservation Area") was set aside as mitigation for the San Bernardino Kangaroo rat (Figure 3, Prescribed Land Uses). During the Section 7 consultation process for the Lytle Creek North Planned Development Project, a mitigation and monitoring program was developed, which included restoration of SBKR habitat on 40 acres of an upland "island" within Lytle Creek Wash, within the SBKR Conservation Area. Habitat manipulation, to improve the habitat quality for the San Bernardino kangaroo rat, has been implemented, which includes reducing the cover of non-native grasses and dense chamise.

Slender-horned Spineflower

PCR biologists performed focused surveys for the federally and state endangered slender-horned spineflower for four consecutive years throughout the project area with particular attention to areas of suitable habitat (i.e., sandy benches exposed to periodic flooding within alluvial fan sage scrub). Prior to conducting the 2004, 2005, 2006, and 2007 surveys, a reference site in Redlands (Fred Roberts, Botanists, email Comm. with Linda Robb, April and May, 2005) for the slender-horned spineflower was visited by PCR biologists to verify the species was blooming during the survey periods. In 1994, MBA biologists observed three populations of this species in the northwest portion of the project area on both sides of the I-15 Freeway (MBA 1995). The populations, from upstream to downstream, contained 366, 92, and 9 individuals, respectively, for a total of 467 slender-horned spineflower plants observed within the project area in 1994. However, slender-horned spineflower was not observed during the 2004, 2005, 2006, and 2007 sensitive plant surveys despite focused surveys following normal and above normal rainfall seasons and confirmation of this species

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blooming at a known reference site. Therefore, the species is no longer believed to occupy areas on-site.

Santa Ana River Woollystar

PCR biologists reviewed a body of scientific literature on the woollystar (*Eriastrum densifolium*) in an attempt to determine the subspecies that occurs within the project area. In a 1997 study by Brunell and Whitkus, Random Amplified Polymorphic DNA (RAPD) markers (i.e., DNA testing) were not conclusive in distinguishing between subspecies. In order to differentiate the federally endangered Santa Ana River woollystar (*Eriastrum densifolium* ssp. *sanctorum*) from one of its four non-listed closely related subspecies (*Eriastrum densifolium* ssp. *austromontanum*, *Eriastrum densifolium* ssp. *densifolium*, *Eriastrum densifolium* ssp. *elongatum*, and *Eriastrum densifolium* ssp. *mohavense*), recent research indicates that the subspecies *sanctorum* is best characterized by morphological features. *Eriastrum densifolium* ssp. *sanctorum* has greater corolla tube length than other subspecies and its mean filament (i.e., a flower part) length is statistically the largest in the species (Brunell and Whitkus 1999a). Brunell and Whitkus even state that of all the subspecies of *Eriastrum densifolium*, only the subspecies *sanctorum* is distinct enough to be classified as a separate subspecies because of its morphology (Brunell and Whitkus 1999a and 1999b).

Following the literature review, PCR biologist Robert Freese, Ph.D. designed a sampling protocol with input from Dr. Mark Brunell (Mark Brunell, email comm. with Robert Freese, June 15, 2006). PCR biologists sampled 60 individual *Eriastrum densifolium* plants within the project area on June 28, 2006. Samples were taken from throughout the range of the species mapped within the project area during focused sensitive plant surveys performed in 2004 and 2005. The corolla tube length was measured in millimeters (mm) and recorded for three separate flowers per plant. The height of each plant was also recorded, as well as any other characteristics that appeared significantly different for any given plant. The data were analyzed and an average corolla tube length was calculated for each plant as well as for the sample population as a whole. This was then compared to the range of corolla tube lengths suggested by the scientific literature for *Eriastrum densifolium* ssp. *sanctorum* and *Eriastrum densifolium* ssp. *elongatum*. The results of PCR's 2006 analysis, which was based upon work and peer-reviewed by Dr. Mark Brunell, indicate that the project area does not support the endangered subspecies *Eriastrum densifolium* ssp. *sanctorum*.

Plummer's Mariposa Lily

PCR biologists performed focused surveys for the CNPS List 1B.2 Plummer's mariposa lily in 2005 throughout the project area, ~~with the exception of Neighborhood I~~. Particular attention was given to areas of suitable habitat (i.e., a variety of dry habitat including Riversidean alluvial fan sage scrub, Riversidean sage scrub, and chaparral). In 2006, ~~focused surveys were completed within Neighborhood I, while areas of~~ the project area previously surveyed were verified. In 2005, PCR biologists observed 103 individuals within Neighborhoods II, III, and IV, and another estimated 127,200 plants over approximately 45.1 acres in the eastern portion of the large island, adjacent to the SBKR Conservation Area, within the Lytle Creek Wash portion of the project area.

Parry's Spineflower

PCR biologists performed focused surveys for the CNPS List 3.2 Parry's spineflower in 2004 and 2005 throughout the project area, with particular attention given to areas of suitable habitat (i.e., open areas within Riversidean alluvial fan sage scrub, Riversidean sage scrub, and chaparral). In 2006, areas previously mapped were verified and any new significant populations observed were added to the data set. In 1994, Parry's spineflower was found within the project area by MBA biologists in 157 acres of chamise chaparral, alluvial fan sage scrub, and Riversidean sage scrub habitats. At that time, thirteen populations of Parry's spineflower containing an estimated 440,000 individuals were identified within the project area. During the 2005 and 2006 survey seasons, PCR biologists confirmed the locations of the major populations within the project area and also documented several additional small populations (totaling 61,280 plants) throughout the project area. Therefore, an estimated 501,280 plants have been mapped as occurring within the project area.

Coastal California Gnatcatcher

Focused surveys for the federally-threatened coastal California gnatcatcher were conducted in 2005, 2006, and 2007 by PCR biologists Susan Erickson (Permit No. TEO85187-0), Kristin Szabo (Permit No. TE016487-0), Jenni Snibbe (Permit No. TE044520-0), Linda Robb (Permit No. TE093591-0), and Jason Berkley (Permit No. TE009015-1) in accordance with the USFWS Coastal California Gnatcatcher Presence/Absence Survey Guidelines, issued July 28, 1997 (USFWS 1997). Accordingly, six surveys were performed at least one week apart, between 6:00 a.m. and 12:00 noon, within all portions of the project area containing suitable habitat. A maximum of 80 acres per person per survey day were surveyed. To ensure coverage of adjacent areas, vocalizations were broadcast outside the boundaries where suitable coastal California gnatcatcher habitat exists. Focused protocol surveys performed in 2005, 2006, and 2007 did not detect this species within the project area.

Burrowing Owl

PCR biologist Jason Berkley conducted the Phase I (Habitat Assessment) and Phase II (Burrow Surveys) surveys for burrowing owl, a California Species of Special Concern (CSC), on May 25, 2005 and on April 21, 2006. The Phase I and Phase II surveys were repeated in the spring of 2007. To determine presence/absence of suitable habitat for burrowing owl, the project area was thoroughly searched for areas containing suitable habitat indicators. A Phase II, Burrow Survey was conducted immediately following the Phase I, Habitat Assessment to determine if any of the existing small fossorial mammal burrows contained evidence of burrowing owl. The burrowing owl was incidentally observed within the project area but outside the development footprint in the San Bernardino kangaroo rat Conservation Area in September of 2006 and in February of 2007 during trapping for the San Bernardino kangaroo rat. However, the Phase I, Habitat Assessment and Phase II, Burrow Survey in 2006 and 2007 did not detect this species within the development portion of the project area.

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Southwestern Willow Flycatcher

Focused surveys for the federally and state endangered southwestern willow flycatcher were performed in 2006, 2007, and 2008 by PCR permitted biologists in accordance with USFWS's *Southwestern Willow Flycatcher Presence/Absence Survey Guidelines*, issued July 11, 2000. Accordingly, five surveys of all riparian habitat within the project area were conducted within three survey periods. All surveys were conducted at least five days apart and began at dawn and ended between 9:00 a.m. and 11:00 a.m., with all portions of the project area containing suitable habitat. No state and federally listed endangered southwestern willow flycatcher are present in the Specific Plan area. Two transient willow flycatchers (state-listed endangered) were observed within the project area in 2006 and another transient individual was observed in 2007 and another transient individual in 2008. Based on willow flycatcher migration patterns and periods, these birds are considered to be transient, as they were not observed on any previous or subsequent surveys for the state and federally-listed endangered southwestern willow flycatcher.

Least Bell's Vireo

Focused surveys for the federally and state endangered least Bell's vireo were performed in 2006, 2007, and 2008 by PCR permitted biologists in accordance with USFWS's Least Bell's Vireo Survey Guidelines, issued January 19, 2001. Accordingly, eight (8) surveys were performed between April 10 and July 31 in each of those three years. Surveys were conducted no less than ten (10) days apart, between dawn and 11:00 a.m., within all portions of the project area containing suitable riparian habitat and adjacent habitat potentially used for foraging. In 2006, two pairs of least Bell's vireos were observed during the survey period within Sycamore Flat in Neighborhood I and at least one least Bell's vireo was observed numerous times within a narrow riparian corridor along the western portion of Neighborhood II. During protocol breeding season surveys in 2007, no pairs were found within the project area, but one pair of least Bell's vireo was observed within the Sycamore Flats riparian corridor just north of land off site of the Neighborhood I project area boundary, within the County Parks parcel. During protocol breeding season surveys in 2008, no least Bell's vireos were detected within the project area.

San Bernardino Kangaroo Rat

Based upon focused trapping surveys in 2005, 2006 and 2007, the San Bernardino kangaroo rat occupied a total of 696.8 acres within Neighborhoods II, III, and IV within the project area.

Los Angeles Pocket Mouse

Los Angeles pocket mouse occupies approximately 399.9 acres within Neighborhoods III and IV.

2.5.6 CULTURAL RESOURCES

An archaeological record search and field investigation were previously conducted for the Lytle Creek project. The record search indicated the limited presence of both prehistoric and historic resources within the project boundaries; however, during the field survey of the property, many of these resources originally identified through the record search were found to have been destroyed

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or substantially altered by natural phenomena and human activity. The field investigation also resulted in identification of several previously unidentified historic resources. A detailed impact analysis and appropriate mitigation measures are provided in the Environmental Impact Report prepared for this project.

According to the 1992 General Plan Update and the City's adopted Specific Plans there are no known paleontological sites in the City of Rialto. A paleontologic field investigation conducted on the project area indicates that paleontologic sensitivity remains low and paleontologic resources, if any, are not expected to be adversely impacted.

2.5.7 EXISTING CIRCULATION

The Interstate 15 Freeway (I-15) bisects a portion of the project site. Regional access to the site is available from the I-15 Freeway at the Sierra Avenue and Glen Helen Parkway intersections. Other regional access to the site is available from State Route 210 (SR-210) via an interchange at Riverside Avenue. The project site is accessible from several local streets including Glen Helen Parkway, ~~Clearwater Parkway~~, Lytle Creek Road, Riverside Avenue, Country Club Drive, and Oakdale Avenue. Riverside Avenue is designated on the City's Master Plan of Arterial Highways as a Major Arterial. Several streets terminate at the project boundary including Live Oak Avenue (a Major Arterial), Alder Avenue (a Major Arterial), Locust Avenue (a Secondary Arterial), and Linden Avenue (a Secondary Arterial).

2.6 RIGHTS-OF-WAY/EASEMENTS

The project site is crossed by a number of rights-of-way and easements, which are depicted on a series of maps. There are separate exhibits for Neighborhoods II, III, and IV, ~~and two exhibits for Neighborhood I~~. Refer to Figures 2-6 through 2-10.

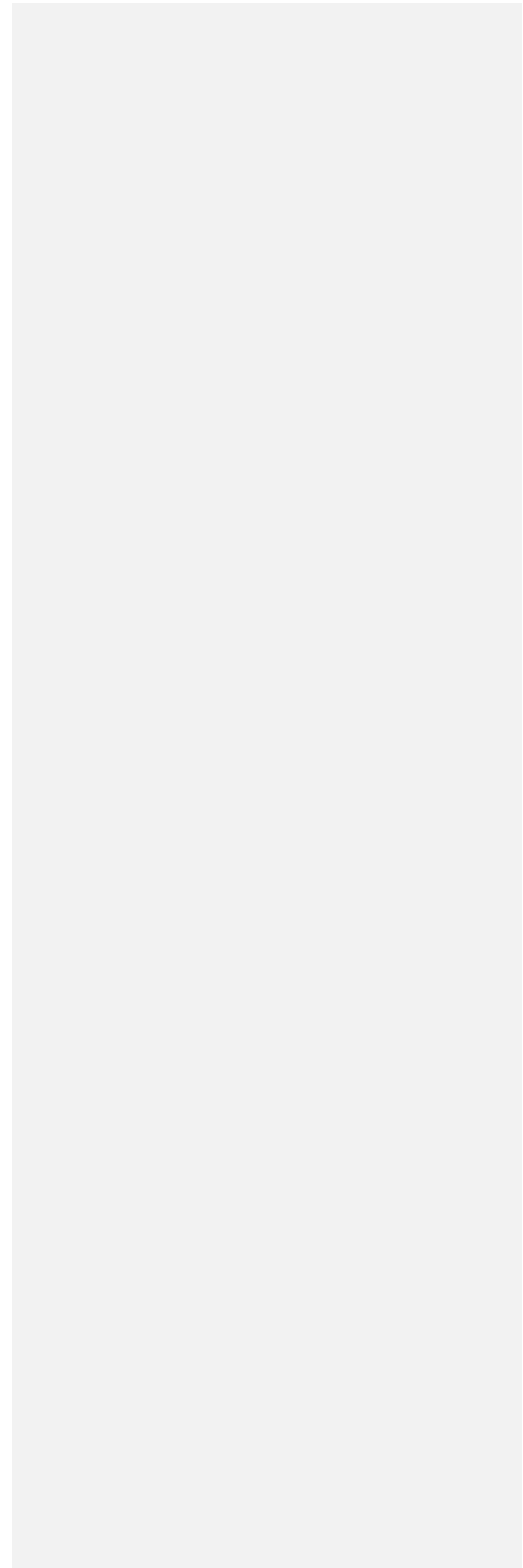
2.7 RELATIONSHIP TO OTHER PLANS

The Lytle Creek Ranch Specific Plan will require an amendment to the City of Rialto General Plan in order to ensure full consistency between the two documents. The General Plan Amendment will require changes to the General Plan Land Use Map, as well as select changes to the General Plan document text.

When approved, this Specific Plan will also supersede a portion of other existing approved documents, including the Glen Helen Specific Plan (County of San Bernardino) and the Lytle Creek North Preliminary Development Plan (County of San Bernardino). Areas to be removed from these adopted plans include Planning Areas 1 through 15 of the Lytle Creek Ranch Specific Plan.

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Figure 2-6
Site Constraints – Neighborhood I (East Portion)

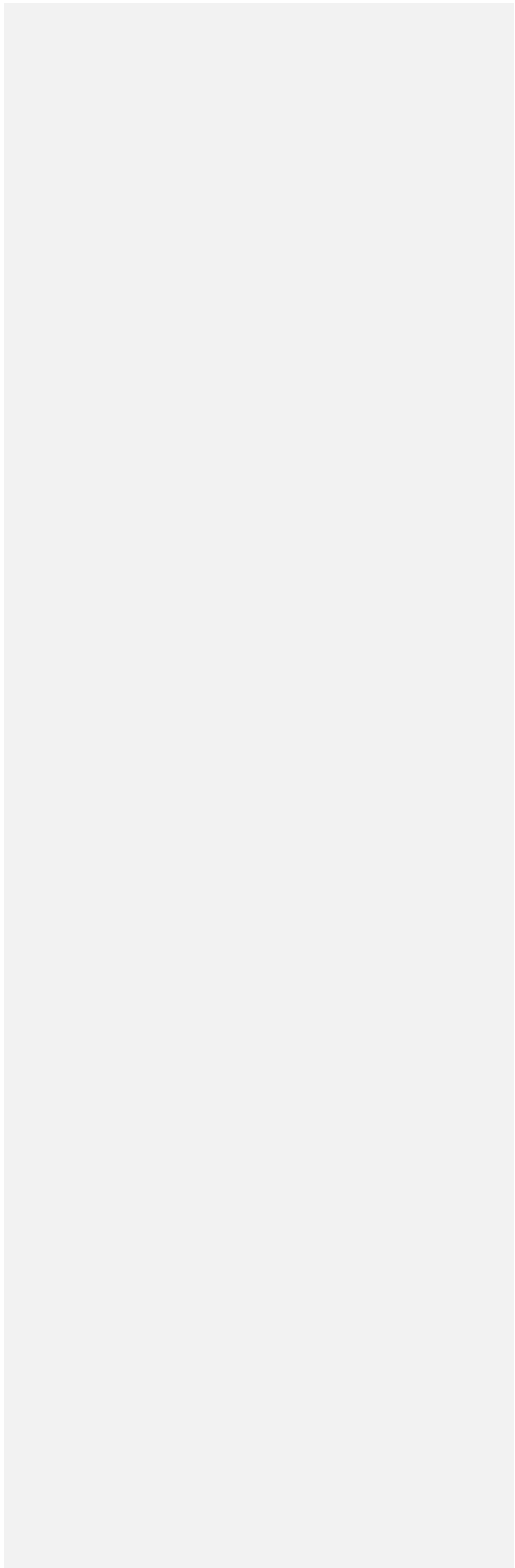


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Figure 2-7
Site Constraints – Neighborhood I (West Portion)

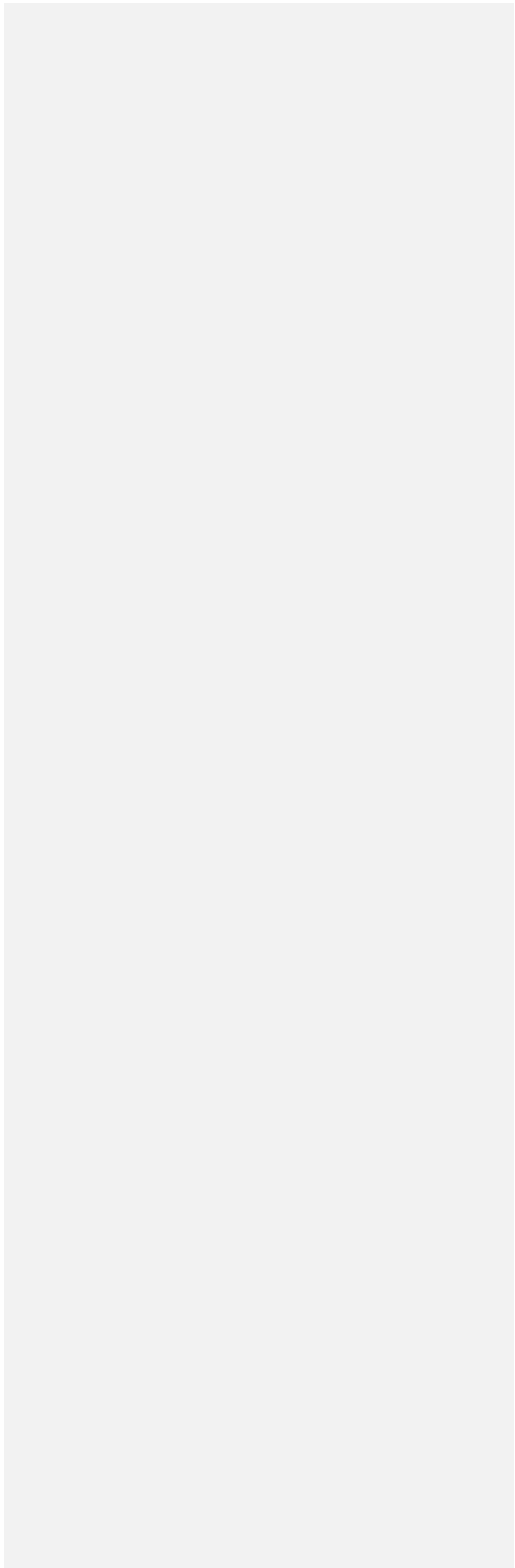


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Figure 2-8
Site Constraints – Neighborhood II

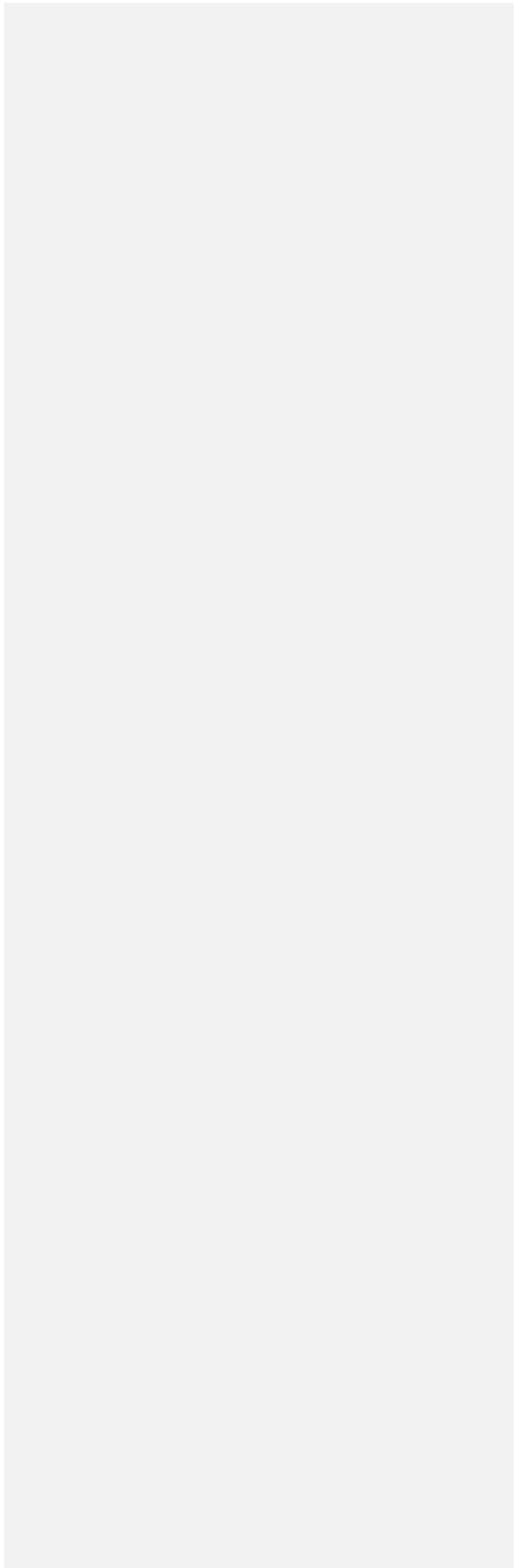


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LYTLE CREEK RANCH
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Figure 2-9
Site Constraints – Neighborhood III

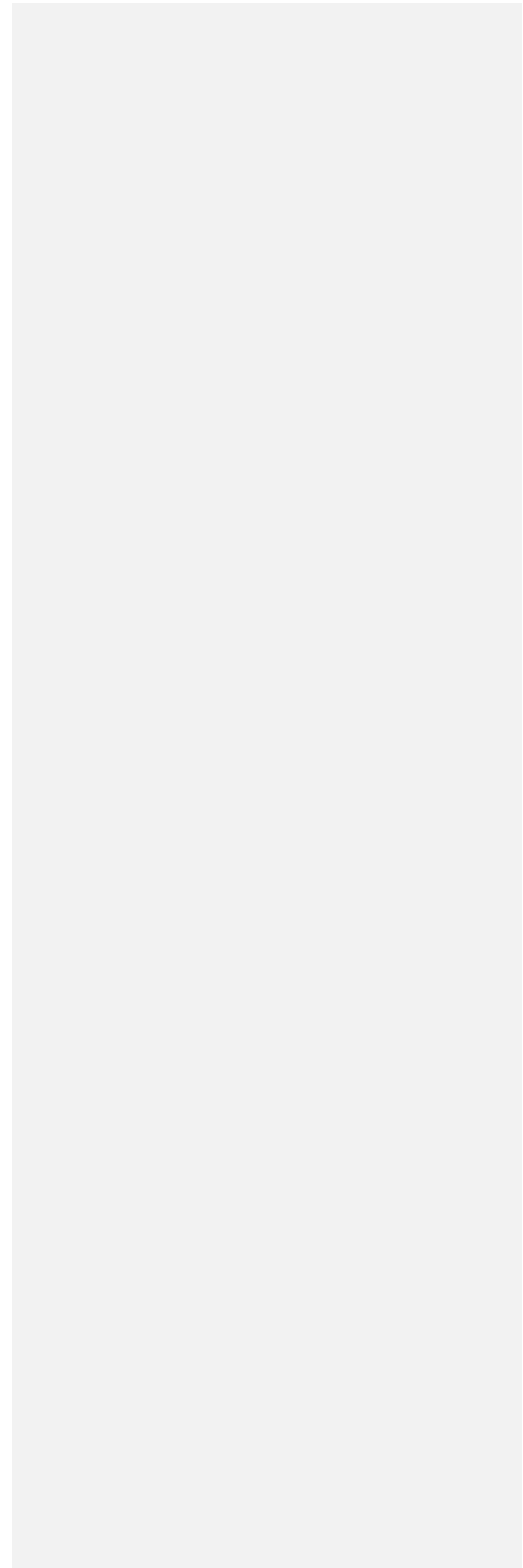


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Figure 2-10
Site Constraints – Neighborhood IV



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3.0 PLAN ELEMENTS

3.1 PURPOSE AND INTENT

This Chapter contains a discussion of the various plan elements for Lytle Creek Ranch, including the following:

- Land Use Plan
- Parks and Recreation Plan
- Open Space and Conservation Plan
- Circulation Plan
- Infrastructure and Services Plan
- Grading Plan

Each plan works in tandem with the other plans to establish a framework for Lytle Creek Ranch, ensuring that the project will develop as a rich, vibrant community with schools, parks, open space, residences, retail uses, and other types of development.

3.2 LAND USE PLAN

3.2.1 LAND USE PLAN DESCRIPTION

Lytle Creek Ranch is a ~~2,447.030~~-acre master planned community located partially within the city limits of Rialto and mostly within the City's sphere of influence in San Bernardino County. The project is designed to contain a series of neighborhoods clustered around public amenities such as parks, ~~a public 18-hole golf course~~, an elementary school, an elementary/middle (K-8) school, recreation areas, and open space. Lytle Creek Ranch will focus on the health, wellness, and fitness of its residents.

This community is designed as four separate and unique neighborhoods, each with its own identity:

- ~~**Neighborhood I**—includes the 417 acres of land in the areas located partially within the boundaries of the adopted Glen Helen Specific Plan (County of San Bernardino). This area is sometimes referred to as “Sycamore Flats East” and “Sycamore Flats West.” The remaining portions of Neighborhood I are located within the “Lytle Creek North Planned Development.” Most of the Lytle Creek North Planned Development is under construction and is known as “Rosena Ranch.” When approved by the City, the Lytle Creek Ranch Specific Plan will become the governing document for all of Neighborhood I. This document will supersede those portions of the Glen Helen Specific Plan and the Lytle Creek North Planned Development located within the Lytle Creek Ranch Specific Plan area.~~

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- **Neighborhood II** – is planned as a conventional single-family neighborhood or potentialan Active Adult ~~golf course~~ community on approximately 802 acres and includes the entire El Rancho Verde Specific Plan area. When approved by the City of Rialto, the Lytle Creek Ranch Specific Plan will supersede the Amended and Restated El Rancho Verde Specific Plan.
- **Neighborhood III** – is located south of I-15 and is planned to appeal to families of all ages, couples and singles. Neighborhood III will include a mix of single-family detached and attached homes, as well as retail/commercial/office development, on approximately 969 acres.
- **Neighborhood IV** – includes multi-family residential and retail/commercial/office development on approximately 259 acres located north of the I-15, adjacent to Lytle Creek Road and Sierra Avenue.

The following Table 3-1 shows a breakdown of the different land uses proposed within the Lytle Creek Ranch Specific Plan. Please also refer to Figure 3-1, Conceptual Land Use Plan.

**TABLE 3-1
LYTLE CREEK RANCH LAND USE SUMMARY**

Use	Density Range (du/ac)	Target Dwelling Units (du)	Median Density (du/ac)	Intensity (sq. ft.)	Acres
Residential Land Uses					
Single-Family Residential One (SFR-1)	2 – 5	943748	3.6	--	263.22 <u>06.2</u>
Single Family Residential Two (SFR-2)	5 – 8	1,9081,721	6.39	--	304.52 <u>50.4</u>
Single-Family Residential Three (SFR-3)	8 – 14	2,4031,753	10.911.1	--	220.01 <u>57.4</u>
Multi-Family Residential (MFR)	14 – 28	1,828	17.2	--	106.3
High Density Residential (HDR)	25 – 35	1,325989	29.26	--	45.433 <u>4</u>
Subtotal	--	8,4077,039	8.99.3	--	939.47 <u>53.7</u>
Elementary School	2 – 14	as transferred	--	--	10.0
Elementary School/Middle School	2 – 14	as transferred	--	--	14.0
Village Center Commercial (VC)	--	--	--	849,420 ¹	95.6
Open Space / Recreation	--	--	--	--	328.82 <u>82.5</u>
Open Space / Joint-Use Park and School	--	--	--	--	17.0
Open Space (undisturbed)	--	--	--	--	908.07

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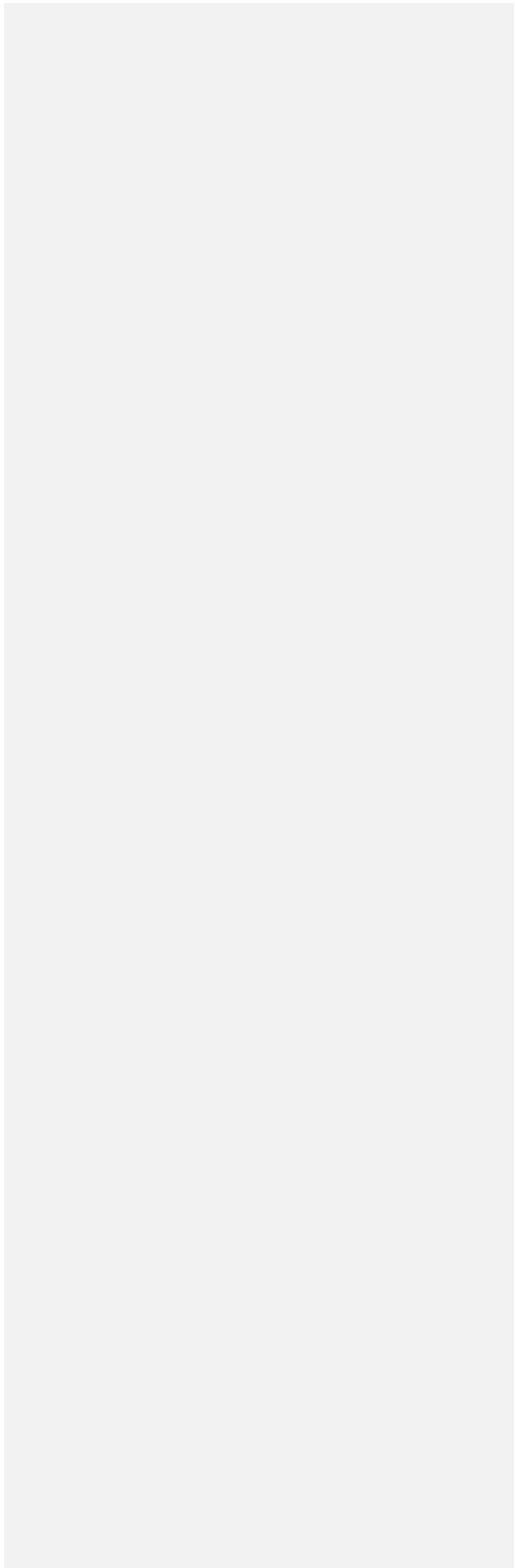
					69.0
Roadways	--	--	--	--	134.58 8.3
Totals	--	8,407,039	3,443.47	849,420	2,447.3 2,030.1

Note

¹ Assumes 849,420 square feet of retail uses and commercial uses.

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Figure 3-1
Conceptual Land Use Plan



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Planning Area Development Standards

The following standards shall apply to development in Lytle Creek Ranch:

- A. Lytle Creek Ranch will build-out with a total of 103 planning areas. Each planning area is assigned a planning area number, land use, and acreage. Because of refinements in design and engineering, it is anticipated that planning areas will change somewhat in configuration and size as the land use plan is implemented. Table 3-2, Detailed Summary by Planning Area, includes detailed information on acreage, target number of dwelling units, density range, target density, and retail square footage, as applicable, for each planning area within Lytle Creek Ranch.
- B. Residential planning area land use categories (i.e. SFR-1, SFR-2, SFR-3, MFR, HDR) within the Lytle Creek Ranch Specific Plan area, as depicted in Figure 3-1, Conceptual Land Use Plan, are target density ranges only. The Specific Plan requires that no more than 8,407 dwelling units may be constructed in the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. Gross density shall be calculated by including all Specific Plan area acreage including open space land use categories and roadway acreages.
- C. Village Center Commercial areas may build out with any combination of retail, commercial, office and business park, and medical/dental uses.
- D. Any and all planning areas designated as “Open Space/Recreation” may be reconfigured and re-sized to respond to actual park and open space needs pursuant to Quimby Act requirements.
- E. The total acreages of Planning Area 48 (Open Space/Joint-Use) and Planning Area 49 (Elementary School), when combined, shall not be less than 15.0 acres; provided, however, that a reduction in acreage shall be permitted if the Rialto Unified School District determines that a smaller school site and joint-use park/school site is acceptable for their purposes.
- F. The total acreages of Planning Area 69 (Elementary/Middle School) and Planning Area 74 (Open Space/Joint-Use), when combined, shall not be less than 26.0 acres; provided, however, that a reduction in acreage shall be permitted if the Rialto Unified School District determines that a smaller school site and joint-use park/school site is acceptable for their purposes.
- G. If the Rialto Unified School District elects not to utilize one or both of the designated school sites, then the unused school site(s) may develop with Single-Family Residential One, Two, or Three (SFR-1, SFR-2, or SFR-3) development, subject to City approval of a Site Plan. In such instance, unused dwelling units from elsewhere within Lytle Creek Ranch Neighborhoods II and III may be transferred to the unneeded school site(s). The maximum number of dwelling units within Lytle Creek Ranch shall not exceed 8,407 dwelling units in any event.

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- H. If it is determined by the Rialto Unified School District that the “Open Space/Joint-Use” planning areas (Planning Areas 48 and 74) may be reduced in size, then the excess land may develop with Single-Family Residential One, Two, or Three (SFR-1, SFR-2, or SFR-3) development, subject to City approval of a Site Plan. Unused dwelling units from elsewhere within Lytle Creek Ranch Neighborhoods II and III may be transferred to the unneeded “Open Space/Joint-Use” land. The maximum number of dwelling units within Lytle Creek Ranch shall not exceed 8,407 dwelling units in any event.
- I. If Lytle Creek Ranch builds out with fewer than 8,407,039 dwelling units, the corresponding need for park and recreation land may also decrease. In such instance, portions of the land use plan designated as “Open Space/Recreation” (in excess of the acreage required to comply with the Quimby Act) may develop instead with Single-Family Residential One, Two, or Three (SFR-1, SFR-2, or SFR-3) development, subject to City approval of a Site Plan. Unused dwelling units from elsewhere within Lytle Creek Ranch Neighborhoods II and III may be transferred to the unneeded “Open Space/Recreation” land. The maximum number of dwelling units within Lytle Creek Ranch shall not exceed 8,407,039 dwelling units in any event.
- J. Private recreation centers will be constructed in portions of Planning Areas 40, 53, and 64 for use by Neighborhood III residents. These private recreation centers are anticipated to vary in size between approximately two to five acres. These recreation centers are planned to be privately owned and maintained by a homeowners association or other entity acceptable to the City of Rialto.
- K. Residents of Neighborhood II will have their own recreational facilities designed specifically for their community Active Adult users. The Active Adult recreation center is planned to be privately owned and maintained by a homeowners association or other entity acceptable to the City of Rialto.
- L. At the discretion of the project master developer or builder, small, private recreation centers (consisting of a swimming pool, restrooms, drinking fountain, and/or other recreational amenities) may be constructed within any planning area designated for residential development within Lytle Creek Ranch, subject to City approval of a Site Plan. As determined by the project master developer or builder, these small recreation centers may be limited to use solely by one particular planning area, or for use by two or more planning areas. If provided, these recreation centers shall be privately owned and maintained by a homeowners association or other entity acceptable to the City of Rialto.
- M. If a warehouse operation is located within Planning Areas 3, 4, 11, 13, 15, and/or 20, which is adjacent to a planning area developed with residential uses, then the operating hours of the warehouse operation shall be restricted between the hours of 8 p.m. until 5 a.m. daily.

TABLE 3-2
DETAILED SUMMARY BY PLANNING AREA

Planning Area	Land Use	Acreage	Target No. of Dwellings	Density Range (du/ac)	Target Density* (du/ac)	Retail Square Footage
Neighborhood I						
4	Open Space	29.0	-	-	-	-
2	Open Space	14.0	-	-	-	-
3	SFR-1 Residential	46.0	129	2-5	2.8	-
4	High Density Residential	12.0	336	25-35	35.0	-
5	Open Space	40.0	-	-	-	-
6	Open Space	43.0	-	-	-	-
7	Open Space	44.0	-	-	-	-
8	SFR-1 Residential	85.0	347	2-5	4.1	-
9	Open Space	1.0	-	-	-	-
10	Open Space/Recreation	11.0	-	-	-	-
11	SFR-3 Residential	3.0	24	8-14	8.0	-
12	Open Space	3.0	-	-	-	-
13	SFR-3 Residential	29.0	370	8-14	12.8	-
14	Open Space	2.0	-	-	-	-
15	SFR-3 Residential	9.0	72	8-14	8.0	-
	Roadways	46.2	-	-	-	-
-	Neighborhood I Total	417.2	1,278	-	-	0
Neighborhood II						
80	Open Space	468.0202	-	-	-	-
		8	-	-	-	-
81	Open Space/Recreation	5.0	-	-	-	-
82	SFR-3 Residential	30.024.9	336292	8-14	11.211.7	-
83	SFR-2 Residential	107.079.8	692640	5-8	6.58.0	-
84	SFR-3 Residential	23.020.5	249235	8-14	10.811.4	-
85	Open Space/Recreation	1.0	-	-	-	-
86	Open Space/Recreation	3.0	-	-	-	-
87	Open Space/Recreation	45.0	-	-	-	-
88	Open Space/Recreation	5.0	-	-	-	-
89	Village Center Commercial	6.0	-	-	-	54,885
90	Village Center Commercial	2.0	-	-	-	18,295
91	Village Center Commercial	3.2	-	-	-	29,272
92	High Density Residential	13.0	364	25-35	28.0	-
93	SFR-3 Residential	54.0	551	8-14	10.2	-
94	SFR-2 Residential	30.0	177	5-8	5.9	-
95	Open Space/Recreation	67.0	-	-	-	-
96	Open Space	8.0	-	-	-	-
97	Open Space/Recreation SFR-2 Residential	5.020.2	-130	-5-8	-6.4	-
98	SFR-2-1 Residential	54.026.9	346146	5-82-5	5.95.4	-
99	Open	60.020.1	-87	-2-5	-4.3	-

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Planning Area	Land Use	Acreage	Target No. of Dwellings	Density Range (du/ac)	Target Density* (du/ac)	Retail Square Footage
	<u>Space/Recreation</u> <u>SFR-1 Residential</u>					
100	SFR-3-1 Residential	14.06	42677	8-142-5	9.05.3	-
	<u>Open</u> <u>Space/Recreation</u> <u>SFR-1 Residential</u>					
101	SFR-2 Residential	35.023.4	-101	-2-5	-4.3	-
102	SFR-2 Residential	41.020.1	80131	5-8	7.36.5	-
	<u>SFR-1 Residential</u> <u>Open</u> <u>Space/Recreation</u>					
103	Roadways	41.6	-	-	-	-
	Neighborhood II Total	801.8	2,931	-	-	102,452

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Neighborhood III						
		<u>396.0398.</u>				
28	Open Space	2	-	-	-	-
29	Open Space/Recreation	5.0	-	-	-	-
30	High Density Residential	4.5	126	25-35	28.0	-
31	High Density Residential	15.9	499	25-35	31.4	-
32	Open Space/Recreation	9.8	-	-	-	-
	<u>Village Center</u>					
33	Commercial	24.1	-	-	-	230,955
34	Open Space/Recreation	7.3	-	-	-	-
35	SFR-3 Residential	11.5	115	8-14	10.0	-
36	SFR-2 Residential	3.8	28	5-8	7.4	-
37	Open Space/Recreation	2.6	-	-	-	-
38	SFR-2 Residential	10.6	77	5-8	7.3	-
39	MFR Residential	5.8	196	14-28	28.0	-
40	Open Space/Recreation	6.0	-	-	-	-
41	MFR Residential	4.8	96	14-28	20.0	-
42	SFR-2 Residential	13.0	66	5-8	5.1	-
43	SFR-1 Residential	22.0	62	2-5	2.8	-
44	MFR Residential	4.5	72	14-28	16.0	-
45	Open Space/Recreation	3.1	-	-	-	-
46	SFR-2 Residential	9.9	75	5-8	7.6	-
47	SFR-3 Residential	5.4	54	8-14	10.0	-
	<u>Open Space/Joint-Use</u>					
48	Park and School	5.0	-	2-14	-	-
49	Elementary School	10.0	-	2-14	-	-
50	MFR Residential	4.8	96	14-28	20.0	-
51	Open Space/Recreation	1.0	-	-	-	-
52	MFR Residential	9.4	141	14-28	15.0	-
53	Open Space/Recreation	8.0	-	-	-	-
54	SFR-2 Residential	13.1	75	5-8	5.7	-
55	MFR Residential	6.0	96	14-28	16.0	-
56	Open Space/Recreation	4.8	-	-	-	-
57	SFR-2 Residential	17.9	98	5-8	5.5	-
58	SFR-3 Residential	12.6	164	8-14	13.0	-
59	SFR-1 Residential	40.0	173	2-5	4.3	-

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Planning Area	Land Use	Acreage	Target No. of Dwellings	Density Range (du/ac)	Target Density* (du/ac)	Retail Square Footage
60	MFR Residential	4.9	78	14-28	15.9	-
61	Open Space/Recreation	1.4	-	-	-	-
62	SFR-2 Residential	26.6 24.4	169	5-8	6.49	-
63	MFR Residential	6.4	93	14-28	14.5	-
64	Open Space/Recreation	6.0	-	-	-	-
65	SFR-1 Residential	10.9	43	2-5	3.9	-
66	MFR Residential	5.7	91	14-28	16.0	-
67	Open Space/Recreation	6.3	-	-	-	-
68	SFR-2 Residential	7.6	55	5-8	7.2	-
69	K-8 School	14.0	-	2-14	-	-
70	SFR-3 Residential	8.4	101	8-14	12.0	-
71	SFR-1 Residential	12.6	49	2-5	3.9	-
72	SFR-1 Residential	35.7	100	2-5	2.8	-
73	SFR-3 Residential	3.8	46	8-14	12.1	-
74	Open Space/Joint-Use Park and School	12.0	-	2-14	-	-
75	Open Space/Recreation	4.3	-	-	-	-
76	SFR-3 Residential	8.4	84	8-14	10.0	-
77	SFR-3 Residential	7.9	111	8-14	14.0	-
78	Village Center Commercial	43.3	-	-	-	335,324
79	Open Space/Recreation	5.2	-	-	-	-
	Roadways	39.2	-	-	-	-
	Neighborhood III Total	968.8	3,329	-	-	566,279
Neighborhood IV						
16	Open Space	100.0	-	-	-	-
17	Open Space	9.0	-	-	-	-
18	Open Space	4.0	-	-	-	-
19	Open Space / Recreation	3.0	-	-	-	-
20	MFR Residential	54.0	869	14-28	16.1	-
21	Open Space	3.0	-	-	-	-
22	Open Space	44.0	-	-	-	-
23	Village Center Commercial	5.0	-	-	-	54,450
24	Open Space / Recreation	1.0	-	-	-	-
25	Village Center Commercial	6.0	-	-	-	60,899
26	Open Space / Recreation	17.0	-	-	-	-
27	Village Center Commercial	6.0	-	-	-	65,340
	Roadways	7.5	-	-	-	-
	Neighborhood IV Total	259.5	869	-	-	180,689
GRAND TOTALS		2,447.32, 030.1	8,4077.03 9	-	3.443.47	849,420

*Residential development within the SFR-1, SFR-2, SFR-3, MFR, and HDR land use categories within the Specific Plan area, as depicted in Figure 3-1, Conceptual Land Use Plan, and in Table 3-2 are target density ranges only. This Specific Plan requires that no more than 8,4077.039 dwelling units may be constructed in

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the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. Provided that the maximum dwelling unit cap, gross density per acre cap, and total number of projected AM + PM vehicle trips per day are not exceeded, no amendment to this Specific Plan shall be required to transfer dwelling units within or between residential planning areas or to exceed the target density.

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3.2.2 RESIDENTIAL DEVELOPMENT

There are five separate categories of residential development in Lytle Creek Ranch:

- Single-Family Residential One (SFR-1) at densities of 2 – 5 du/ac
- Single-Family Residential Two (SFR-2) at densities of 5 – 8 du/ac
- Single-Family Residential Three (SFR-3) at densities of 8 –14 du/ac
- Multi-Family Residential (MFR) at densities of 14 – 28 du/ac
- High Density Residential (HDR) at densities of 25 – 35 du/ac

SFR-1 and SFR-2 land use categories will contain only single-family detached residential development. SFR-3 will include a combination of single-family detached and attached residential product types. MFR will include only attached housing products consisting primarily of townhomes, condominiums, stacked flats, garden courts, apartments, and other higher density product types. It is anticipated that planning areas designated as “MFR” in Lytle Creek Ranch will, in most instances, be located adjacent to parks, greenbelts, or the Grand Paseo in Neighborhood III. Because of the substantial public and community open space provided within the Specific Plan area in proximity to the MFR planning areas, the actual amount of common open space within each individual MFR planning area will be reduced. The intent is to concentrate public open space in larger, more useable areas, rather than provide smaller areas of common open space that are not particularly useful for recreational use. HDR will include attached housing products consisting of townhomes, stacked flats, podium units, and apartments that tend to be located in proximity to key project area roadways.

These residential land uses categories are planned to integrate together to form cohesive neighborhoods. Specifically, Lytle Creek Ranch is organized into ~~four~~three residential neighborhoods defined by physical features, public amenities and target market. ~~Two~~Three of the neighborhoods will be built-out with traditional family-oriented housing (Neighborhoods ~~I~~, III, and IV), while the ~~fourth~~third neighborhood (Neighborhood II) will be built as a gated, age-qualified community for residents age 55 and older. Up to ~~4,325~~989 dwelling units are planned in the High Density Residential category. Approximately 95.6 acres of Village Center Commercial uses are planned on-site. The Village Center Commercial areas will develop with retail, office and/or medical/dental uses. One of the Village Center Commercial areas, located at the juncture of Sierra Avenue and Riverside Avenue (in Planning Area 33), is expected to build-out as a major retail shopping center with up to 230,955 square feet of retail uses.

The Specific Plan envisions variations in intensity within individual residential planning areas to promote diversity and create dynamic neighborhoods. ~~In addition to housing designed for families, couples, and singles, Lytle Creek Ranch will also provide a neighborhood (Neighborhood I) for move-up and luxury level buyers. Many of the single-family detached homes in Neighborhood I will be constructed on 6,000 square foot lots and larger. These areas will contain large, luxurious homes.~~

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Along with its other housing choices, Lytle Creek Ranch ~~will~~ could include an entire neighborhood devoted to Active Adult housing. Like most areas, the baby boomer segment of the San Bernardino County population is quickly approaching retirement age. A portion of Lytle Creek Ranch (Neighborhood II) ~~could~~ will build-out as a lifestyle community catered to households within the expanding Active Adult (age 55 and older) population. Active Adult communities allow residents of similar ages and interests a place to come together to enjoy an active lifestyle and sense of community. The age-qualified community ~~could be~~ is designed to accommodate housing without burdening active parks and local schools and certain other facilities and services. Planning Areas 82-84, 93-94, 98, 100, and 102-103 ~~could be potentially are anticipated to build~~ out with Active Adult housing. In addition, Planning Area 92 may develop with either traditional family-oriented housing or Active Adult housing at the discretion of the project master developer.

3.2.3 VILLAGE CENTER COMMERCIAL DEVELOPMENT

The project proposes approximately 95.6 acres for Village Center Commercial development. Potential uses for these areas could include retail, commercial, office, business park, and medical/dental uses and other uses pursuant to Table 5-1 in this Specific Plan. A total of 849,420 square feet of retail and commercial development is permitted within Village Center Commercial planning areas.

3.2.4 MAXIMUM ALLOWABLE DEVELOPMENT

The Lytle Creek Ranch Specific Plan permits a maximum of ~~8,407,039~~ dwelling units to be constructed within the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. If ~~8,407,039~~ dwelling units are constructed, the maximum amount of Village Center Commercial development permitted within the Specific Plan area shall not exceed 849,420 square feet of retail uses. It should be noted that the Lytle Creek Ranch Specific Plan permits the Village Center Commercial development cap to exceed 849,420 square feet of retail uses; provided, however, that a corresponding decrease occurs in the permitted dwelling units such that the total number of AM + PM vehicle trips per day for Neighborhoods ~~I and IV combined~~ shall not exceed 3,853 projected AM + PM trips (combined total), and provided that the total number of total number of AM + PM vehicle trips per day for Neighborhoods II and III combined shall not exceed 12,483 projected AM + PM trips (combined total). Use of the maximum daily vehicle trips as a cap on overall development will provide flexibility in determining the appropriate mix of commercial, light industrial, and residential uses in the Specific Plan area as it builds out.

3.2.5 TRANSFER OF DEVELOPMENT BETWEEN PLANNING AREAS

Residential development within the SFR-1, SFR-2, SFR-3, MFR, and HDR land use categories within the Specific Plan area, as depicted in Figure 3-1, Conceptual Land Use Plan, and in Table 3-

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2 are target density ranges only. This Specific Plan requires that no more than ~~8,407,039~~ dwelling units may be constructed in the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. No amendment to this Specific Plan shall be required to transfer dwelling units within or between designated residential planning areas or to any SFR Overlay and/or HDR Overlay planning area(s) or to exceed the target density provided that the development transfer conditions below are met.

Development Transfer Conditions:

- A. Residential planning area land use categories (i.e., SFR-1, SFR-2, SFR-3, MFR, HDR) within the Lytle Creek Ranch Specific Plan area, as depicted in Figure 3-1, Conceptual Land Use Plan, are target density ranges only. The Specific Plan requires that no more than ~~8,407,039~~ dwelling units may be constructed in the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. Gross density shall be calculated by including all Specific Plan area acreage including open space land use categories and roadway acreages. As long as the maximum dwelling unit cap and the gross density per acre cap are not exceeded, no amendment to the Specific Plan will be required to transfer dwelling units from one residential planning area to another. The project master developer shall have the right to increase or decrease dwelling unit counts in any residential planning area; provided that prior to the time such as a transfer is made, the project master developer submits to the City's Planning Division Figure 3-1 and Table 3-2 of the Specific Plan, as revised, for administrative approval. No tentative map or parcel map may be approved unless it is consistent with the adopted Specific Plan, as revised. Land designated as residential shall continue to be entitled for residential development after the density transfer.
- B. The total number of projected AM + PM vehicle trips per day for all uses in Neighborhoods ~~I and IV combined~~ shall not exceed ~~3,853~~1,818 projected AM + PM trips (~~combined~~-total). In addition, the following conditions shall be met:
1. Dwelling units may be transferred between any and all residential planning areas in Neighborhoods ~~I and IV~~; and
 2. Dwelling units may be transferred between any and all residential planning areas and any planning area with a SFR Overlay and/or HDR Overlay in Neighborhoods ~~I and IV~~.
- C. The total number of projected AM + PM vehicle trips per day for all uses in Neighborhoods II and III combined shall not exceed 12,483 projected AM + PM trips (combined total). In addition, the following conditions shall be met:
1. Dwelling units may be transferred between any and all residential planning areas in Neighborhoods II and III; and
 2. Dwelling units may be transferred between any and all residential planning areas and any planning area with a SFR Overlay and/or HDR Overlay in Neighborhoods II and III.

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- D. Grading and landform alteration would substantially comply with that previously approved for the Specific Plan.
- E. No new significant environmental impacts that were not previously assessed in the Lytle Creek Ranch EIR would result from the transfer.

Transfer of dwelling units between residential planning areas, in accordance to the provisions of this Specific Plan, shall not constitute or require a Specific Plan Amendment. However, prior to City acceptance of a dwelling unit or intensity transfer, supporting documentation, which verifies that the maximum number of permitted AM+PM vehicle trips per day are not exceeded, shall be prepared by a qualified traffic engineer and submitted to the City for review and approval by the City's Traffic Engineer and Planning Division.)

The following tables (Table 3-3 and 3-4) shall be completed and submitted to the City's Planning Division in conjunction with either a Precise Plan of Design or a Tentative Tract Map for review by the division prior to transfer of development of any dwelling units within the boundaries of the Lytle Creek Ranch Specific Plan: The transfer of density shall be permitted if the above conditions (3.2.5.A through E, above) are met.

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TABLE 3-3
DEVELOPMENT DENSITIES & INTENSITIES – BEFORE & AFTER TRANSFERS

Planning Area	Land Use	Acreage	Target No. of Dwellings (Before Transfer)	Adjusted No. of Dwellings (After Transfer)
Neighborhood I				
1	Open Space	29.0	-	
2	Open Space	14.0	-	
3	SFR-1 Residential	46.0	129	
4	High Density Residential	12.0	336	
5	Open Space	40.0	-	
6	Open Space	43.0	-	
7	Open Space	44.0	-	
8	SFR-1 Residential	85.0	347	
9	Open Space	1.0	-	
10	Open Space/Recreation	11.0	-	
11	SFR-3 Residential	3.0	24	
12	Open Space	3.0	-	
13	SFR-3 Residential	29.0	370	
14	Open Space	2.0	-	
15	SFR-3 Residential	9.0	72	
	Roadways	46.2	-	
-	Neighborhood I Total	417.2	1,278	
Neighborhood II				
80	Open Space	168.0	0	
81	Open Space/Recreation	5.0	0	
82	SFR-3 Residential	30.0	336	
83	SFR-2 Residential	107.0	692	
84	SFR-3 Residential	23.0	249	
85	Open Space/Recreation	1.0	0	
86	Open Space/Recreation	3.0	0	
87	Open Space/Recreation	45.0	0	
88	Open Space/Recreation	5.0	0	
89	Village Center Commercial	6.0	0	
90	Village Center Commercial	2.0	0	
91	Village Center Commercial	3.2	0	
92	High Density Residential	13.0	364	
93	SFR-3 Residential	54.0	551	
94	SFR-2 Residential	30.0	177	
95	Open Space/Recreation	67.0	0	
96	Open Space	8.0	0	
97	Open Space/Recreation	5.0	0	
98	SFR-2 Residential	54.0	316	
99	Open Space/Recreation	60.0	0	
100	SFR-3 Residential	14.0	126	

LYTLE CREEK RANCH
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Planning Area	Land Use	Acreage	Target No. of Dwellings (Before Transfer)	Adjusted No. of Dwellings (After Transfer)
101	Open Space/Recreation	35.0	0	
102	SFR-2 Residential	11.0	80	
103	SFR-1 Residential	11.0	40	
	Roadways	41.6	0	
Neighborhood II Total		801.8	2,931	

Neighborhood III				
28	Open Space	396.0	0	-
29	Open Space/Recreation	5.0	0	-
30	High Density Residential	4.5	126	
31	High Density Residential	15.9	499	
32	Open Space/Recreation	9.8	0	
33	Village Center Commercial	24.1	0	
34	Open Space/Recreation	7.3	0	
35	SFR-3 Residential	11.5	115	
36	SFR-2 Residential	3.8	28	
37	Open Space/Recreation	2.6	0	
38	SFR-2 Residential	10.6	77	
39	MFR Residential	5.8	196	
40	Open Space/Recreation	6.0	0	
41	MFR Residential	4.8	96	
42	SFR-2 Residential	13.0	66	
43	SFR-1 Residential	22.0	62	
44	MFR Residential	4.5	72	
45	Open Space/Recreation	3.1	0	
46	SFR-2 Residential	9.9	75	
47	SFR-3 Residential	5.4	54	
48	Open Space/Joint-Use Park and School	5.0	0	
49	Elementary School	10.0	0	
50	MFR Residential	4.8	96	
51	Open Space/Recreation	1.0	0	
52	MFR Residential	9.4	141	
53	Open Space/Recreation	8.0	0	
54	SFR-2 Residential	13.1	75	
55	MFR Residential	6.0	96	
56	Open Space/Recreation	4.8	0	
57	SFR-2 Residential	17.9	98	
58	SFR-3 Residential	12.6	164	
59	SFR-1 Residential	40.0	173	
60	MFR Residential	4.9	78	
61	Open Space/Recreation	1.4	0	
62	SFR-2 Residential	26.6	169	
63	MFR Residential	6.4	93	
64	Open Space/Recreation	6.0	0	

LYTLE CREEK RANCH
 SPECIFIC PLAN

Planning Area	Land Use	Acreage	Target No. of Dwellings (Before Transfer)	Adjusted No. of Dwellings (After Transfer)
65	SFR-1 Residential	10.9	43	
66	MFR Residential	5.7	91	
67	Open Space/Recreation	6.3	0	
68	SFR-2 Residential	7.6	55	
69	K-8 School	14.0	0	
70	SFR-3 Residential	8.4	101	
71	SFR-1 Residential	12.6	49	
72	SFR-1 Residential	35.7	100	
73	SFR-3 Residential	3.8	46	
74	Open Space/Joint-Use Park and School	12.0	0	
75	Open Space/Recreation	4.3	0	
76	SFR-3 Residential	8.4	84	
77	SFR-3 Residential	7.9	111	
78	Commercial	43.3	0	
79	Open Space/Recreation	5.2	0	
	Roadways	39.2	0	
Neighborhood III Total		968.8	3,329	
Neighborhood IV				
16	Open Space	100.0	0	
17	Open Space	9.0	0	
18	Open Space	4.0	0	
19	Open Space / Recreation	3.0	0	
20	MFR Residential	54.0	869	
21	Open Space	3.0	0	
22	Open Space	44.0	0	
23	Village Center Commercial	5.0	0	
24	Open Space / Recreation	1.0	0	
25	Village Center Commercial	6.0	0	
26	Open Space / Recreation	17.0	0	
27	Village Center Commercial	6.0	0	
	Roadways	7.5	0	
Neighborhood IV Total		259.5	869	
GRAND TOTALS		2,447.32 030.1	8,407.03 9	

**TABLE 3-4
MAXIMUM DAILY TRIP CALCULATIONS BY NEIGHBORHOOD**

Neighborhood	Original Allocated AM + PM Trips	Revised Allocated AM + PM Trips	Difference Between Original & Allocated Trips
Neighborhood I Estimated AM + PM Trips	2,035		
Neighborhood IV Estimated AM + PM Trips	1,818		
Number of Total Trips for Neighborhoods I and IV (Combined)	3,853 1,818 (Maximum Total)		
Neighborhood II Estimated AM + PM Trips	4115		
Neighborhood III Estimated AM + PM Trips	8368		
Number of Total Trips for Neighborhoods II and III (Combined)	12,483 (Maximum Total)		
Maximum Number of Total Trips (All Neighborhoods Combined)	16,336 14,301 (Maximum Total)		

Note: In Table 3-4, Revised Allocated AM + PM Trips for Neighborhoods I and IV (combined) may be less than or equal to, but cannot exceed, ~~3,853~~**1,818** AM + PM Trips. Likewise, Revised Allocated AM + PM Trips for Neighborhoods II and III (combined) may be less than or equal to, but cannot exceed, 12,483 AM + PM Trips.

3.2.6 GATED VS. NON-GATED DEVELOPMENT

Lytle Creek Ranch is designed to appeal to a broad spectrum of users and buyers. Therefore, it is anticipated that some portions of the project may be gated, while other portions will not be gated. ~~Most, if not all, Some~~ of the planning areas within Neighborhood II (~~i.e., the Active Adult Community~~) will be enclosed by gates, although selected planning areas may not be gated at the discretion of the master developer/builder. In addition, some planning areas in Neighborhoods ~~I, II, and IV~~ may also be gated. It shall be the decision of the master developer to determine which planning areas shall be gated.

3.2.7 SCHOOLS

The Specific Plan area is located within three different school districts. Students in the north and northeastern portions of the Specific Plan will attend existing schools in the San Bernardino City Unified School District (SBCUSD). It is anticipated that existing schools in the SBCUSD will have sufficient capacity to serve the new students generated by this area of the Specific Plan. ~~This project will involve adjustments between the school district boundaries of the Rialto Unified School~~

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~~District / San Bernardino County Unified School District in Neighborhood I in order to avoid splitting up individual residential enclaves into two different school districts.~~

Students in the northwestern portion of the Specific Plan (Neighborhood IV) will attend existing schools in the Fontana Unified School District (FUSD), as it is anticipated that existing schools will have sufficient capacity to serve the new students generated by this area of the Specific Plan. The FUSD will make the final determination as to which schools the Lytle Creek Ranch students located within the FUSD boundaries will attend.

Students in the southern portion (Neighborhood III) of the Specific Plan will attend schools in the Rialto Unified School District (RUSD). An elementary school is planned on 10 acres located directly adjacent to a 5-acre joint-use park in Neighborhood III. Additionally, a 14-acre elementary/middle school is planned adjacent to a 12-acre joint-use park site in Neighborhood III. It is anticipated that high school students will attend Carter High School or other high school in the RUSD.

With the possible exception of Planning Area 92 in Neighborhood II, no students will be generated by Neighborhood II, as it is planned as an age-qualified community for adults 55 years and older. Should Planning Area 92 develop with family-oriented housing, then any students generated by Planning Area 92 development would attend schools in the RUSD.

If the RUSD elects not to utilize one or both of the designated school sites, then the unused site(s) may develop with Single-Family Residential One (SFR-1), Two (SFR-2), or Three (SFR-3) development. In such instance, unused dwelling units from elsewhere within Lytle Creek Ranch Neighborhood II and III may be transferred to the unneeded school site(s). The maximum number of dwelling units within Lytle Creek Ranch shall not exceed ~~8,407~~8,039 dwelling units.

The project will pay its fair share of fees to each school district as required by California state law and/or the project master developer shall enter into a mitigation agreement with the school district.

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3.2.8 OPEN SPACE

Of the ~~2,447.32,030.1~~ acres located within the Specific Plan boundaries, a minimum of ~~829.2769~~ acres will be preserved as open space by clustering development along Riverside Avenue, Lytle Creek Road/Sierra Avenue, Glen Helen Parkway, ~~Clearwater Parkway~~, and the I-15 corridor. Lytle Creek Wash bisects a portion of the project site. While the actual acreage included with this natural open space area may be adjusted to a limited degree as a result of future reconfiguring/refinement of neighborhoods, in no event shall the total acreage of natural open space be less than ~~829.2769~~ acres for Lytle Creek Ranch. To protect the flora and fauna in these natural open space areas, no trails will be constructed within these areas (except for trails that may be necessary for habitat restoration and species conservation efforts) and public access shall not be allowed. The areas to be included as undisturbed open space in a natural condition for habitat and wildlife potential are:

- A total of 612.5 acres of additional natural open space, including 444.8 acres within and immediately adjacent to Lytle Creek Wash ~~and 167.7 acres within Neighborhood I (adjacent to the San Bernardino National Forest and Glen Helen Regional Park)~~, to be preserved in perpetuity as part of the Lytle Creek Ranch project.
- Approximately 160.5 acres of land in Lytle Creek Wash that has been set aside for San Bernardino kangaroo rat (SBKR) conservation as part of the adjacent Lytle Creek North Biological Opinion/404 permit.
- An additional 52.2 acres for SBKR mitigation in Lytle Creek Wash immediately adjacent to these 160.5 acres and set aside by Lytle Development Company in conjunction with the Lytle Creek North project.
- Four acres of SBKR habitat in Lytle Creek Wash, which were purchased by the West Valley Water District, and set aside as an expansion of the SBKR conservation area previously mentioned.

~~The areas along and within Lytle Creek Wash and portions of the hillsides in Neighborhood I will be preserved as open space in its natural state. To protect the flora and fauna, no trails will be constructed within these areas and public access will not be allowed.~~

3.2.9 OPEN SPACE/RECREATION

Up to ~~344.7282.5~~ acres will be devoted to Open Space/Recreation (OS/R) uses. Areas designated as OS/R will open space, neighborhood parks, golf, and recreation areas. The project proposes an extensive system of green spaces, such as neighborhood parks, and recreation areas linked together by a network of trails, parkways, and paseos.

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3.2.10 LAND USE PLAN GENERAL DEVELOPMENT STANDARDS

The Lytle Creek Specific Plan contains a combination of residential, Village Center Commercial, open space, school, park, and recreation uses. The specific land uses described will require infrastructure, public services and facilities, and special techniques or mitigations in each neighborhood to accommodate the proposed uses and provide adequate transportation to neighboring uses.

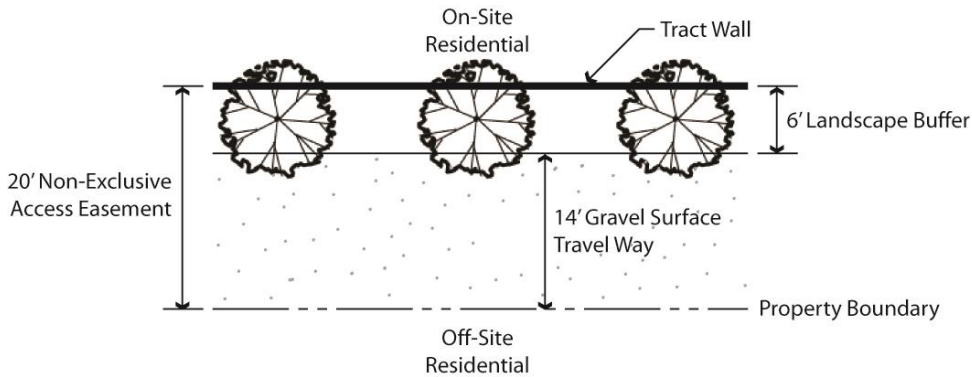
Project-wide development standards have been prepared to manage implementation of general or unique conditions in each area. General standards are listed below. For development standards by land use category, see Chapter 5.0, Development Standards.

- A. The total Specific Plan shall be developed to a maximum of ~~8,4077,039~~ dwelling units. No more than ~~8,4077,039~~ dwelling units are permitted. Dwelling units may be either detached or attached at the discretion of the master developer and individual builder(s).
- B. The overall project density (gross density) of Lytle Creek Ranch shall not exceeded 3.5 dwelling units per acre. Gross density shall be calculated by including all Specific Plan area acreages including open space land use categories and roadway acreages.
- C. Residential development within the SFR-1, SFR-2, SFR-3, MFR, and HDR land use categories within the Lytle Creek Ranch Specific Plan area, as depicted in Figure 3-1, Conceptual Land Use Plan, are target density ranges only. The Specific Plan requires that no more than ~~8,4077,039~~ dwelling units may be constructed in the Specific Plan area and requires that the overall project-wide gross density shall not exceed 3.5 dwelling units per acre. The project master developer shall have the right to increase or decrease dwelling unit counts in any residential planning area; provided that at such time a transfer is made, the project master developer submits to the City's Planning Division Figure 3-1 and Table 3-2 of the Specific Plan, as revised.
- D. The Specific Plan shall contain a minimum of ~~829,2769~~ acres of natural open space, and approximately ~~328-8282.5~~ acres of open space, neighborhood parks, ~~golf~~, and recreation areas, as illustrated on Figure 3-1, Conceptual Land Use Plan.
- E. Uses and development standards shall be in accordance with the zoning regulations and planning area development standards established for this Specific Plan (see Chapter 5.0), and shall be defined by Specific Plan objectives, future detailed tract maps, development plans, and potential conditional use permits as appropriate.
- F. The development of the property shall be in accordance with the mandatory requirements of all City of Rialto and state laws, and shall conform to the approved Specific Plan as filed in the office of the City of Rialto Development Services Department, unless otherwise amended.

LYTLE CREEK RANCH
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- G. Any development standard not addressed in the Specific Plan shall be subject to the City of Rialto Municipal Code.
- H. All tentative maps prepared for any portion of Lytle Creek Ranch must be consistent with this Specific Plan.
- I. Design features, such as special architectural treatments, perimeter and interior landscaping, and buffering of parking lot/loading zone areas shall be incorporated if needed into the project design to minimize any potential conflict between uses on-site and any existing abutting residential enclaves.
- J. Where feasible and appropriate, this Specific Plan encourages the provision of bicycle and pedestrian facilities that extend through and link the residential neighborhoods and commercial areas, and accessible recreational facilities within Lytle Creek Ranch with one another.
- K. Shared parking of uses may be allowed in all planning areas designated for Village Center Commercial development, subject to preparation of a shared parking analysis. CEQA review may be required. The analysis shall be reviewed and approved by both the Director of Development Services and the City Traffic Engineer.
- L. Tiered vegetative landscaping shall be installed between the I-15 Freeway or Cemex USA and any residential unit located within 500 feet of I-15 Freeway or Cemex USA. Studies have shown that vegetative landscaping can reduce particulate emissions by up to 65 to 85 percent, with greater removal rates expected for ultra-fine particles < 0.1 µm in diameter.
- M. The Applicant shall offer to dedicate a 20-foot wide non-exclusive access easement to the County Improvement District to be identified by the County for the benefit of the residences that currently abut the existing access road adjacent to Planning Area 93, depicted in Figure 3-1 of the Lytle Creek Ranch Specific Plan. The access easement shall be improved by the developer to provide a 14-foot wide gravel surfaced travel way adjacent to the abutting homeowners with a 6-foot wide landscape area for a tree buffer adjacent to the tract wall along Planning Area 93. The dedication of the access will have a reservation for storm drain facilities and public utilities in favor of the developer. The offer of dedication shall occur prior to recordation of a Final Map that includes any portion of Planning Area 93, and ONLY after the Lytle Creek Ranch Specific Plan providing for 8,407,039 residential dwelling units and 849,420 square feet of nonresidential development has been adopted and established as zoning for the property, and any legal challenges to its adoption resolved. The offer of dedication shall be accepted within 30 days after completion of the trail and landscape improvements by the developer.

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3.3 PARKS AND RECREATION PLAN

The project proposes approximately ~~328.8282.5~~ acres of open space/recreation areas, as depicted in Figure 3-2, Parks and Recreation Plan. Another 35.7 acres of land in Planning Area 72 is planned as a park overlay. A detailed discussion of uses is provided below. The recreation/open space area consist of a mix of recreation types including, but not limited to, ~~a public 18-hole golf course~~, neighborhood parks, a private ~~Active Adult~~ Community Center, an extensive central linear paseo (i.e., the “Grand Paseo”), and the three private recreational centers in Neighborhood III.

The Parks and Recreation Plan provides for a vast array of recreational opportunities for Lytle Creek Ranch residents. Many of these facilities will also be available for use by all residents of Rialto. The program incorporates many diverse elements in a coordinated, cohesive plan that interrelates with and links the various neighborhoods of the community with each other and to certain destination points, such as the neighborhood parks, joint-use parks/schools, and the two planned schools. The recreational opportunities on-site will vary from active uses in the joint-use parks/schools, to passive uses in the neighborhood parks and the “Grand Paseo.” Varying types of activities will be available that will provide residents with opportunities to: 1) enjoy walks in the parks, 2) participate in community meetings and social gatherings, 3) participate in active outdoor informal recreational activities, and 4) participate in potentially informal and organized sporting events.

The areas designated as “Open Space/Recreation” will be owned and maintained by the Master Homeowners Association or by another entity approved by the City of Rialto’s Planning Division. The park land will be phased to come on-line in accordance with the demand created by the construction of the housing units on-site.

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Figure 3-2
Parks and Recreation Plan

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LYTLE CREEK RANCH
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3.3.1 COMPREHENSIVE PASEO AND TRAIL SYSTEM

Many of the areas designated as open space/recreation within Lytle Creek Ranch will contain pedestrian walkways, sidewalks, and multi-purpose trails. Figure 3-3, Trail Plan, is a comprehensive trail system planned throughout Lytle Creek Ranch. This system includes a paved multi-purpose trail that runs adjacent to Lytle Creek in Neighborhoods II, III, and IV. A multi-purpose trail also runs through the length of central linear paseo (i.e., the “Grand Paseo”) in Neighborhood III. The multi-purpose trail in the Grand Paseo will be a minimum of eight feet in width and will consist of an all-weather surface that accommodate both pedestrian and bicycle traffic. The trails adjacent to Lytle Creek will be a minimum of 20 feet in width, but will not provide public access into the wash. This paved trail will provide service access adjacent to the creek, as well as a route for walking and biking. A wrought iron or tubular steel fence will be provided between the creek and the trail to limit access by pets and humans into the wash.

The Grand Paseo will weave through most of Neighborhood III, linking together the three neighborhood parks. This paseo will vary in width from a minimum of 70 feet up to approximately 110 feet. The paseo will be with a mix of native and non-native species organized around a landscaped drainage corridor. The purpose of this drainage corridor is to accommodate storm water flows. Portions of the Grand Paseo will be designed to function as a large bioswale that will naturally filter out chemicals and other potential pollutants as the water flows through the system.

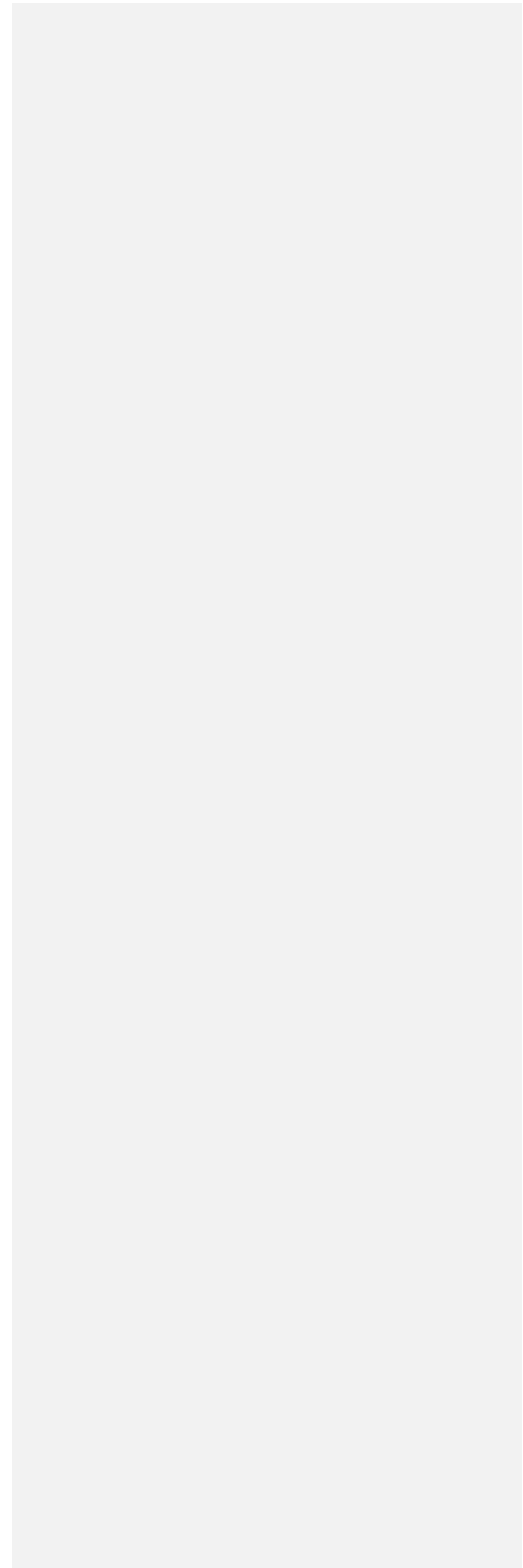
In addition to the on-site paseo system, a landscape parkway up to 24 feet in width will be provided along Riverside Avenue in Neighborhood III. This landscape parkway will contain a five foot wide walkway. In addition, a network of sidewalks will be provided in Neighborhood I that will connect to the existing trail and walkway system planned in the adjacent Rosena Ranch community.

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Figure 3-3
Trail Plan



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3.3.2 NEIGHBORHOOD PARKS

There will be three neighborhood parks in Neighborhood III, including two 6-acre parks (Planning Areas 40 and 64) and an 8-acre neighborhood park (Planning Area 53). All three will be designed to accommodate up to private recreation facilities (ranging from approximately two to five acres in size). ~~In addition, Planning Area 10 in Neighborhood I will consist of an 11-acre neighborhood park, which connects to additional park acreage in the adjacent Rosena Ranch community.~~ The neighborhood parks may accommodate a mix of uses including, but not limited to, private recreation centers, picnicking areas, playgrounds, shade structures, gardens, sitting areas, informal turf play areas, and attractive landscaping. All neighborhood parks within Neighborhood III shall be public parks; provided, however, that portions of Planning Areas 40, 53, and 64 will develop with private recreation centers ranging in size from approximately two to four acres.

Neighborhood parks in Neighborhood III of Lytle Creek Ranch may be constructed in any order pursuant to the discretion of the project master developer and as set forth in the approved Lytle Creek Ranch Development Agreement.

3.3.3 SPORTS PARK (PLANNING AREA 72)

Planning Area 72, which contains approximately 35.7 acres of land, is designated on the Land Use Plan with a Park Overlay. This land may develop with a mixture of neighborhood and community park elements. If developed as a park, the project master developer and the City of Rialto will mutually agree on the improvements and athletic fields to be provided in the sports park. A proposed conceptual illustration of the sports park is depicted in Figure 3-4, Sports Park Concept.

3.3.4 JOINT-USE PARKS

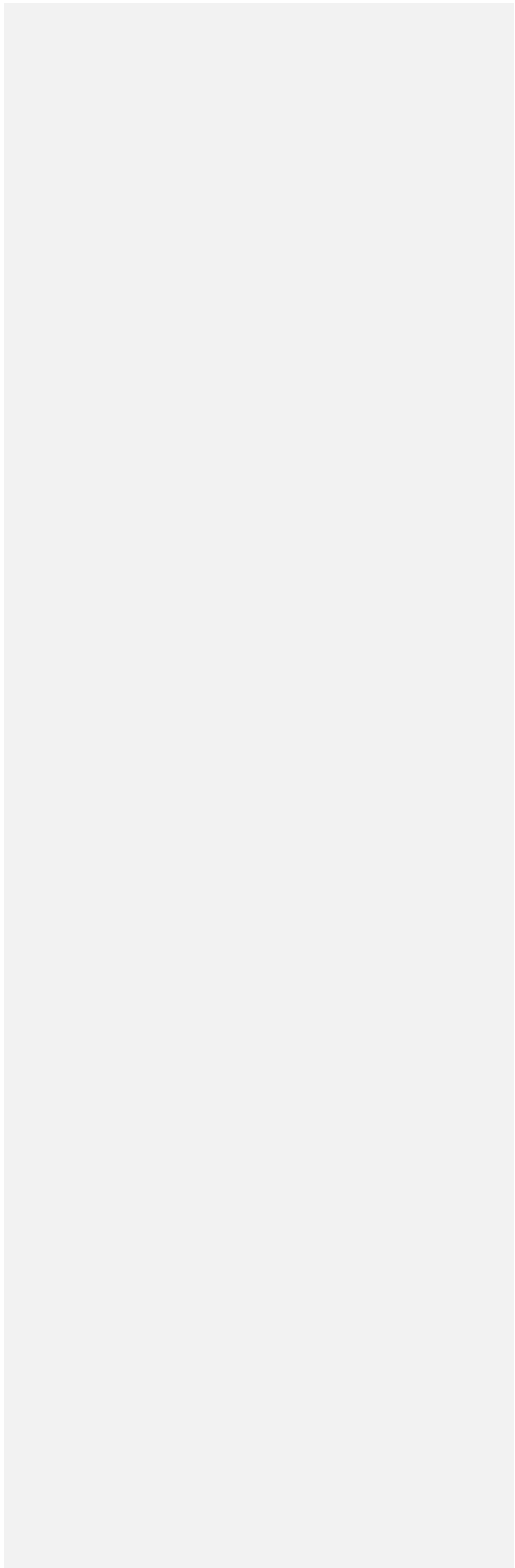
Two areas within Neighborhood III are proposed as possible joint-use school/park facilities (i.e., Planning Areas 48 and 74) if agreeable to both the City and the Rialto Unified School District. These parks are designed to function as potential joint-use facilities with the adjacent elementary school (Planning Area 49) and K-8 school (Planning Area 69). These joint-use school/park facilities are anticipated to contain athletic fields, playgrounds, and informal play areas, which will be available for use by the school, and also by the general public when the school is not using the facilities. The actual sizes of and uses contained within these joint-use parks may vary depending on the needs of the Rialto Unified School District (RUSD). The master developer reserves the right to develop all or portions of the joint-use park sites with Single Family Density Residential 1, 2, and 3 (SFD-1, 2, 3) uses, should the RUSD elect to reduce the amount of land required for the joint-use park, or should the RUSD elect not to construct the adjacent school. The maximum permitted residential density in Planning Areas 48 and 74, should one or both joint-use parks not be provided, shall not exceed 14 du/ac.

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Figure 3-4
Sports Park Concept



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3.3.5 PRIVATE RECREATION CENTERS IN NEIGHBORHOOD III

Private recreation centers will be constructed in Neighborhood III. These recreation centers will be constructed on approximately two to five acres in Planning Areas 40, 53, and 64. The recreation centers in Neighborhood III will be gated and available for the private use of residents in Neighborhood III and their guests. Residents of Neighborhood II will be served by their own ~~Active Adult~~ recreation center. The Neighborhood III recreation center will be well designed and landscaped, and will serve as a community focal and gathering point. The largest of the recreation centers may contain a central clubhouse facility with a meeting room, exercise rooms, locker rooms, restrooms, and other amenities. Additional amenities could include a swimming pool with a spa, as well as BBQs and outdoor areas for picnics and special events. Please refer to Figures 3-5 and 3-6 for conceptual recreation center plans.

3.3.6 ACTIVE ADULT RECREATION CENTER

A minimum of one private recreation center is planned within Neighborhood II (if Active Adult is built). The Neighborhood II Active Adult Recreation Center will be limited to use by Active Adult residents and their guests only. ~~The recreation center will be constructed in Planning Area 86, adjacent to the public 18-hole golf course.~~ The facility will be well-designed and landscaped, and will serve as a neighborhood focal and gathering point. The recreation center will contain a central clubhouse facility that may contain such amenities as meeting and game/craft rooms, exercise facilities, locker rooms, restrooms, and other facilities. There will also be a swimming pool with a spa, and an outdoor area with BBQs for picnics and special events. ~~The Active Adult recreation center will be a minimum of three acres in size.~~

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Figure 3-5
Conceptual Recreation Center (Planning Area 53)

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SPECIFIC PLAN

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LYTLE CREEK RANCH
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Figure 3-6
Conceptual Recreation Center (Planning Areas 40 and 64)

LYTLE CREEK RANCH
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3.3.7 GOLF COURSE AND CLUBHOUSE

~~Within Planning Area's 87, 95, 99, and 101, the project is planned to include a renovated and reconfigured 18-hole public golf course. The existing golf course (i.e., the El Rancho Verde Royal Vista Golf Club) will close down for a period of approximately 18 to 24 months to be reconfigured. Existing homes that abut an existing fairway/green shall continue to abut a golf course fairway/green after the reconfiguration. The existing fairways and greens shall be reconfigured and improved with enhanced landscaping. The golf course rerouting, grading, and redesign shall be reviewed and approved by both the City of Rialto in conjunction with an application for a Precise Plan of Design.~~

~~The golf course will include a new 19,000-square-foot minimum clubhouse facility with pro shop, men's and women's locker rooms, offices, bar, restaurant, kitchen, and banquet facilities. Other features will include a tournament lawn, driving range, and storage barn for golf carts.~~

~~Although much of the golf course will be surrounded by a gated active adult housing community, the golf course and clubhouse will be open for use by the general public. The architectural style of the clubhouse may be any one of the following styles: Spanish Eclectic, Italianate, Tuscan, or Craftsman/California Bungalow. Ultra-modern or contemporary designs for the clubhouse shall not be permitted. The clubhouse must be built and operational within three (3) years of the reopening of the reconfigured golf course. See Figure 3-7a, Conceptual Golf Course Plan, and Figure 3-8, Conceptual Golf Clubhouse Building Elevations.~~

As an alternative to the golf course, the OS/R zoning designation of the Specific Plan allows the developer of Planning Areas (87, 95, 99 and 101) to develop these Planning Areas for other open space uses as permitted under Section 5.6.10 of this Specific Plan, including but not limited to community facilities, recreation centers and buildings, health clubs, public parks and recreation areas, sports parks, swimming pools, and other outdoor athletic facilities and similar recreational uses. In addition, this zone allows for low intensity, passive recreational uses such as trails, picnic areas, bicycle paths, and sitting areas.

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Figure 3-7a (page 3-41a)
Conceptual Golf Course Plan

Figure 3-7b (page 3-41b)
Conceptual Open Space/Recreation Alternative

NOTE: INSERT BOTH FIGURE 3-7a (PAGE 3-41a) AND FIGURE 3-7b (PAGE 3-41b) HERE.

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Figure 3-8
Conceptual Golf Clubhouse Building Elevations

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3.3.8 PARKS AND RECREATION PLAN DEVELOPMENT STANDARDS

- A. A total of three public neighborhood parks are planned in Neighborhood III (i.e., Planning Areas 40, 53, and 64) will be provided for the benefit of residents of the community as well as the general public. These parks may contain such facilities as gardens and community gardens, plazas, tot lots and playgrounds, basketball courts, open turf areas, BBQ areas and shade structures, picnic tables, benches, drinking fountains, seat walls, night lighting, walkways, multi-purpose trails, parking lots, and other similar amenities. Private recreation centers shall be permitted in all neighborhood parks.
- B. The planning areas designated as "Open Space/Joint-Use" shall be designed to include playgrounds and/or sports fields. These facilities will be used primarily by the Rialto Unified School District. However, when school is not in session, these facilities may be available for use by the general public.
- C. Mini parks are allowed within Lytle Creek. Such parks are optional and may be provided at the discretion of the project master developer or builder(s). If provided, mini parks will typically be less than one acre in size. Mini parks may be located in Neighborhoods II, III, and IV. Because of the extensive amenities already provided in Neighborhood II, it is not anticipated that any mini parks will be located within that neighborhood. These optional mini parks may contain open play turf areas, tot lots, shade structures, benches, ornamental gardens, and other passive amenities. The uses proposed or identified in these private parks shall be conducive to the residential neighborhoods that they serve. Active uses and restroom facilities are not appropriate facilities in mini parks. The exact locations, sizes, and configurations of these mini parks shall be determined in conjunction with an application for a Tentative Tract Map and Precise Plan of Design. All mini parks will be privately owned and maintained by a homeowners association or other entity acceptable to the City of Rialto.
- D. All recreational/open space areas shall be landscaped and contain permanent irrigation systems.
- E. All recreational facilities shall provide parking in accordance to the City of Rialto standards.
- F. Recreation acreage calculations for residential development within Lytle Creek Ranch shall be based upon a minimum of three acres per 1,000 residents.
- G. The design of the public parks will require review and approval by City staff.

LYTLE CREEK RANCH

SPECIFIC PLAN

3.4 OPEN SPACE AND CONSERVATION PLAN

The Lytle Creek Ranch Specific Plan design is sensitive to the existing resources on-site, including the Lytle Creek Wash, habitat and species associated with it, and other sensitive flora and fauna currently found within the site.

Of the ~~2,447.32,030.1~~ acres located within the Specific Plan boundaries, a minimum of ~~829.2769~~ acres of the property, including Lytle Creek Wash, will be preserved as open space by clustering development along Riverside Avenue, Lytle Creek Road/Sierra Avenue, Glen Helen Parkway, ~~Clearwater Parkway~~, and the I-15 corridor (see Figure 3-9, Open Space and Conservation Plan). While the actual acreage included within this natural open space may be adjusted to a limited degree as a result of future reconfiguring/refinement of neighborhoods, in no event shall the total acreage of natural open space be less than ~~829.2769~~ acres for Lytle Creek Ranch. To protect the flora and fauna in these natural open space areas, no trails will be constructed within these areas (except for trails that may be necessary for habitat restoration and species conservation efforts) and public access shall not be allowed. The areas to be included as undisturbed open space in this Specific Plan are:

- A total of 612.5 acres of additional natural open space, including 444.8 acres within and immediately adjacent to Lytle Creek Wash ~~and 167.7 acres within Neighborhood I (adjacent to the San Bernardino National Forest and Glen Helen Regional Park)~~, to be preserved in perpetuity as part of the Lytle Creek Ranch project.
- Approximately 160.5 acres of land in Lytle Creek Wash that has been set aside for San Bernardino kangaroo rat (SBKR) conservation as part of the adjacent Lytle Creek North Biological Opinion/404 permit;
- An additional 52.2 acres for SBKR mitigation in Lytle Creek Wash immediately adjacent to these 160.5 acres and set aside by Lytle Development Company in conjunction with the Lytle Creek North project; and
- Four acres of SBKR habitat in Lytle Creek Wash, which was purchased by the West Valley Water District, and set aside as an expansion of the SBKR conservation area previously mentioned.

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3.4.1 AVOIDANCE AND LONG-TERM PRESERVATION

A minimum of ~~829-2769~~ acres of open space within the project area will be set-aside in perpetuity for avoidance and long-term preservation of habitat and species and as natural open space, including land within Lytle Creek Wash. This natural open space supports several sensitive plant and wildlife species, including San Bernardino kangaroo rat (SBKR), Los Angeles pocket mouse, northwestern San Diego pocket mouse, and Plummer's mariposa lily. A large population of Parry's spineflower plants, estimated at more than 120,000 individuals, will be preserved within the conservation area. A total of 444.8 acres of land are planned as permanent natural open space ~~(not including Neighborhood I open space)~~, which are immediately contiguous with an existing 216.7 acres of SBKR habitat, which Lytle Development Company established largely in conjunction with the Lytle Creek North Planned Development project.

Conservation of the SBKR and an ecologically viable community of Riversidean Alluvial Fan Sage Scrub (RAFSS) is a hallmark of the Lytle Creek Ranch project. The proposed project has been designed to contribute to the conservation and recovery of the SBKR, and to assist with the conservation and recovery of other sensitive species, which could utilize portions of the preservation area in the future. Selection of natural open space areas to be set aside by the project have taken into consideration the areas designated as critical habitat by the U.S. Fish and Wildlife Service (USFWS) for the SBKR and the California gnatcatcher, so as to both contribute to the long-term conservation of these species and to allow these units of critical habitat to continue to provide their identified conservation benefits to the species after project build-out. An additional objective of the project has been to preserve a significant portion of the ecologically viable RAFSS on the project site, including areas of the pioneer, intermediate and mature phases of this plant community and habitat. Although some areas of RAFSS are proposed to be removed by the project, the RAFSS habitat that is proposed to be set aside is expected to remain an ecologically viable community and to provide important conservation habitat for species reliant on this kind of habitat.

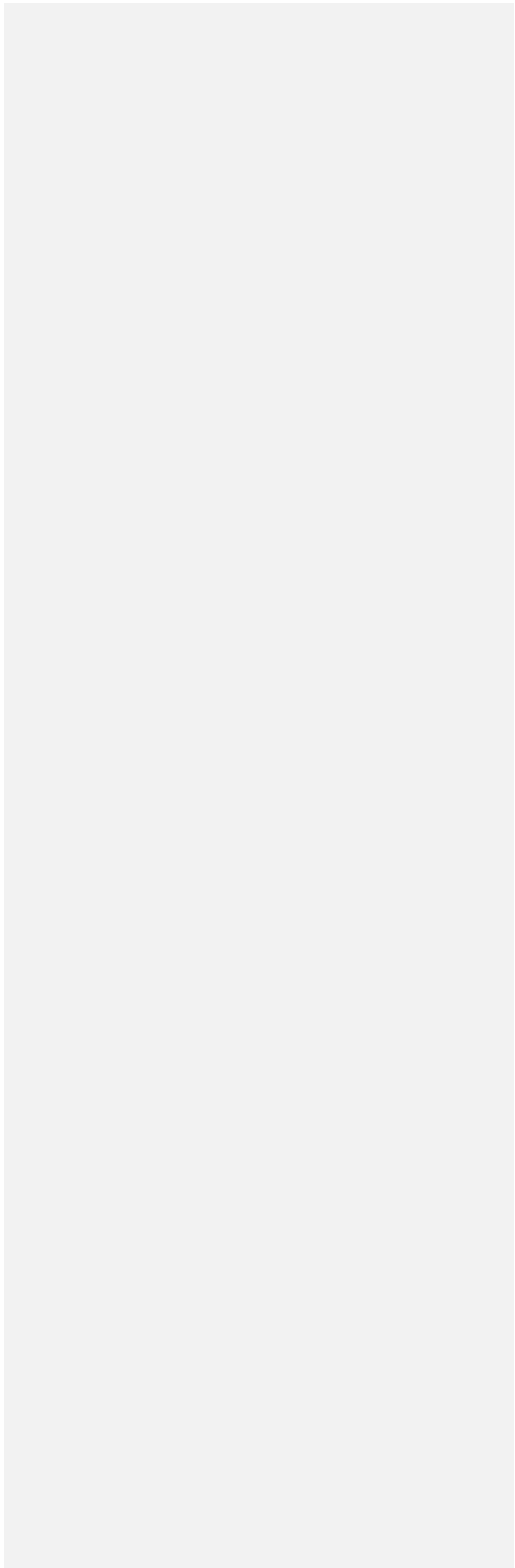
Another advantage to the location of the natural open space to be conserved is the location of other past natural open space dedications benefiting the SBKR and RAFSS habitat in the vicinity. In particular, the project has been designed to make a synergistic contribution to SBKR conservation and recovery by designating suitable SBKR habitat as natural open space which will be contiguous to, expand upon and augment other existing SBKR conservation areas, as well as areas containing RAFSS habitat, through a combination of habitat set aside and active habitat restoration, enhancement, and management. For example, the project would dedicate another roughly 443 acres of open space and RAFSS habitat in and adjacent to Lytle Creek Wash, which would be contiguous with a previous 52-acre area set aside for SBKR conservation by the project applicant in connection with the County's approval of the Lytle Creek North development project and which would also be contiguous with a prior contribution of another 160.5 acres of RAFSS and SBKR habitat in Lytle Creek Wash that had been identified as contributing to SBKR conservation through consultations with the USFWS as part of the Lytle Creek North Planned Development project.

LYTLE CREEK RANCH
SPECIFIC PLAN

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LYTLE CREEK RANCH
SPECIFIC PLAN

Figure 3-9
Open Space and Conservation Plan



LYTLE CREEK RANCH
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SPECIFIC PLAN

The majority of the natural open space to be dedicated by the Lytle Creek Ranch project has also been designated as critical habitat for the SBKR and coastal California gnatcatcher by the USFWS. Importantly, the dedicated natural open space in the Specific Plan would link with and expand upon previous conservation dedications by the project applicant for the SBKR and coastal California gnatcatcher and with hundreds of acres of SBKR conservation lands located both immediately downstream and in the Cajon Wash (including SBKR mitigation areas established by Calmat, San Bernardino County and Cemex). In total, these land set asides would secure a large amount of connected, occupied and suitable SBKR habitat and RAFSS habitat within the Lytle / Cajon Wash system, thereby securing a substantial amount of functional habitat for these species and allowing the species to persist in this area over the long-term.

In addition, the project applicant proposes to extend the scope of its SBKR habitat restoration, enhancement and monitoring program – currently being applied to 216.7 acres of wash and upland habitat (including upland refugia habitat) within Lytle Creek Wash. The USFWS has previously approved a habitat restoration and enhancement plan for approximately 40 acres of upland refugia habitat within a large island in Lytle Creek Wash. The project proposes to expand this restoration program to increase the amount and quality of protected occupied SBKR habitat within Lytle Creek Wash, thereby further enhancing the long-term conservation and recovery of this species within the Lytle / Cajon wash system. The coastal California gnatcatcher would also be expected to benefit from this restoration program, helping to ensure that Lytle Creek Wash can continue to provide for potential movement of this species, as well as others, through this area.

3.4.2 MAINTENANCE OF WILDLIFE MOVEMENT CORRIDOR

The on-site portions of Lytle Creek and the adjoining wash function as a regional wildlife corridor. Preservation of the majority of the wash will ensure the continued viability of this wildlife corridor. The conservation area generally ranges from approximately 600 feet wide within Lytle Creek to 2,400 feet wide within Neighborhood III. The wash provides wildlife cover through scattered islands and patches of vegetated habitat. Natural open space to be set aside by this project will link directly with other open space mitigation areas with similar habitats in Lytle and Cajon washes, including portions of land owned by Cemex, Calmat, and San Bernardino County.

3.4.3 PRESERVATION AND RESTORATION OF RIPARIAN HABITAT FOR LEAST BELL'S VIREO

~~Neighborhood I encompasses a riparian corridor, Sycamore Flats, which will be preserved and enhanced as part of the proposed project. The northernmost portion of the corridor is not included in the preservation/enhancement area for this Specific Plan since it is San Bernardino County's land.~~ Mitigation for riparian habitat impacts elsewhere in the project area will include restoration and enhancement to approximately 18.9 acres of the riparian corridor and the adjacent floodplain. This area serves as habitat for the least Bell's vireo.

3.4.4 PRESERVATION OF PARRY'S SPINEFLOWER AND PLUMMER'S MARIPOSA LILIES

The project will preserve the majority of the Parry's spineflower and Plummer's mariposa lilies in the project area. The Plummer's mariposa lily is considered a sensitive plant species and the Parry's spineflower is a species of interest to the California Native Plant Society, which is in the process of gathering more data to determine whether, and to what extent, this species may be a sensitive species. Although both species have been found in Neighborhood II, Neighborhood III, and Neighborhood IV, the vast majority of the individuals occur within the large island within the wash in Neighborhood III. The sensitive plants on this island occur both within a portion already set aside for SBKR for the Lytle Creek North Planned Development project, as well as within the portion farther east or downstream that is within the proposed open space for the Lytle Creek Ranch Specific Plan. Therefore, the majority of Parry's spineflower and Plummer's mariposa lily will be preserved in perpetuity.

3.4.5 PROTECTION OF NESTING BIRDS

To protect nesting birds regulated by the federal Migratory Bird Treaty Act, efforts will be made to schedule vegetation removal between September 1 and February 14 to avoid the nesting bird season. If clearing and/or grading activities cannot be avoided during the nesting season, all suitable habitat will be thoroughly surveyed for the presence of nesting birds by a qualified biologist prior to removal. If any active nests are detected, the area will be flagged, along with a minimum 100-foot buffer (buffer may range between 100 and 300 feet as determined by the monitoring biologist) and will be avoided until the nesting cycle is complete or it is determined by the monitoring biologist that the nest has failed. In addition, a biologist will be present on the site to monitor any vegetation removal to ensure that nests not detected during the initial survey are not disturbed.

3.4.6 PROTECTION OF BURROWING OWLS

In order to avoid impacts to any burrowing owls that may colonize the development impact footprint prior to commencement of construction activities, a Phase III protocol survey shall be conducted within 30 days prior to commencement of ground disturbance activities (California Burrowing Owl Consortium 1993). This pre-construction survey will entail four separate days between two hours before sunset to one hour after or one hour before sunrise to two hours after. This survey applies during both the breeding season (February 1 through August 31) as well as the non-breeding season when wintering owls are most likely detected if present (December 1 through January 31). If burrowing owls are detected within the development impact footprint or within approximately 80 feet of the impact area, on-site passive relocation would be conducted during the non-breeding season in accordance with the establishment protocol (California Burrowing Owl Consortium, 1993).

3.5 CIRCULATION PLAN

3.5.1 CIRCULATION PLAN DESCRIPTION

Figure 3-10, Circulation Plan, illustrates the project roadway concept, based on input from the City Traffic Engineer and the project Traffic Consultant, Crane and Associates. The main objective of the Circulation Plan is to provide direct and convenient access to individual residential enclaves, employment, and service land uses through a safe and efficient street network and a pedestrian trail/sidewalk system. The Circulation Plan includes standards for vehicular circulation, pedestrian circulation, bikeways, parking facilities, and connections to mass transit. Typical roadway cross sections are shown on Figures 3-11 through 3-14.

Primary project entries will be located at Riverside Avenue/N. Live Oak Avenue and Riverside Avenue/N. Locust Avenue. Secondary entries will be located on Riverside Avenue opposite Redwood Avenue and N. Alder Avenue. The entries on Riverside Avenue opposite N. Live Oak Avenue and N. Locust Avenue will serve as the Primary Entries into Neighborhood III. The entry at Country Club Drive will serve as a Primary Entry into Neighborhood II. Another Primary Entry into Neighborhood II will be provided at Riverside Avenue/Linden Avenue.

3.5.2 VEHICULAR CIRCULATION NETWORK AND HIERARCHY

The Vehicular Circulation Plan includes a network of public and private streets that create an efficient and comprehensive street pattern. The circulation network includes the following streets:

Interstate 15 (I-15) Freeway

The I-15 Freeway generally runs in a north-south direction. The freeway has three to four travel lanes in each direction near Lytle Creek Ranch. Access to I-15 is provided at Sierra Avenue and Glen Helen Parkway.

State Route 210 (SR-210) Freeway

The SR-210 Freeway runs in an east-west direction. The freeway begins in the City of Rialto and extends westerly to merge with Interstate 210 in the City of Glendora. In the project vicinity, SR-210 is being constructed with three mainline travel lines and a High-Occupancy-Vehicle (HOV) lane in each direction. Access to SR-210 is provided via Riverside Avenue.

Riverside Avenue (See Figure 3-11)

Riverside Avenue is designated as a Major Arterial in the City of Rialto. This roadway borders Lytle Creek Ranch on the southwest, and intersects Sierra Avenue just south of the I-15 Freeway/Sierra Avenue interchange. Riverside Avenue provides direct access to Neighborhoods II and III of Lytle Creek Ranch. Figure 3-15 depicts a typical bus mid-block turn-out design along Riverside Avenue.

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As proposed, Riverside Avenue will be improved with a 127' right-of-way consisting of a 14' striped median, three travel lanes in each direction totaling 38', a 24' landscaped parkway that includes an 8' sidewalk/bicycle path on the Lytle Creek Ranch side of the street, and a 13' landscaped parkway with a 4'-6" sidewalk on the south side (Las Colinas side).

Lytle Creek Road/Sierra Avenue (See Figure 3-11)

Lytle Creek Road/Sierra Avenue has a 104' right-of-way north of the I-15 Freeway. South of the I-15 Freeway, Sierra Avenue has a 132' right-of-way. Lytle Creek Road and Sierra Avenue border the project site on the northwest, and Lytle Creek Road provides access to Neighborhood IV ~~and to Neighborhood I~~ via Glen Helen Parkway.

Glen Helen Parkway (See Figure 3-11)

Glen Helen Parkway is a Major Highway in the County of San Bernardino. The roadway extends easterly from Lytle Creek Road, and provides direct access to Neighborhoods ~~I and~~ IV of Lytle Creek Ranch. Glen Helen Parkway is improved with a 114' right-of-way consisting of a 14' striped median, two travel lanes and a breakdown lane (totaling 33' in each direction), and a 17' landscaped parkway that includes a sidewalk on each side of the street.

~~Clearwater Parkway (See Figure 3-11)~~

~~Clearwater Parkway extends through Neighborhood I of Lytle Creek Ranch and the adjacent residential development, and provides access to Glen Helen Parkway at the northern end of the project. As proposed, Clearwater Parkway will have a 104' right-of-way that consists of a 14' striped median, two travel lanes and a breakdown lane (totaling 33' in each direction), and 12' landscaped parkways.~~

Entry Streets – Neighborhood III (See Figure 3-12)

Lytle Creek Ranch proposes several entry streets in to Neighborhood III from Riverside Avenue. The entry streets will be opposite the existing streets of Redwood Avenue, N. Live Oak Avenue, N. Alder Avenue, and N. Locust Avenue. Each Entry Street will have a 118' right-of-way consisting of a 14' landscaped median, one 26' travel lane in each direction, and 26' parkway on either side. A 5' sidewalk will be provided within the parkway on each side of the street. Each entry street will be designed with a special landscaped entry treatment adjacent to Riverside Avenue.

Collector Streets (See Figure 3-12)

Collector streets are designed to collect local residential street traffic to major and secondary entry streets and to Riverside Avenue in Neighborhood III. Collector streets will have a 94' right-of-way, which consists of one 20' travel lane in each direction, a 14' wide landscape median, and a 17' wide landscaped parkway on one side of the street, and a 23' wide parkway on the other side of the street. In addition, both sides of the street will contain a 5' sidewalk.

LYTLE CREEK RANCH
SPECIFIC PLAN

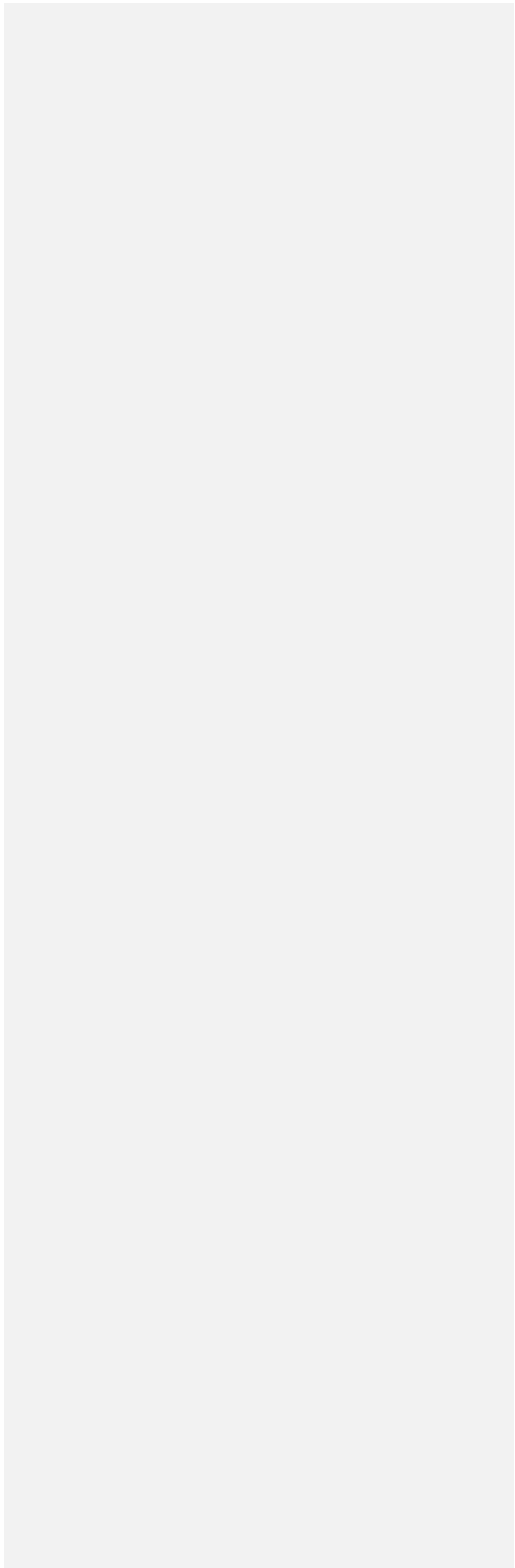
Figure 3-10
Circulation Plan

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LYTLE CREEK RANCH
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Figure 3-11
Roadway Cross-Sections

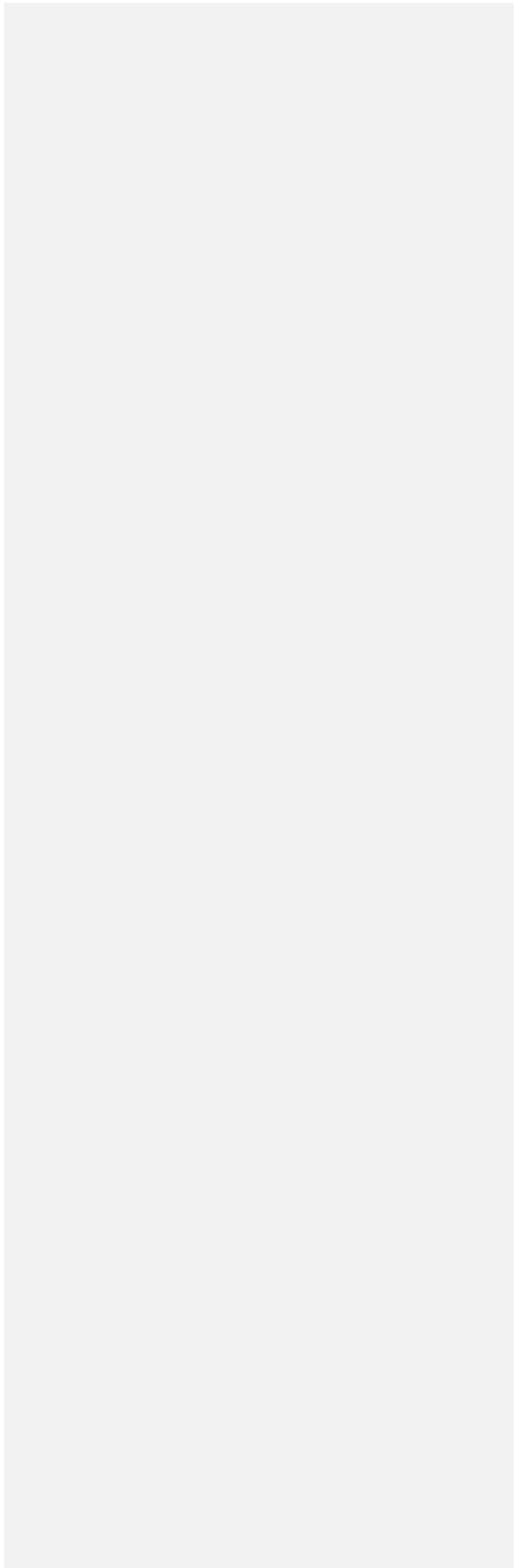


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Figure 3-12
Roadway Cross-Sections



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Figure 3-13
Roadway Cross-Sections

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Figure 3-14
Roadway Cross-Sections

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Figure 3-15
Typical Bus Mid Block Turnout

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LYTLE CREEK RANCH

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Local Streets (See Figure 3-12)

Local streets in Neighborhoods I, III, and IV of Lytle Creek Ranch will provide access to individual properties and connect to collector streets. Local streets will have a 46' right-of-way, which consists of one 18' travel lane in each direction, and a 5' sidewalk on one side of the street and either a sidewalk or landscaping on the other side of the street. Local streets may be either public or private.

Alley Drives (See Figure 3-12)

Alley drives in Lytle Creek Ranch will have a 26' right-of-way, which consists of one 13' drive lane in either direction.

Country Club Drive (See Figure 3-13)

Country Club Drive is a Major Arterial south of Riverside Avenue in the City of Rialto and a local street north of Riverside Avenue in the County of San Bernardino. As part of the Lytle Creek Ranch project, Country Club Drive will be fully improved and re-landscaped from Riverside Avenue to the Lytle Creek Ranch boundary. The reconstructed street will consist of a 102' right-of-way with a 9'-6" raised landscaped median, three travel lanes in each direction (totaling 36' in each direction), and landscaped parkways on either side of the street. Within Lytle Creek Ranch, Country Club Drive will vary in width from a 67' to 72' right-of-way consisting of a central 10' raised landscaped median and a 26' travel lane in each direction. Parkways will be provided on both sides of Country Club Drive directly adjacent to on-site residential uses. No parkways will be provided where Country Club Drive abuts the ~~golf course or~~ open space/recreation uses.

Active Adult Entry Street (See Figure 3-14)

An Active Adult entry street is proposed in Neighborhood II (Active Adult neighborhood). The Active Adult entry street begins with a 102' right-of-way at Riverside Avenue and extends to a point located just east of Planning Area 90, where it transitions into an 84' right-of-way adjacent to Planning Area 92 and residential, open space/recreation, ~~or golf course~~ areas. The Active Adult entry street will consist of a 14' landscaped median, a 12' travel lane adjacent to the median, and a 14' travel lane adjacent to the curb in each direction. There will also be an 18' landscaped parkway on one or both sides of the street, which includes an 8' wide multi-purpose trail.

Active Adult Local Street (See Figure 3-14)

Active Adult local streets are planned in Neighborhood II (Active Adult neighborhood). Similar to local streets, the Active Adult local streets will provide access to individual properties and connect to the Active Adult Entry Street and to Country Club Drive. The Active Adult local street will have a 42' right-of-way, consisting of one 18' travel lane in each direction, and a 10' landscaped parkway that includes a 5' sidewalk on one side of the street.

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Roundabouts (Active Adult Neighborhood Only)

The Lytle Creek Ranch project includes two roundabouts in Neighborhood II and three roundabouts in Neighborhood III to facilitate continuous, safe and orderly traffic movement, while minimizing unnecessary stops. It should be noted that modern roundabouts are not the same as older traffic circles, which exist in some cities. Three basic principles distinguish the modern roundabout from a traffic circle:

- A. Modern roundabouts follow the "yield-at-entry" rule in which approaching vehicles must wait for a gap in the circulating flow before entering the circle. Many traffic circles in the United States require circulating vehicles to grant the right of way to entering vehicles. Some traffic circles also use stop signs or signals to control vehicle entry.
- B. Modern roundabouts involve low speeds for entering and circulating traffic, as governed by small diameters and deflected (curved) entrances. In contrast, traffic circles emphasize high-speed merging and weaving, made possible by larger diameters and tangential (straight) entrances.
- C. Adequate deflection of the vehicle entering a roundabout is the most important factor influencing their safe operation. Roundabouts should be designed so that the speed of all vehicles is restricted to 30 mph or less within the roundabout.

In giving priority to entering vehicles, a traffic circle tends to lock up at higher volumes. The operation of a traffic circle is further compromised by the high speed environment in which large gaps are required for proper merging. These deficiencies have been essentially eliminated with the modern roundabout designs.

The roundabouts in Lytle Creek Ranch will also serve as important iconic elements that will help to create a unique identity for the project circulation system. To keep maintenance requirements to a minimum, each roundabout will contain trees and plant materials, while minimizing the use of turf and other high-maintenance plantings. To help prevent distractions to vehicular traffic circulating through the roundabouts, no potentially distracting features such as fountains, sculptures, community signage, or other similar elements will be permitted within the central island in each of the roundabouts. Traffic-related signage shall be permitted as needed anywhere within the roundabouts.

3.5.3 MASS TRANSIT CONNECTIONS

Lytle Creek Ranch has been designed to promote the use of alternative forms of transportation. The project site is located close to several major transportation routes, including the I-15 (which bisects a portion of the site), I-210, and I-10 freeways. Bus transportation is currently provided along portions of Riverside Avenue (Bus Route 22) by Omnitrans. Bus Route 22 includes stops along a portion of Riverside Avenue and at or near Carter High School, Kolb Junior High School, the Rialto Civic Center, and the Metrolink station located at 261 South Palm Avenue in Rialto. The Metrolink line provides stops in San Bernardino to the east and Fontana, Rancho Cucamonga, Upland, Montclair, Claremont, Pomona, Covina, Baldwin Park, El Monte, Cal State Los Angeles, and Union Station in downtown Los Angeles to the west. Many of the Omnitrans buses are low emitting vehicles and run on Compressed Natural Gas (CNG), while the newest vehicles are electric/gasoline hybrids.

The project proposes a system of bicycle trails and walking trails throughout the project site. These trails will follow the alignment of the major streets in Lytle Creek Ranch and connect to a trail system to be established along the northeast side of Riverside Avenue. Thus, residents will be able to walk or ride their bikes to Riverside Avenue and catch a ride on the bus to other portions of the City.

Convenient access to Village Center Commercial development in Lytle Creek Ranch will be available via the project's internal trail and roadway systems. Retail, commercial and office uses are planned along Riverside Avenue. It is anticipated that there will be a shopping center located near the juncture of Sierra Avenue/Riverside Avenue/I-15 in Planning Areas 31 and 33. This shopping center may contain such uses as big and medium box retailers, a supermarket or grocery store, and other services such as dry cleaners and restaurants. Residents will be able to walk or bike to this center, or drive to the center without placing additional strain on the surrounding off-site roads.

3.5.4 PARKING

Parking shall be provided in accordance with City of Rialto requirements as set forth in Section 18.58 of the City's Municipal Code, except as otherwise amended in this Specific Plan. Shared parking may be permitted in planning areas designated as Village Center Commercial, subject to preparation of a shared parking analysis by a qualified Traffic Engineer. CEQA review may be required. This study shall be reviewed and approved by both the Director of Development Services and the City Traffic Engineer.

3.5.5 CIRCULATION PLAN GENERAL DEVELOPMENT STANDARDS

- A. The proposed project includes an efficient and safe circulation design that shall accommodate traffic from land uses as well as public safety, security and public transportation needs. All on-site roadway improvements shall be phased in accordance with the Infrastructure phasing plan.
- B. Heavy through traffic shall be eliminated from residential neighborhoods. Major roadways shall be implemented as non-access roadways, with residential neighborhoods served by smaller residential collectors.
- C. Provisions shall be made for a safe and efficient trail and sidewalk network, providing pedestrian and bicycle circulation in conjunction with the roadway network. A sidewalk system shall be developed along all key streets within Lytle Creek Ranch. Pedestrian traffic shall be separated from vehicular traffic, particularly in commercial and retail areas.
- D. Bicycle paths shall be located along interior and exterior streets where they will be safe and effective in serving local residents' needs.
- E. All subdivisions shall comply with the street improvement recommendations/mitigations outlined in the project Traffic Analysis and as determined by the Engineering Division of Public Works and Development Services (EIR Technical Appendices).
- F. In selected locations raised planters may be used as local street medians to provide a higher level of street character and visual interest. Specific locations will be identified at the tentative tract map stage.
- G. All roads within Lytle Creek Ranch shall be constructed to the standards contained in this Specific Plan.
- H. Connections to mass transit are encouraged to facilitate and promote alternative transportation.
- I. The master developer shall install all traffic signals within the Specific Plan area as required by the Department of Public Works.
- J. A traffic signal shall be installed at the intersection of Linden Avenue and Riverside Avenue prior to the issuance of the first occupancy permit during the first phase of development. The developer shall be reimbursed for the cost of the installation of the signal less the amount of the project fair share contribution toward the signal as specified in the Traffic Impact Analysis and Mitigation Monitoring and Reporting Plan for the project.

3.6 INFRASTRUCTURE PLAN

3.6.1 WATER SYSTEM

Water Plan Description

The backbone water facilities and infrastructure shall be owned, operated, and serviced by the West Valley Water District (West San Bernardino County Water District). The fair-share cost of designing and constructing the water system shall be financed by the project master developer, project area builders, and/or other financing mechanism(s) acceptable to the City of Rialto. The conceptual water plans for each neighborhood are depicted in Figures 3-16 through 3-19, Water Plans.

~~A. Neighborhood I – The water system for Neighborhood I will consist of a series of new waterlines of varying widths that will connect with existing lines, and a new 6.5 million gallon (MG) reservoir with an approximate site area of 2.2 acres. Neighborhood I lies within Zone 7 of the conceptual water plan.~~

B. Neighborhood II – The water system for Neighborhood II will consist of a series of new waterlines of varying widths, a new 8.5 MG reservoir with an approximate site area of three acres, and a new booster station. Two additional reservoirs are currently in place near Neighborhood II. Neighborhood I lies within Zone 4 of the conceptual water plan.

C. Neighborhood III – The water system for Neighborhood III will consist of a series of new waterlines of varying widths, two new reservoirs, and two new booster stations. Neighborhood III is divided between Zones 5 and 6 of the conceptual water plan. Within Zone 5, a 10.7 MG reservoir covering an approximate site area of 3.5 acres is planned. A 10.1 MG reservoir covering an approximate site area of 3.5 acres is planned in Zone 6. Two additional reservoirs currently exist near Neighborhood III.

D. Neighborhood IV – The water system for Neighborhood IV will consist of a series of new waterlines of varying widths, one new reservoir, and one new booster station. Neighborhood IV is divided between Zones 7 and 8 of the conceptual water plan. A new 4.7 MG reservoir, which will require an approximate site area of 1.6 acres, will be located within Zone 8. Two additional reservoirs currently exist near Neighborhood IV.

Water Plan General Development Standards

A. All lines shall be designed in accordance with the West Valley Water District requirements.

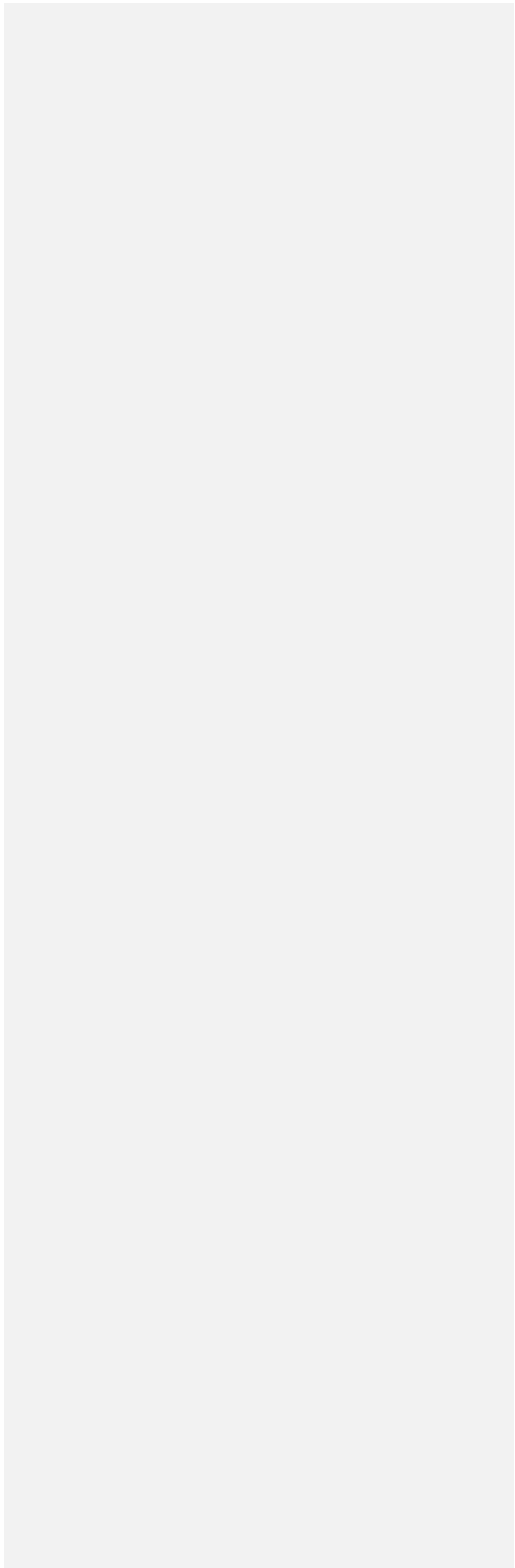
B. Water facilities shall be installed in accordance with the requirements and specification of the West Valley Water District.

LYTLE CREEK RANCH
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- C. Assurance for provision of adequate water service is required prior to approval of a subdivision map, and/or Plot Plan for retail and office uses in accordance with the State Subdivision Map Act.
- D. If a convenient, readily available, and affordable source of recycled water exists, then the project shall incorporate recycled water for landscaping and non-potable uses.
- E. The project shall comply with Title 20, California Code of Regulations Section 1604 (f) (Appliance Efficiency Standards), which establishes efficiency standards that set the maximum flow rate of all new showerheads, lavatory faucets, as well as Health and Safety Code Section 17621.3 which requires low-flush toilets and urinals in virtually all buildings.

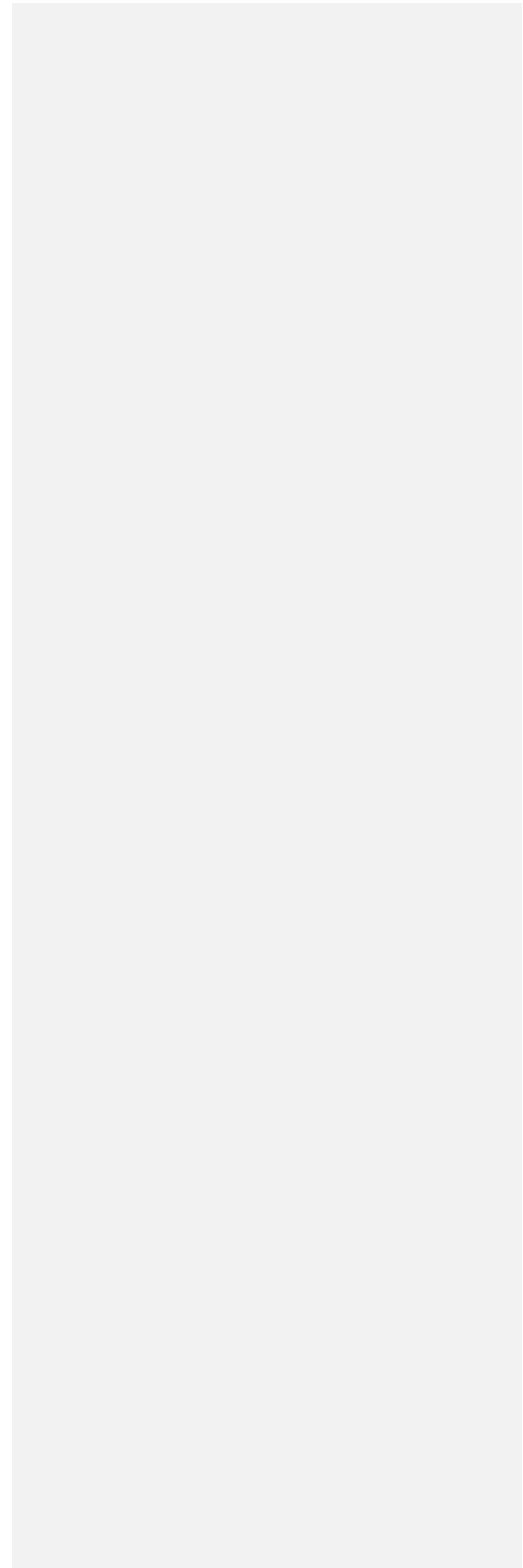
LYTLE CREEK RANCH
SPECIFIC PLAN

Figure 3-16
Water Plan – Neighborhood I



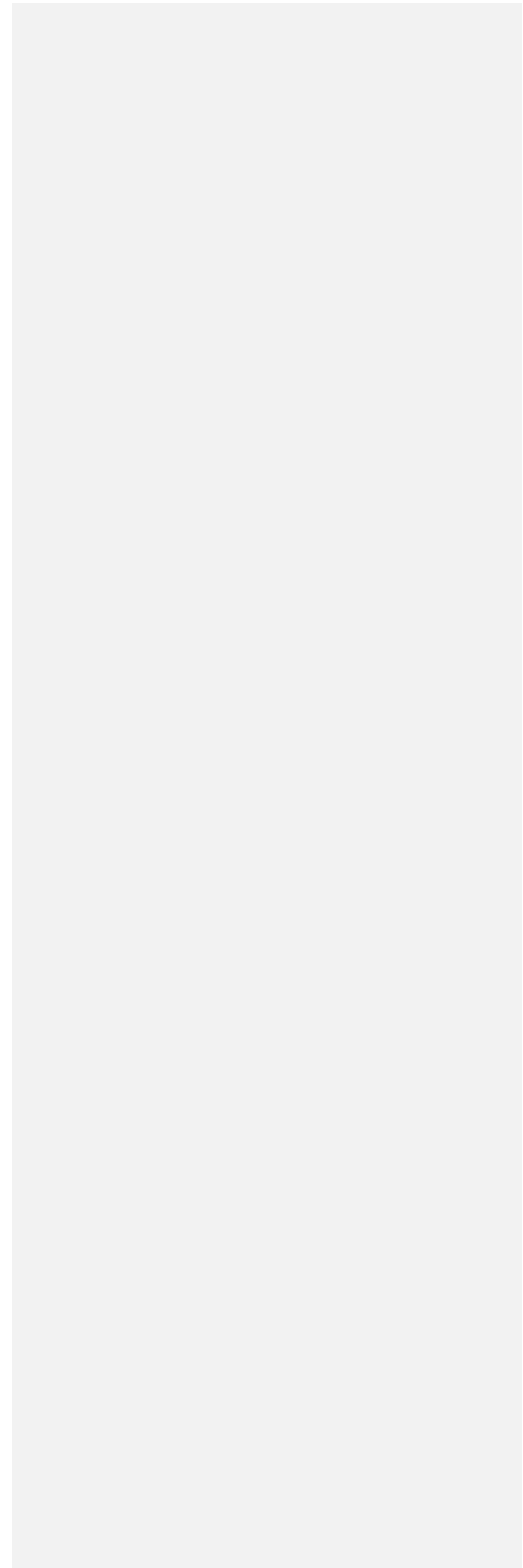
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LYTLE CREEK RANCH
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Figure 3-17
Water Plan – Neighborhood II

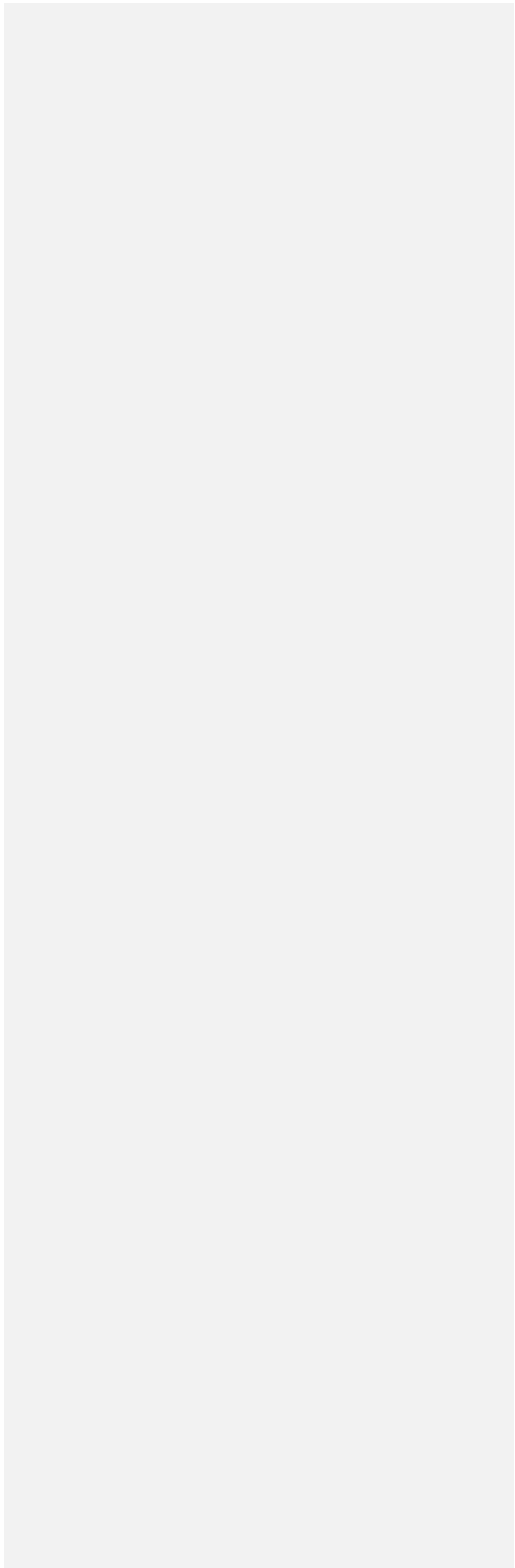


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Figure 3-18
Water Plan – Neighborhood III



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Figure 3-19
Water Plan – Neighborhood IV

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3.6.2 SEWER SYSTEM

Sewer Plan Description

The backbone sewer facilities and infrastructure shall be owned and operated by the City of Rialto. The fair share cost associated with designing and constructing the sewer system shall be financed by the project master developer, project area builders, and/or other financing mechanism(s) acceptable to the City of Rialto.

The proposed on-site collection system for each neighborhood is depicted on a separate exhibit. Please see Figures 3-20 through 3-23 for the sewer exhibits.

- ~~A. Neighborhood I – The sewer collection system for Neighborhood I will collect the sewage and direct the flows into a sewer located within Clearwater Parkway. These flows, in turn, will be handled by the existing wastewater treatment plant, which was constructed as part of the Rosena Ranch community. (See Figure 3-20.)~~
- B. Neighborhood II – The Neighborhood II sewer collection system will be located within the streets in Neighborhood II. A scalping plant will be located at the southern portion of Neighborhood II within the ~~golf course open space~~. The scalping plant will remove water from the sewage and, through the use of micro filtration and reverse osmosis, will reuse the water to water the ~~golf course open space~~. The scalping plant will not produce smell because the sewage will be put back into the sewer system, as all of the water extraction part will take place below ground. The entire process will occur inside of a building or underground. (See Figure 3-21.)
- C. Neighborhood III – The backbone infrastructure in this neighborhood will be located primarily within the central Collector Street and the Grand Paseo open space. To the north, the project will connect underneath the I-15 Freeway to the sewer collector system in Neighborhood IV. To the south, the project will connect to the sewer collector system. (See Figure 3-22.)
- D. Neighborhood IV – The sewer collector system through Planning Areas 20, 23, and 25 in Neighborhood IV will connect underneath the I-15 Freeway to the sewer collection system in Neighborhood III. (See Figure 3-23.)

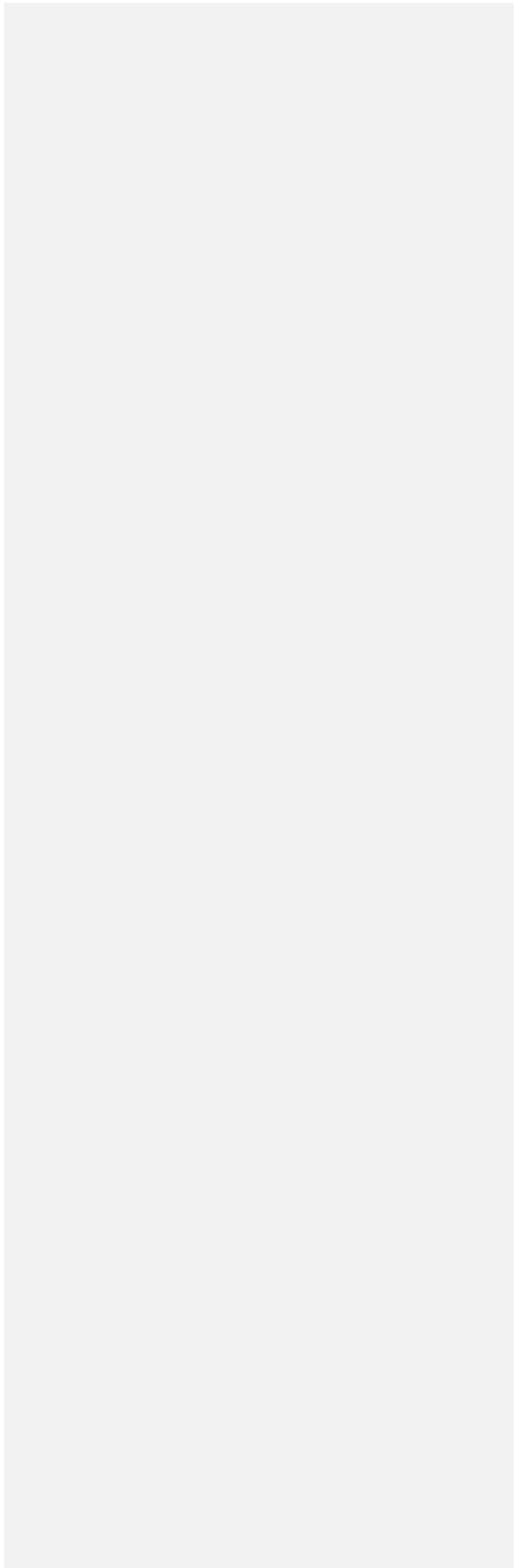
LYTLE CREEK RANCH
SPECIFIC PLAN

Sewer Plan General Development Standards

- A. All sewer lines shall be designed per City of Rialto requirements.
- B. Sewage disposal facilities shall be installed in accordance with the requirements and specifications of the City of Rialto and/or San Bernardino County Special Districts.
- C. Assurance for provision of adequate water service is required prior to approval of a subdivision map, and/or Plot Plan for retail and office uses in accordance with the State Subdivision Map Act.
- D. The project shall comply with Title 20, California Code of Regulations Section 1604 (f) (Appliance Efficiency Standards), which establishes efficiency standards that set the maximum flow rate of all new showerheads, lavatory faucets, as well as Health and Safety Code Section 17621.3 which requires low-flush toilets and urinals in virtually all buildings.

LYTLE CREEK RANCH
SPECIFIC PLAN

Figure 3-20
Sewer Plan – Neighborhood I

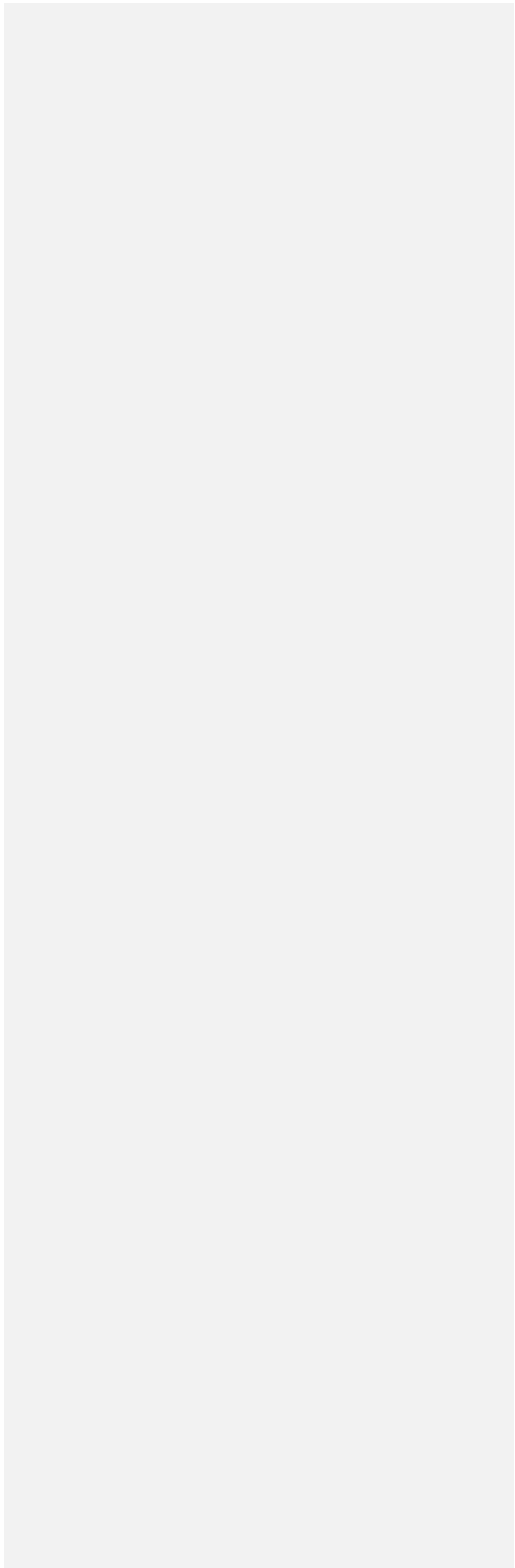


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Figure 3-21
Sewer Plan – Neighborhood II

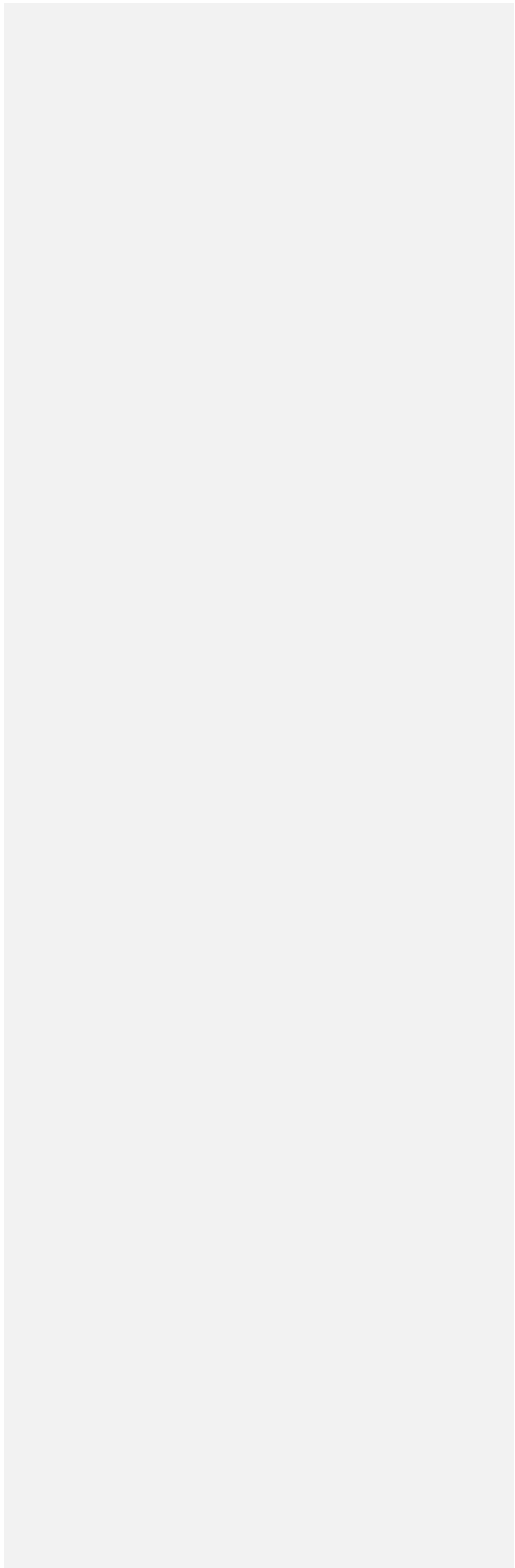


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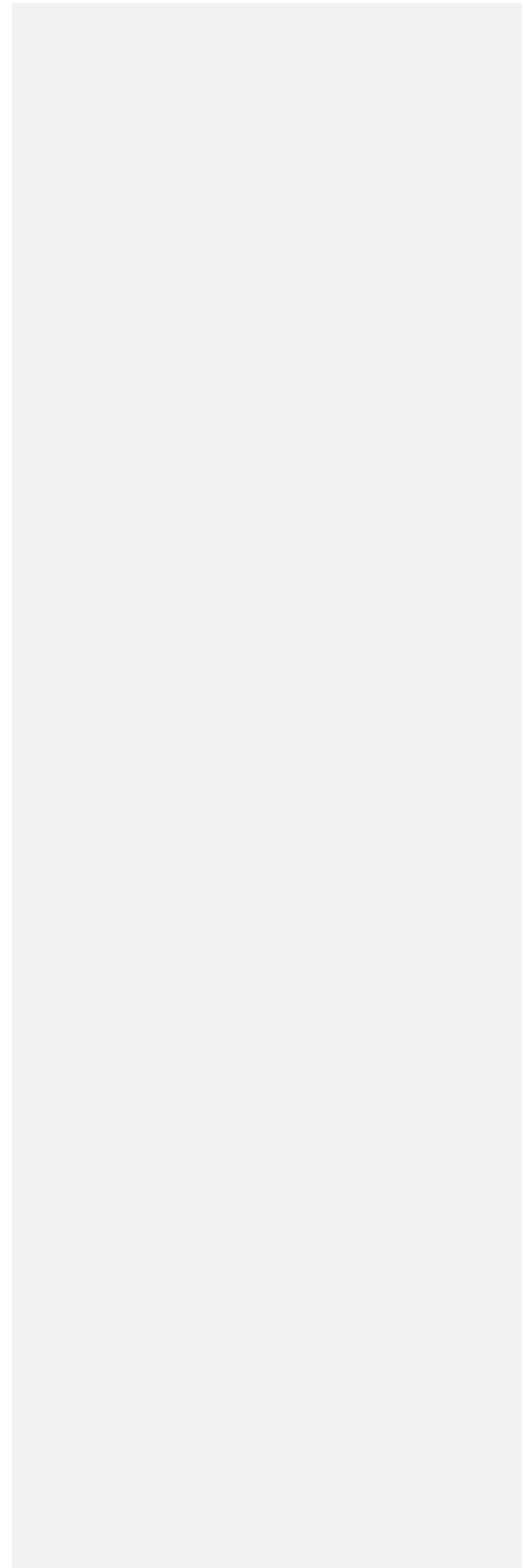
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Figure 3-22
Sewer Plan – Neighborhood III



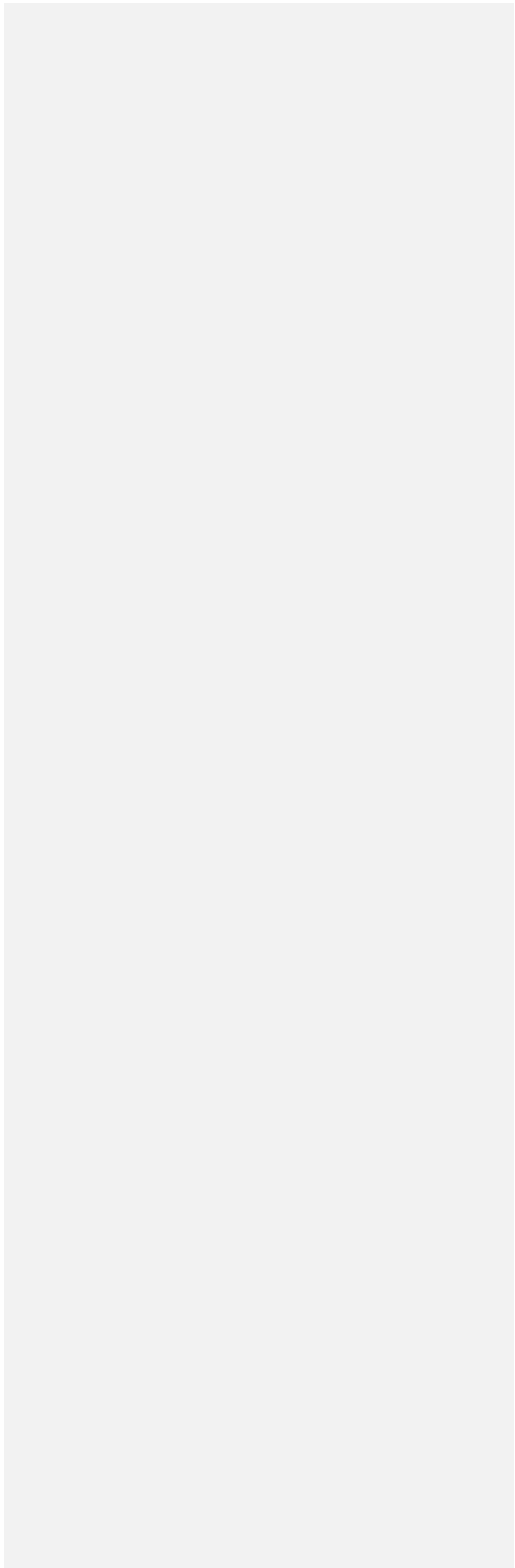
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Figure 3-23
Sewer Plan – Neighborhood IV



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3.6.3 DRAINAGE PLAN

Drainage Plan Description

The project proposes a master drainage plan for the site in order to protect the proposed site from the 100-year flood potential from Lytle Creek (see Figures 3-24 through 3-27, Drainage Plans). The proposed plan utilizes the project streets, storm drains, and the "Grand Paseo" bioswale to carry stormwater through the site.

The local storm drain system shall be funded and constructed by the project master developer, project area builders, and/or other financing mechanism(s) acceptable to the City of Rialto. The regional storm drain system and flood control improvements associated with Lytle Creek Wash is expected to be funded and constructed by a Community Facilities District or other similar mechanism.

- ~~A. Neighborhood I – Water in Neighborhood I will generally flow from northwest to southeast in the portions of Neighborhood I located north of the I-15 freeway, and toward the southwest in the portions to the east of the freeway. Water will flow both on streets and in storm drains. Four water quality treatment basins are planned north of the I-15 freeway, and two basins are planned south of the freeway. (See Figure 3-24.)~~
- B. Neighborhood II – Water in Neighborhood II will generally flow toward the southern portion of Neighborhood II, both on streets and in storm drains. In addition, the reconfigured ~~golf course~~ open space will accommodate much of the drainage flow in the neighborhood. Approximately eight vegetated basins and six water quality treatment basins will be provided within this neighborhood. ~~These basins and the water flowing between them will also act as a series of water features as part of the golf course.~~ (See Figure 3-25.)
- C. Neighborhood III – Water runoff in Neighborhood III will generally drain from north to south. Drainage from each of the proposed catchment areas will be collected at node locations, which will channel the water through a system of urban storm drain piping, which will terminate in water quality treatment basins located within the Grand Paseo. Neighborhood III will contain twelve water quality treatment basins within the Grand Paseo. These basins will detain and treat all first flush water runoff, which is then released further downstream through the Grand Paseo and ultimately discharge at the southerly end of the neighborhood into a system of urban storm drain piping within the Riverside Avenue right-of-way. This piping system will then carry the water runoff east into the Neighborhood II water quality basin system. (See Figure 3-26.)
- D. Neighborhood IV – Water will generally flow drain from north to south in Neighborhood IV, both on streets and in storm drains. Four water quality treatment basins are included in this neighborhood. (See Figure 3-27.)

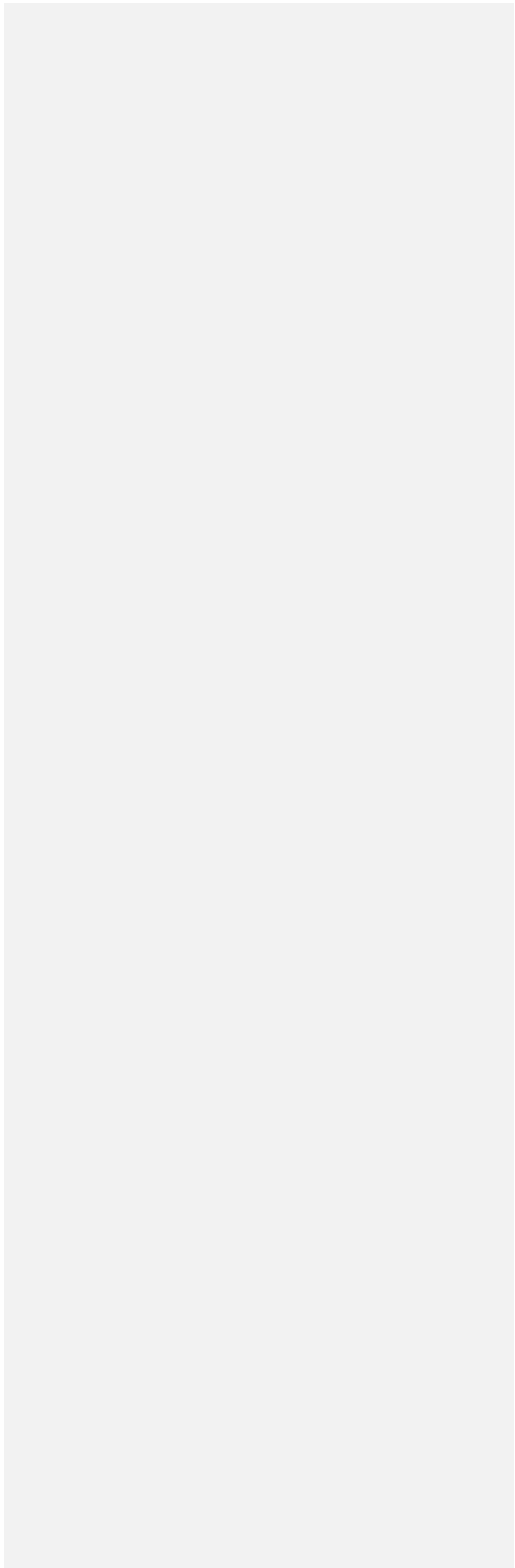
LYTLE CREEK RANCH
SPECIFIC PLAN

Storm Drainage Plan General Development Standards

- A. Drainage and flood control facilities and improvements shall be provided in accordance with the City of Rialto and the County of San Bernardino Flood Control District.
- B. It is anticipated that the major backbone drainage/flood control facilities will be maintained by the City of Rialto and/or County of San Bernardino Flood Control District. Local drainage devices will be maintained by the City of Rialto or a similar public/private entity.
- C. All proposed construction activities including, clearing, grubbing or excavation shall obtain the appropriate State general permit for National Pollutant Discharge Elimination System (NPDES) permits and pay the appropriate fees. Proposed Best Management Practices (BMPs) may include, but are not limited to, on-site retention, vegetated swales (bioswales), and monitoring programs.
- D. A Stormwater Pollution Prevention Plan (SWPPP) will be prepared in accordance with the California State Water Resources Control Board (State Water Board) Order No. 92-08-DWQ National Pollutant Discharge Elimination System (NPDES) General Permit No. CAS000002. This SWPPP complies with Best Available Technology (BAT) achievable and Best Conventional Pollutant Control Technology (BCT) to reduce or eliminate stormwater pollution from areas of a construction activity. The SWPPP document will be certified in accordance with the signatory requirements of Standard Provisions C. 9 in the State General Construction Stormwater Permit.

LYTLE CREEK RANCH
SPECIFIC PLAN

Figure 3-24
Drainage Plan – Neighborhood I



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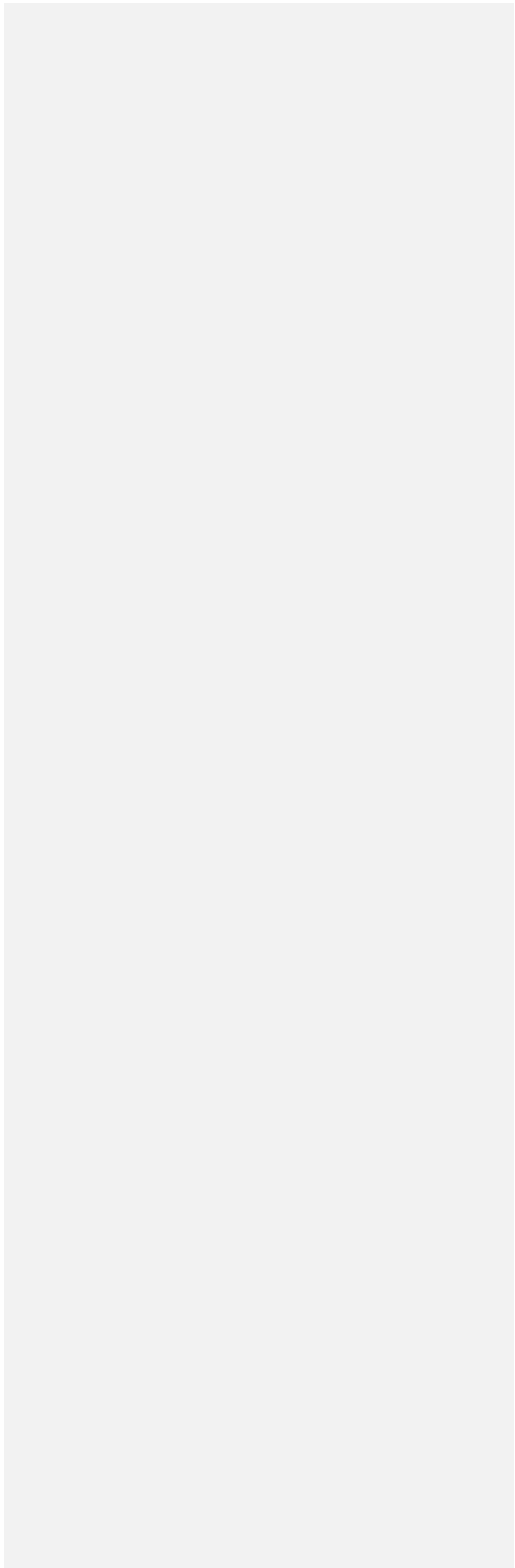
Figure 3-25
Drainage Plan – Neighborhood II

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Figure 3-26
Drainage Plan – Neighborhood III



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Figure 3-27
Drainage Plan – Neighborhood IV

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3.6.4 UTILITIES AND PUBLIC SERVICES

Utilities

It is anticipated that utilities will be provided by the existing public utility companies, Southern California Edison (SCE) and Southern California Gas Company (SCG) or other service providers allowed to enter the market place under Assembly Bill 1890.

Electricity

Electrical service is currently provided in the area by Southern California Edison Company (SCE). All new lines installed in Lytle Creek Ranch shall be placed underground.

Adequate electric power supply will be provided by SCE. SCE will install the necessary distribution facilities to serve the project site.

It should be noted that Assembly Bill 1890, commonly referred to as the "Public Utilities Act," has allowed for the deregulation of public utilities in California. Based on this Act, a number of other service providers are able to enter the marketplace. Consequently, there may be additional utility service providers in the future providing the same services that SCE currently provides to southern California and the proposed project site.

Natural Gas

Natural gas service is currently provided by Southern California Gas Company. Existing facilities in the area include existing lines located in Riverside Avenue and Knollwood/Country Club Drive.

SCG indicates that gas service could be provided to the Specific Plan area in accordance with the Company's policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made.

It should be noted that Assembly Bill 1890, commonly referred to as the "Public Utilities Act," has allowed for the deregulation of public utilities in California. Based on this Act, a number of other service providers are able to enter the marketplace. Consequently, there may be additional utility service providers in the future providing the same services that SCG currently provides to southern California and the proposed project site.

Cable Services

Cable services are provided by Time Warner Cable, which is located at 3430 East Miraloma Avenue in Anaheim, California. Services offered by Time Warner Cable include cable television (e.g., HDTV, DVR, On Demand), high-speed internet, and digital telephone service.

Telephone Service

Telephone service to the site is available from AT&T.

LYTLE CREEK RANCH

SPECIFIC PLAN

Public Services

Public Services are considered and planned as part of the overall Specific Plan development concept. Public Services include solid waste disposal, hazardous waste disposal, police protection, fire protection, schools, and libraries.

Solid Waste Disposal

Trash collection and waste disposal is available through EDCO Disposal, which is located at 1850 Agua Mansa Road in Riverside, California. EDCO is a family-owned and locally operated waste collection and recycling company that has been serving various communities in Southern California since 1967. The EDCO family of companies offers integrated waste removal and recycling programs to serve residential homes, multi-family properties, commercial businesses, multi-tenant buildings, industrial centers, construction sites, and community events. Trash service in the City of Rialto is mandatory and EDCO Disposal is the only service provider approved by the City Council with an exclusive franchise.

The Mid-Valley Landfill (permitted as the Fontana Refuse Disposal Site, permit number 36-AA-0055) is located at 2390 Alder Avenue in Fontana. It is owned by San Bernardino County and operated under contract by Burrtec Waste Industries Inc., which operates and maintains all disposal facilities owned by San Bernardino. The site is located in Fontana, approximately 0.5-mile north of Highland Avenue and approximately 0.25-mile east of Sierra Avenue.

The landfill site includes 498 acres. Its ultimate capacity is listed as 62 million cubic yards. The estimated capacity remaining is listed as 694,000 cubic yards. It is expected to continue in operation as an active disposal site until at least 2033. It is estimated that the landfill has capacity remaining until the year 2040 (estimated). The final use of the landfill site after closure has not been decided.

Hazardous Waste Disposal

Disposal of motor oil and oil filters, antifreeze, weed killer and fertilizers, household cleaners, latex and oil base paints, wood preservative, paint thinner, car polish/wax, furniture and floor polish, auto and household batteries, aerosol paint, medicines (prescription and over the counter), pesticides, cosmetics, nail polish and remover, or pet care products is available at the City of Rialto Household Hazardous Waste Site, which is located behind 246 South Willow Avenue in Rialto, California.

Police Protection

Law enforcement services shall be provided by the City of Rialto Police Department. Services will be provided as residential units and development come online. The Fire Department is currently responsible for response to 911 police calls, traffic collisions, medical, and other types of emergencies. Additional services provided include crime prevention, investigation, and enforcement of the law, providing police support to the area with patrol responses, reporting, and investigative support. A portion of Lytle Creek Ranch falls within the response time radius of the new County sheriff's facility in Neighborhood I and, as such, could potentially be served by that sheriff's facility.

LYTLE CREEK RANCH

SPECIFIC PLAN

Fire Protection

Fire protection and emergency response services for the project area are provided by the City of Rialto Fire Department. In 2007, the City had four fire stations. It is anticipated that Fire Station 202, which is located at 1925 N. Riverside Ave in Rialto, will serve the project. Station 202 has one fire engine and two paramedic ambulances (one in reserve). The fire station will provide wildland and structural fire protection, and response to 911 medical aid calls, traffic accidents, and hazardous materials.

Additional support may be provided by Fire Station 204, which is located at 3288 N. Alder in Rialto. Fire Station 204 has two fire engines (one in reserve), one water tender, and two specialized units. Services will be provided as residential units and development come online.

In addition, a new County fire station is planned as part of the adjacent Rosena Ranch community. This new fire station must be constructed prior to occupancy of the 1,000th dwelling unit in Rosena Ranch and will be operated by San Bernardino County. This fire station will be sited on a parcel of land situated between Planning Areas 14 and 15. Portions of the Lytle Creek Ranch community (Neighborhoods I, IV, and portions of III) falls within the response time radius of the new County fire station and, as such, can be served by that fire station.

Schools

The proposed project is located within ~~three~~two different school districts. ~~Students in a portion of Neighborhood I will attend existing schools in the San Bernardino City Unified School District (SBCUSD).~~ It is anticipated the existing schools will have sufficient capacity to serve the new students generated by the proposed project.

Students in the northwestern portion of the Lytle Creek Ranch (Neighborhood IV) will attend schools in the Fontana Unified School District (FUSD). Students in the FUSD will attend existing schools, as it is anticipated that existing schools will have sufficient capacity to adequately serve the new students generated by the proposed project.

As previously mentioned, an elementary school and a joint elementary/middle school are proposed in Neighborhood III on 10-acre and 14-acre sites, respectively. Both proposed school sites are located within the jurisdictional boundaries of the Rialto Unified School District.

No students will be generated by Neighborhood II as it is planned as an age-qualified community for adults aged 55 years and older.

The project will pay its fair share of fees to each school district as required by California state law and/or the project master developer shall enter into a mitigation agreement with the school district.

LYTLE CREEK RANCH

SPECIFIC PLAN

Libraries

Library services to the project site will be provided by the San Bernardino County Library System from a series of branch libraries. The closest branch library to Lytle Creek Ranch is located at 251 West 1st Street in Rialto. Development of the project will generate additional patrons and will ultimately create a need for additional staff and space for additional resources. The project will pay library fees, which are included in the City's Development Impact Fees (DIF) to offset the project's effect on library services.

3.7 GRADING PLAN

3.7.1 GRADING PLAN DESCRIPTION

The Conceptual Grading Plan is sensitive to the natural topography of the site, which slopes gently toward the south. The Conceptual Grading Plan will create large pads that conform generally to the existing natural landforms. (See Figures 3-28 through 3-31, Grading Plans). Additional conditions that act as constraints for grading of the site include Lytle Creek, existing perimeter conditions; existing street grades; the ability to use gravity sewers; and no diversion of storm flows on adjacent properties. The may require some off-site material to be imported in order to reflect the grading depicted on the Conceptual Grading Plans. The Conceptual Grading Plans are subject to modification pending final design and engineering.

3.7.2 GRADING PLAN GENERAL DEVELOPMENT STANDARDS

1. All grading activities shall be in substantial conformance with the overall Conceptual Grading Plans (see Figures 3-28 through 3-31), and shall implement the recommendations outlined in the Geotechnical Study (EIR Technical Appendices). Grading for the site shall balance on-site.
2. For erosion control purposes, slopes exceeding five feet in vertical height shall be hydromulched, prior to final acceptance and prior to the beginning of the rainy season (October – March).
3. All grading shall be accomplished in accordance with the City of Rialto standards.
4. The applicant shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations become the responsibility of other entities.
5. Graded, but undeveloped land shall be maintained weed-free and planted with interim landscaping, such as hydroseed, and temporary irrigation within one year (365 days) of completion or grading, unless building permits are obtained.

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SPECIFIC PLAN

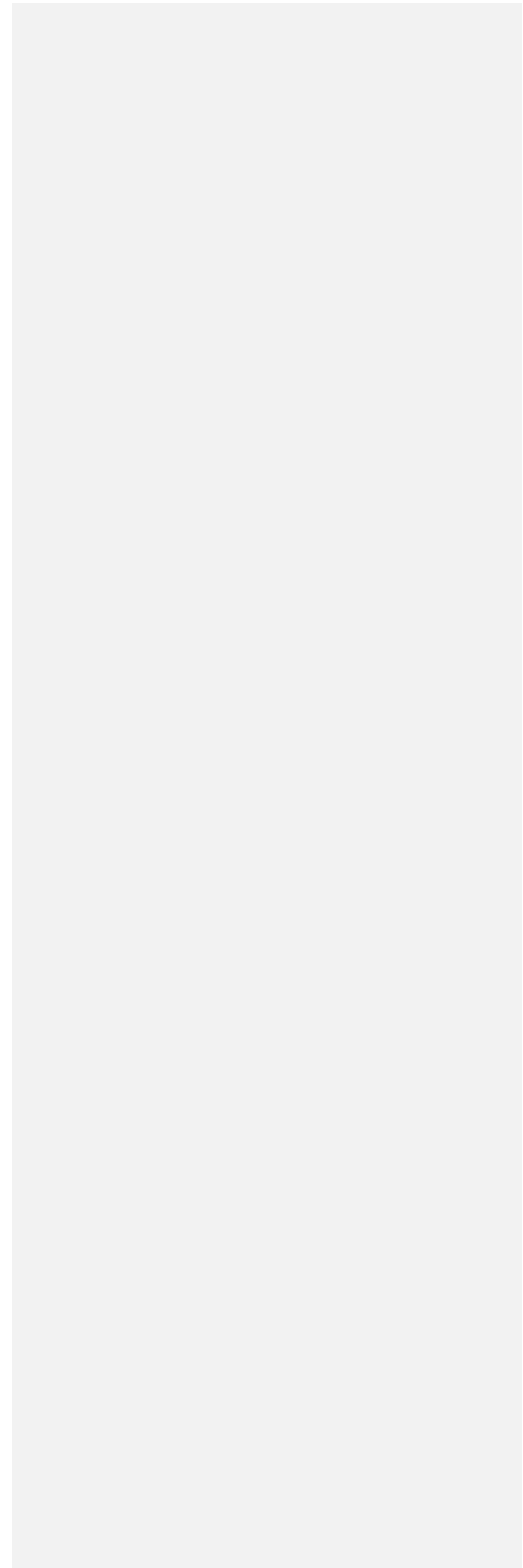
6. Slopes shall not be steeper than 2:1 unless approved by the Planning and Public Works Departments and considered safe in a slope stability report prepared by a soils engineer or an engineering geologist.
7. Prior to commencing any grading, including clearing and grubbing, a grading permit shall be obtained from the City of Rialto.
8. Soil stabilizers shall be used to control dust as required by SCAQMD Rule 403.

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LYTLE CREEK RANCH
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Figure 3-28
Grading Plan – Neighborhood I

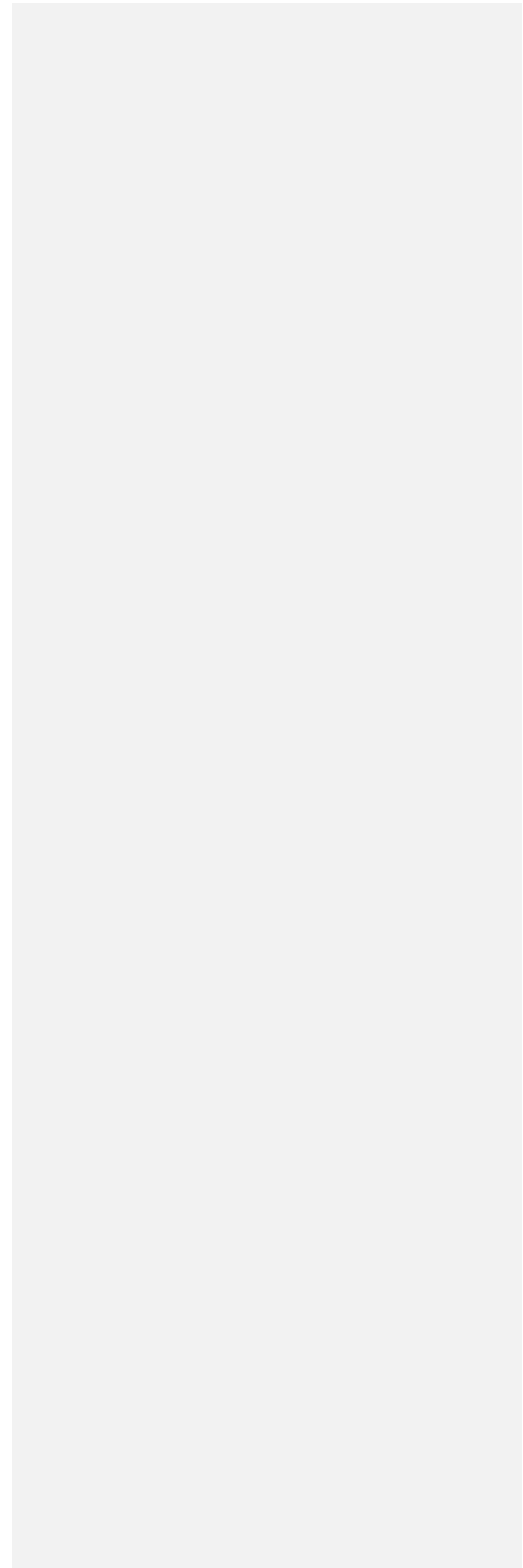


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Figure 3-29
Grading Plan – Neighborhood II



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Figure 3-30
Grading Plan – Neighborhood III

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Figure 3-31
Grading Plan – Neighborhood IV

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4.0 DESIGN GUIDELINES

This Chapter contains the site planning, architectural, and landscaping design guidelines for the Lytle Creek Ranch community. These guidelines, when implemented, will ensure that Lytle Creek Ranch develops as a quality master planned community with consistent design elements. These guidelines are intended to provide general direction to planners, builders, architects, landscape architects, and engineers, and should not be construed to be rigid standards that cannot be modified. The essence of good design is creativity and flexibility – these guidelines are intended to foster those ideals and promote innovation.

4.1 PLANNING GUIDELINES

It should be noted that the design guidelines and the standards contained in this Specific Plan will guide development within Lytle Creek Ranch.

4.1.1 DESIGNING THE CITY'S NORTHERN GATEWAY

Currently, there is no clear boundary to the northern edge of the City of Rialto. The master planned community of Lytle Creek Ranch is designed to serve as the northern gateway into the City. A striking new entry feature will be erected on Riverside Avenue, near its intersection with Sierra Avenue, announcing to residents and visitors alike that they are entering Rialto. This entry feature will be a representation of the famous “Rialto Bridge” (see image below).



Artist's Conception of “Rialto Bridge” Entry Feature on Riverside Avenue

4.1.2 INTERFACE WITH LYTLE CREEK WASH

There will be a defined edge of development created along Lytle Creek in Neighborhoods ~~I, II, III,~~ and IV. A 20 foot wide public trail will run along the length of Neighborhoods II, III, and IV, providing visual public access to the creek, but not direct public access to the wash itself.

4.1.3 NEIGHBORHOOD CHARACTER AND DESIGN

A. Neighborhood Charm and Character

Lytle Creek Ranch will include a variety of residential housing types in community settings that reflect the neighborhood charm and structure reminiscent of small Southern California towns. The community is being designed as a mix of family-oriented ~~and Active Adult~~ homes clustered into identifiable neighborhoods. The entire community will contain well-designed housing and nicely landscaped residential areas. Each neighborhood will be designed with its own unique identity and character. This will be accomplished by promoting authentic architecture and designing iconic streets so that each street “tells a story.” Each street will have its own design elements and features and landscape palette to create an identifiable streetscape.

It is one of the key goals of these design guidelines to promote development of a community that, while unique, is reminiscent of the architectural heritage of Rialto and other local agrarian communities from the early to mid 20th century. It is intended that the architecture in Lytle Creek Ranch incorporate historic details and stylistic characteristics, while responding to the needs of modern buyers. Each aspect of every project should be designed to reinforce the neighborhood concepts for the Lytle Creek Ranch community.

B. Neighborhoods of Lytle Creek Ranch

Lytle Creek Ranch is actually ~~four~~three separate neighborhoods situated within a larger master planned community as follows:

Neighborhood I

~~This neighborhood will include some of the largest lots on-site with some of the best views. The homes in these areas will include a mix of home sizes including, but not limited to, 6,000, 7,200, and 10,800 square foot lots. Development in this area will reflect large gracious homes with variable building setbacks to create an attractive streetscene. The landscaping will be designed to reflect the proximity of the homes to nearby Glen Helen Regional Park and the San Bernardino National Forest. Streetscenes will include native and water-wise landscaping interspersed with carefully selected ornamental plantings. The larger sized lots will be large enough to accommodate swimming pools and other private recreational amenities. Individual housing developments within Neighborhood I may be gated to promote a sense of luxury and security. Some or all of the residential developments within Neighborhood I may be gated at the discretion of the master developer or builder(s).~~

Neighborhood II

This neighborhood ~~is planned for conventional single-family homes with the potential for~~ will be devoted exclusively to Active Adult housing for residents aged 55 and older. ~~Because many of the homeowners will be downsizing from larger properties,~~ these neighborhoods will contain clusters of homes on smaller sized lots. Neighborhoods will incorporate a mix of housing types to foster visual interest. Front porches, patios, and enhanced entries will promote a sense of neighborliness. The focal point of the community will be ~~an enhanced and reconfigured public 18-hole golf course and an Active Adult~~ recreation center. ~~Neighborhood II will be constructed as a gated community.~~

Neighborhood III

Neighborhood III is the largest of the neighborhoods and is targeted at families of all sizes, couples, and singles with a range of incomes and housing needs. This neighborhood will contain a mix of both attached and detached single family housing, as well as higher density housing including, but not limited to, condominiums, townhomes, courtyard homes, motorcourts, mansionettes, and apartments. This Neighborhood will contain extensive amenities including an elementary school, a K-8 school, several neighborhood parks, three private recreation centers, as well as trails, a large greenbelt (i.e., the "Grand Paseo"), and landscaped parkways. Clusters of homes will be arranged to promote a sense of place and neighborhood. Some of the enclaves of homes within this neighborhood may be gated; It is desirable, however, that most of the enclaves remain ungated with "architectural forward" designs to promote social interaction between neighbors.

Neighborhood IV

The fourth and final residential neighborhood will consist primarily of multi-family and attached housing. While single family housing is not prohibited, it is anticipated that this area will develop mostly with higher density residential development. The developments in Neighborhood IV may be gated at the discretion of the master developer or the builder(s).

4.1.4 PLACEMAKING

Successful neighborhood design within Lytle Creek Ranch depends on site planning, architecture, and landscaping being integrated into unified neighborhood concepts. The project approach includes "placemaking" to ensure that each neighborhood has a distinctive character and "sense of place." Walled subdivisions are permitted within all four of Lytle Creek Ranch's neighborhoods, particularly in planning areas where issues of privacy, security, or noise concerns exist. In most areas of Neighborhood III, however, Lytle Creek Ranch should contain open, unrestricted planning areas that promote walking between uses and social interaction. It is desirable that development in Neighborhoods III not consist entirely of a collection of walled subdivisions.

Lytle Creek Ranch will contain integrated communities of single-family detached homes, alley-loaded and courtyard style homes, manor homes, motorcourts, townhomes, mansionettes, flats, apartments, and other innovative products. Neighborhoods should be designed to avoid the appearance of a walled fortress. Although select planning areas within Lytle Creek Ranch may be

fully walled, it is intended that the use of walls be minimized in most areas. Entrances of multi-family and attached homes should generally front onto the major backbone streets so as to create appealing streetscenes, rather than having the rear of the units face the major streets. Residential subdivisions should, in general, connect to one another through street and alley connections and via paseos, sidewalks and trails, and landscaped parkways. An important goal of these guidelines is to create a streetscene possessing both functional and visual variety. Plotting and massing garage placement and building elevation criteria are intended to provide this variety in appearance, as well as a sense of individuality for each detached home.

4.1.5 LIVABLE STREETS

Lytle Creek Ranch is designed to promote interplay between streets and houses. Homes will be designed to have a strong relationship to the street. Residents should have direct views of the street and outdoor living space to enhance the sense of safety and security. One way to enhance security is to orient rooms, doors, and windows toward streets and public areas. Another way is to have houses “open up” to the street by incorporating architectural elements such as front stoops and porches.

A strong balance will be established between the built form and the landscaping with the introduction of continuous parkways along streets, uniform street trees on each street, paseos and greenways, and sidewalks for pedestrian connectivity. The goal is to create intimate, socially interactive and secure neighborhoods that encourage street activity, promote walking, and allow convenient access to parks, schools, and shopping.

In order to create more “livable streets,” it is also necessary to control traffic and reduce speeds. On-street parking will help to calm traffic in residential neighborhoods, as will narrower street cross-sections. Two traffic roundabouts are planned in ~~the Active Adult neighborhood (Neighborhood II)~~ to help slow down traffic. Additionally, the use of curb separated sidewalks will help create a more visual pleasing streetscene.

Livable streets may be achieved by implementing some or all of the following strategies:

- Different Product Types for Single-Family Detached Residential Dwellings
- Forward Architecture/Recessed Garages
- Varied Setbacks
- Varied Building Massing
- Variable Lot Sizes
- Neighborhood Edge Treatments

These strategies are described below:

Different Product Types for Single-Family Detached Residential Dwellings

Each single-family residential planning area will be required to provide at least four different architectural products. Also, each single-family detached product will have no fewer than three distinct building elevations. In addition, each single-family detached residential planning area shall contain a minimum of three architectural styles in order to add visual interest and variety. No one architectural style shall constitute more than forty (40) percent of all of the single-family detached residences constructed in the Lytle Creek Ranch Specific Plan area, excluding attached and multi-family development. For attached dwelling units and multi-family units, the products may either be designed with one consistent architectural style or designed with two or more architectural styles as determined by the builder(s). To enhance the streetscene, floor plans shall be interspersed periodically with reverse floor plans. Overly repetitious plans and building footprints shall be discouraged. Innovative floor plans that maximize interior space are encouraged.

Architecture Forward Design/Recessed Garages

The "living" portions of the house will be allowed to be placed forward on the lot so that active, articulated architecture will visually dominate the streetscene. House designs are encouraged that place entries, windows, front porches, and living areas close to the street on most plan variations. The living spaces of the home shall be designed in front of the garages such that the predominant features of the home fronting the street are the windows and the front door. Other architectural forward features include providing articulation on two-story homes facing streets and other areas exposed to public view, such as single story elements and covered front porches.

Varied Setbacks

Varied building setbacks are encouraged along the street frontage to create a dynamic streetscene. Strict compliance to the minimum garage setback is discouraged so as not to contribute to a repetitious and monotonous appearance along the street. Where feasible, a mix of housing forms and plans resulting in a variation of front yard setbacks is encouraged in single-family detached residential planning areas.

Building Massing

On larger lots, single-story elements may be incorporated into two story buildings, especially on corner lots, to create more pleasant streetscene. Roof planes and types will be varied between different products to enhance the feeling of diversity. The building massing should be reduced, where appropriate, through the addition of details and varying roof forms and styles between buildings.

Variable Lot Sizes

Where possible and appropriate, variable lot sizes in adjacent planning areas may be used to increase buyer selection and variety in house and lot size combinations.

Neighborhood Edge Treatments

In order to avoid a continuous “walled” character along residential collector streets, special treatments are encouraged. These may entail open cul-de-sacs, view fencing, hedges, shrubs, neighborhood street tree programs, trail connections, and variable wall alignments and treatments.

Wrap-Around Architecture

The front façade treatment of residential structures should wrap partially around onto the side of the house. This is especially important on corner lots, where the side of the homes is highly visible. Where side yards are screened from view by adjacent homes, the treatment may extend for only a portion of the front. However, some elements (such as trim) should continue fully on the sides of the buildings.

Four-Sided Architecture

Four-sided architecture will be required for homes abutting Riverside Avenue, the Main Entry Street and all Secondary Entry Entries in Neighborhood III, Collector Streets in Neighborhoods II and III, the ~~Active Adult~~ Entry Street in Neighborhood II, and on homes located adjacent to parks, the Grand Paseo, schools, and recreation centers, ~~and the Neighborhood II golf course.~~

Variations in Garages (Applies to Single-Family Detached Homes Only)

Variations in garage type, placement, sizes, and locations are desirable. In general, the home and yard, rather than the garage should be the primary emphasis of the elevation as seen from the street. The visual impact of the garage may be minimized by varying garage setbacks and allowing recessed garages, side-on garages, and other different types of garages. A variety of garage placement solutions may be considered within Lytle Creek Ranch, including the following:

Set Back Garages

On larger and wider lots, it is possible to set the garage back to the middle or rear of the lot. This design treatment strives to expose more habitable architecture toward the street, and pushes the innovation of the plan.

Rear Access

The use of rear accessways relocates garages off neighborhood streets and creates a more traditional streetscene, without garages dominating the front of the homes. This condition occurs on alley loaded designs and is especially desirable on narrower lots.

Side Entry Garages

The periodic use of side entry garages on lots at least 55 feet wide will break the continuous view of garage doors along the street. This design treatment allows for a formal motorcourt entrance that differentiates this type of home from those on narrower lots.

Tandem Garages

This garage layout de-emphasizes the second or third garage by concealing it behind a standard one- or two-car garage condition. The tandem space is located such that it may option into living space while still only showing the original one- or two-car garage to the street. Tandem garages are permitted for all products types within Lytle Creek Ranch.

Corner Lot Garage with Wrap-Around Architecture

A corner lot garage plan will have an entry door on the front elevation while the garage doors will be found on the side elevation. This plan form creates the opportunity for architectural enhancements that wrap around the home from front to side elevation.

4.1.6 DEFENSIBLE SPACE

Good design and site planning techniques will help foster a sense of perceived and actual security in Lytle Creek Ranch. Defensible space involves using architectural and environmental design to minimize the potential for criminal activity by promoting visibility and creating a sense of ownership. When space is used in such a way that makes people feel safe and secure in the community, it fosters the likelihood for increased social interactions — a primary source of crime deterrence. Techniques such as lighting, walls and fencing, and landscaping, can define spaces in a manner that promote community safety by decreasing criminal activity.

In developing site plans and designing neighborhoods for Lytle Creek Ranch, security should be given serious consideration. Design parameters that ought to be considered for new developments in Lytle Creek Ranch include the following:

- a. The front yards, the fronts of buildings, and the main entries to dwelling units should face streets or driveways so as to facilitate normal patrolling by police cars and police response to residents' request for assistance. This will also enable residents across the street, whose units also face the street, to survey their neighbors' front doors.
- b. Sidewalks or walkways shall be provided for safe convenient direct access to each dwelling unit and for safe pedestrian circulation throughout a development between facilities and locations where major need for pedestrian access is anticipated. Walkways shall be located so that they are easily seen from the interior of units.
- c. Lighting shall be provided for the entire developed site with concentrations at walks, ramps, parking lots, and entrances to dwelling units. The intent is not to bathe the site with light, but to provide adequate lighting for surveillance purposes. In most cases, lighting should be directional to avoid unnecessary sky glow, glare, and light trespass.

- d. Plantings should not be placed so as to screen the doors and windows of dwelling units from views from the street or from walkways leading from the street to the dwelling unit entries.
- e. Plant materials should be selected and arranged to permit full safe sight distance between approaching vehicles at street intersections. Additional attention is required where driveways enter streets, at crosswalks and especially in areas of concentrated mixed pedestrian and vehicular movement. Plantings that hide pedestrians from passing motorists should be avoided.
- f. Distinguish private spaces from public spaces by using landscaping plantings, pavement designs, walls and fences, grade changes, and other visual cues to differentiate spaces.
- g. Perceived safety is as important as actual safety. A park that is well-maintained and cared for presents itself as a safe and fun place where people want to spend time.
- h. Design walls to be “graffiti resistant” through carefully selecting materials and coatings. Installing plant materials along walls will help to make walls less desirable to graffiti vandalism.
- i. Design spaces around public buildings so that residents can meet there (e.g., foundations, benches, playgrounds, seating walls, etc.).

4.2 ARCHITECTURAL GUIDELINES

The purpose of these Architectural Guidelines is to identify the general architectural design concepts for the design and implementation of the buildings in Lytle Creek Ranch. The design concepts presented herein are intended to establish the overall architectural character for the neighborhoods and reflect the historical development precedents of the City of Rialto and the Inland Empire. The goal is to promote both visual compatibility and variety in a community setting achieved by utilizing a number of compatible traditional and contemporary styles, and through quality architectural innovation. This provides a strong framework to ensure that Lytle Creek Ranch is developed in a manner that enhances the existing development in the City.

The architectural design concept for Lytle Creek Ranch is based on creating a thematic community, reflective of early and mid 20th century Southern California architectural styles. To achieve this, specific architectural styles consistent with this concept have been identified as especially appropriate for use in Lytle Creek Ranch. These architectural styles include, but are not limited to, American Farmhouse, California Bungalow, Craftsman, Spanish Eclectic, Monterey, Tuscan, and Italianate, and focus on human scale details, thus enhancing the pedestrian-friendly character of the neighborhoods in Lytle Creek Ranch. Such elements may include the use of front porches, patios, enhanced entries, a mix of materials, colors and textures, and detailing on features such as

columns, balconies, windows, doors, shutters, wrought iron, wood trim, and lighting. Together, such design features enliven the streetscene and promote the friendly interaction of neighbors.

The descriptions provided in these Architectural Guidelines are intended to help serve as the design “inspiration” for the development of architecture within the Lytle Creek Ranch community. The photographs and graphic representations contained in this section are provided for conceptual purposes only, and are designed to help architects and designers envision the basic intent of the residential architecture in the Lytle Creek Ranch community.

4.2.1 ARCHITECTURAL STYLES

The community of Lytle Creek Ranch will contain a mix of architectural styles to promote interest and diversity and establish a distinct sense of place. The architectural character envisioned for the residential neighborhoods of Lytle Creek Ranch is influenced by the historical precedents of development traditionally found in Rialto and the Inland Empire during the late 19th century and early to mid 20th century. Neighborhoods of these eras can be characterized by their use of a mixture of architectural building styles on any given street, homes oriented to the street featuring architecturally expressive elevations and front porches, and a streetscape treatment all combining to create a pleasant neighborhood environment.

Permitted architectural styles in Lytle Creek Ranch include, but are not limited to:

- American Farmhouse
- California Bungalow
- Craftsman
- Monterey
- Spanish Eclectic
- Tuscan
- Italianate

The inherent attractiveness, informality, and sense of elegance of these styles have enabled the styles to remain popular over a long period.

Lytle Creek Ranch will contain smaller residential groupings of homes within each neighborhood. Rather than limit housing types to one or two styles, a variety of architectural styles are permitted and encouraged within Lytle Creek Ranch. The need for variety is especially important given the community's long build-out period and the desire to respond to changing consumer tastes. Because market conditions and homeowner preferences are constantly evolving, additional architectural styles not specifically identified in this Specific Plan shall be permitted within the Lytle Creek Ranch Specific Plan area. Site plans, building elevations, and a colors and materials palette for all architectural products shall be submitted to the City of Rialto for Design Review to ensure that the quality design is commensurate with the standards contained in this Specific Plan.

LYTLE CREEK RANCH
SPECIFIC PLAN

Lytle Creek Ranch is designed to promote interplay between streets and houses. Homes should be designed to have a strong relationship to the street. Residents should have direct views of the street and outdoor living space to enhance the sense of safety and community. One way to accomplish this is through the orientation of rooms, doors, windows. Another way is to have houses “open up” to the street by incorporating architectural elements such as front stoops and porches.

It should be noted that the photographs contained in this Chapter are representative of concepts envisioned for Lytle Creek Ranch. The intention is to incorporate a variety of these design features into the community. These concepts may be subject to future refinements based upon economic, marketing, detailed architectural design, and other factors. The photographs shown are not intended to be indicative of the actual future product types for the Lytle Creek Ranch Specific Plan, but are instead provided as a source of inspiration for the architects, designers, and builders that will actually design and build the architecture in Lytle Creek Ranch.

These Design Guidelines provide the flexibility to create variety in the architectural expression and interpretation of these design styles, while also providing the framework to achieve harmony and compatibility throughout the neighborhoods. The following style elements for each of the seven architectural styles proposed are encouraged and appropriate for use in any of the ~~four~~^{three} neighborhoods of Lytle Creek Ranch. The ultimate determination as to which architectural style(s) will be used in each residential or Village Center Commercial planning area will be made by the master developer or the builder(s) of each planning area.

A. AMERICAN FARMHOUSE

Historical Precedent

The American Farmhouse architectural style is defined by simple practicality. Homes were designed to provide basic comfort and utility, be attractive, and offer flexibility to grow and change uses over time. Well into the early 20th century, most homes were designed and built by local craftsmen, resulting in substantial regional deviations across the country. Because the Farmhouse architecture across the country showed the impact of local immigrant groups; the style was often a hybrid of ideas from different parts of the world combined with the unique circumstances of American small towns.

Design Characteristics

The core design elements of Farmhouse, found in structures in many parts of the country, include:

- Covered porches
- Dormer windows
- Gabled roofs
- Wood and stucco siding
- Typically homes consisting of two stories

Further details of these design concepts include:

Roofs:

Roofs are typically gable roofs. Roofs will often have dormers.

Overhangs:

Narrow roof overhangs with a plain frieze board.

Siding Materials:

Incorporates horizontal wood siding.

Window shapes/Treatments:

Horizontal windows with 6/6 lights. Plain window trim. Dormer windows are common.

Porches:

Buildings have large porches that take up most or all of the front façade and sometimes portions of the side façade. Porch supports are usually simple with little adornment.

Color Palette:

Colors include earth tone colors, reds and browns, white, grays, and off-whites. Trim is usually white or off-white.

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Exhibit 4-1
Examples of American Farmhouse Architecture

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Exhibit 4-2
Examples of American Farmhouse Architecture

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Exhibit 4-3
American Farmhouse Details

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B. CALIFORNIA BUNGALOW

Historical Precedents

The California Bungalow first appeared as a reaction to the elaborate decoration of the then-popular Victorian style. They were relatively easy and affordable to construct; kits could even be purchased through mail-order catalogs, which contained the plans and materials required for construction. The design spread east from California and remained popular into the Great Depression.

Design Characteristics

A number of features help define the California Bungalow style. These characteristics include:

- Low-pitched gabled or hipped roofs
- Large covered porches at the front entry
- Generally smaller overall size, but some large examples
- Typically one- to one-and-a-half stories
- Windows typically grouped in sets of two or three
- Large windows on front façade
- Large, decorative doors
- Front stoop

Further details of these design concepts include:

Massing:

California bungalows are typically narrow rectangular houses. Often one to 1½ stories in height. Two story buildings in the California Bungalow style are permitted.

Roofs:

Bungalows have low-pitched gabled or hipped roofs. Homes often have street-facing gables with shingled roofs.

Overhangs:

Usually provide wide overhangs that serve to shade the house.

Siding Materials:

California bungalows are typically made of stucco or sided with horizontal wood siding.

Windows:

Bungalows typically include a mix of window sizes and shapes with large front windows, often with divided panes above larger solid panels or panes. Windows are typically either single- or double-hung windows or casement windows. Sliding windows are not used. Window frames should be constructed of wood or wood-appearing materials. Windows are commonly grouped into sets of two or three.

LYTLE CREEK RANCH
SPECIFIC PLAN

Doors:

Front doors are typically large and decorative, often with glazing.

Porches:

Bungalows have covered front porches that are a defining characteristic of the style. The porches typically have rectangular or tapered columns. The lower part of the porch may be constructed of wood, stone, brick, river rock, or manufactured stone or brick.

Color Palette:

Colors vary widely, but include blues, greens, grays, creams, white, and beiges and other earth tone colors. Trim is often white or off-white.

Exhibit 4-4
Examples of California Bungalow Architecture

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Exhibit 4-5
Examples of California Bungalow Architecture

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Exhibit 4-6
California Bungalow Details

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C. CRAFTSMAN

Historical Precedent

The Craftsman style grew out of Bungalow architecture and was strongly influenced by the English Arts and Crafts movement. It is truly an American style which originated in southern California, and spread across the country during the 1920's and 30's through pattern books and catalogues.

The craftsman style of architecture sought the elimination of superfluous ornamentation, creating beauty instead through the simplified lines and masses of the building itself. The Craftsman style is characterized by one-story masses, although many California examples include second stories, and feature low-pitched, gable roofs (occasionally hipped) with wide, unenclosed eaves overhangs. Many times the roof extends to cover a full-width or partial-width front porch. Roof rafters are usually exposed, and decorative beams or braces are commonly added under gable roof ends.

The front porches are supported by heavy wood columns resting on tapered square masonry pilasters which frequently extend to the ground without a break at the level of the porch floor. The materials used in the Craftsman style were common to the region so that there was a strong integration of the structure to the landscape.

Craftsman Design Characteristics

Craftsman architecture was widely employed throughout the country during the early 20th century. This unique style promoted hand crafted quality to create natural, warm and livable homes. Symbolic characteristics of this architectural style are particularly emphasized by:

- Full- or partial-width porches with horizontal railings pickets
- Low- to moderate-pitch gable roofs with broad or deep overhangs with exposed rafter tails at the eaves and trellises over the porches
- Knee braces
- Detailed porch columns
- Grouping of windows in pairs or groups of three
- Shed or gable dormers
- Use of stone, brick, stucco, shingles, and horizontal siding
- Horizontal rather than vertical lines

Further details of these design concepts are included as follows:

Roofs:

Roofing material shall consist of flat or rustic concrete tile or architectural grade asphalt shingle. All roofing materials shall be fire retardant. Wood roofs are prohibited in Lytle Creek Ranch.

Overhangs:

Wide overhangs of 24 inches to 30 inches with unenclosed eaves.

Siding Materials:

Siding materials often consists of medium sand finish stucco, which may be used in combination with a manufactured stone wainscot base. Horizontal and vertical lap siding can be used as an accent feature. Stone, brick, or masonry, either real or manufactured, may be used as accent features and on post bases, piers, and fireplaces.

Chimneys:

Chimneys, if provided, should reflect building materials consisting of stone, brick, or stucco. Chimneys may incorporate a stucco, concrete, or metal cap.

Porches:

Porches will generally encompass no less than one half the length of the façade (exclusive of the garage). The porch base will generally be constructed with masonry. Lattice treatments generally are not appropriate. Porches should be designed as integral element of the building. Porch railings should compliment the building's architectural style. Instead of railings, porch masonry bases may continue as columns or as low walls.

Porch posts, columns, and piers will typically consist of double columns over brick, grounded stone column, tapered wood over stone pillars, double wood columns with trim, square columns on masonry, or tapered columns on masonry. Where railing exists, they shall reflect metal pickets, straight wood pickets, or a combination of wood and metal fascia pickets.

Balconies:

This style typically does not include balconies.

Windows:

Windows are grouped in twos or threes. Windows shapes shall consist of vertical and horizontal rectilinear or square windows with unique light divisions. Double-hung windows with divided lights are allowed in upper sash only. Small accent windows and angled bays shall be limited.

Window trim will consist of wood or simulated wood materials. Window trim shall be either wide (5 to 6 inches) with head trim extended past jamb trim or head trim with cap molding, or tapered side trip with head trim flared at ends. The use of mirrored or highly reflective glass is generally not permitted; provided, however, that energy efficient windows are encouraged.

Typical window sills will consist of projected wood or concrete, projected brick, or recessed and projected stucco or other similar materials.

LYTLE CREEK RANCH
SPECIFIC PLAN

Doors:

Doors may include some unique pattern of glass. Configurations may include a unique glass division, rectangular glass over rail, single divided light window, small square window over rail, or vertical glass over rail.

Garage Doors:

Typical garage doors may feature a glass over horizontal panel, glass over vertical panel, or vertical plank.

Entry:

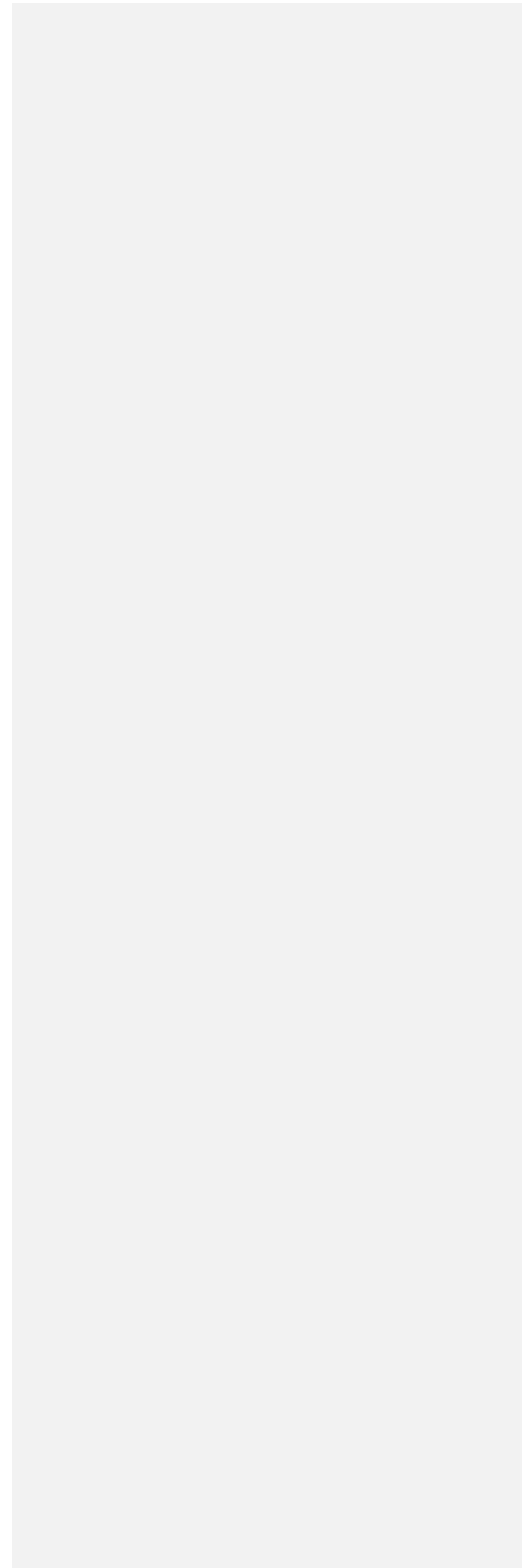
Entries will be covered (i.e., a porch or overhang).

Color Palette:

Acceptable colors include, but are not limited to, stone, cream, browns, tans, beiges, yellows, yellow, grey-greens, yellow-greens, azure blue, light blue, and pure blue, with white and light colored trim.

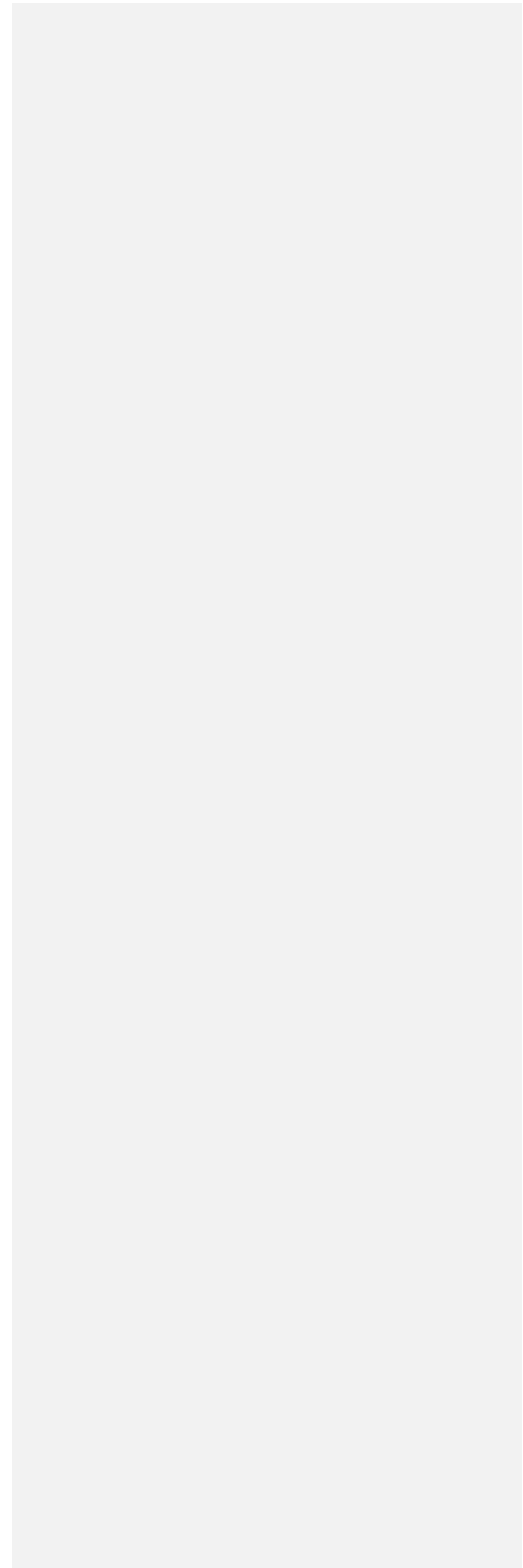
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Figure 4-7
Examples of Craftsman Architecture



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Figure 4-8
Examples of Craftsman Architecture



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LYTLE CREEK RANCH
SPECIFIC PLAN

Exhibit 4-9
Craftsman Architecture Details

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D. MONTEREY

Historical Precedent

The Monterey style developed in the town of Monterey on California's central coast in the mid-19th century. The style developed from a combination of New England Colonial architecture brought by American settlers with the adobe architecture of the Mission period in California.

The major features of Monterey-style architecture are simple, two-story masses, similar to Colonial designs; a projecting balcony on the second floor extending along all or most of the façade; stucco or plaster exteriors (a result of adobe influences); and wood shake or clay tile roofs.

Design Characteristics

Monterey architecture is defined by several key features. These characteristics include:

- Simple, two-story masses
- Cantilevered balconies (sometimes serving as a porch) on the second floor, extending along all or most of the façade
- Stucco or plaster exteriors, occasionally with wood siding on the second story
- Wood shake or clay tile roofs
- Wood shutters are common, generally the same width as the adjacent multi-paned windows. Paired windows and false shutters are also common.
- Simple wood doors
- Colonial details such as pedimented doors and windows

Further details of these design concepts include:

Roofs:

Roofs forms are primarily front-to-back gables; intersecting cross-gables are permitted. Roof materials shall be flat concrete tiles or concrete shakes with a simulated wood appearance. Real wooden shakes are not permitted in Lytle Creek Ranch because of fire concerns.

Overhangs:

Overhangs shall extend a minimum of 12 inches.

Siding Materials:

Front elevations shall consist of stucco, brick, or cementuous siding.

Windows:

At least one principal window shall be included along the front elevation, featuring shutters, and/or traditionally-detailed trim. Generally, windows shall be vertically-oriented, with the height greater than the width.

LYTLE CREEK RANCH
SPECIFIC PLAN

Entry:

If the entry is not covered, the front door shall be recessed a minimum of 12 inches.

Color Palette:

Light stucco body colors and white-washed brick shall be used with dark contrasting colors or white for trim and accent elements.

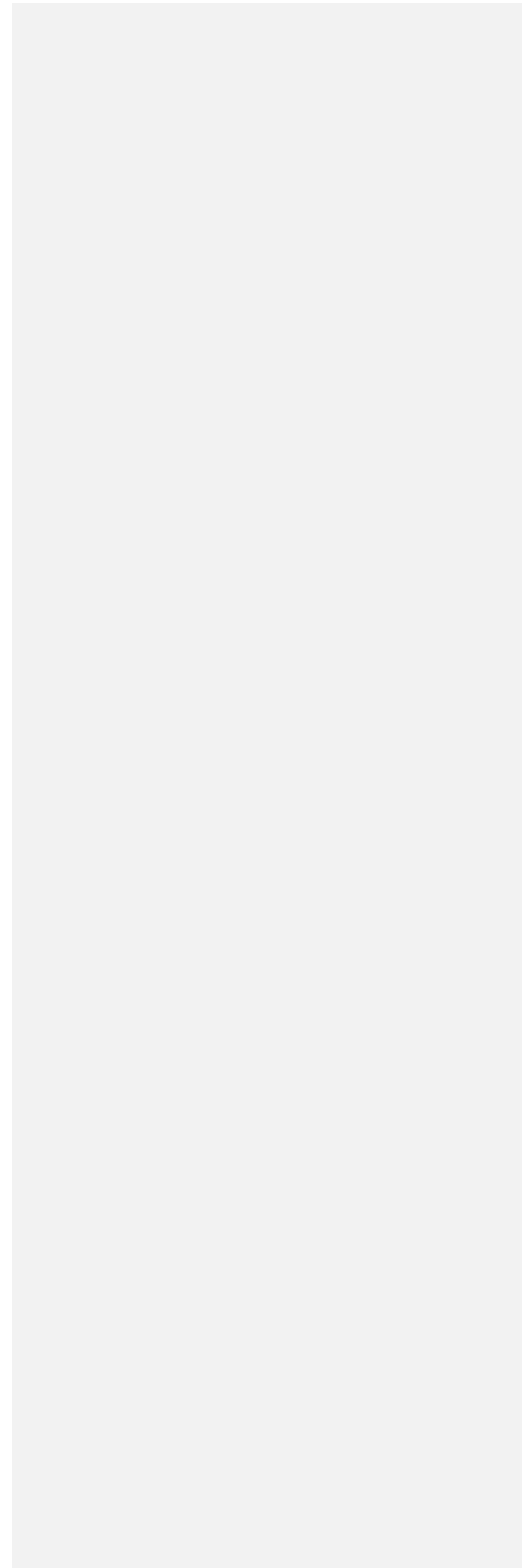
Porches:

A porch or veranda shall be incorporated below a cantilevered balcony.

Balconies:

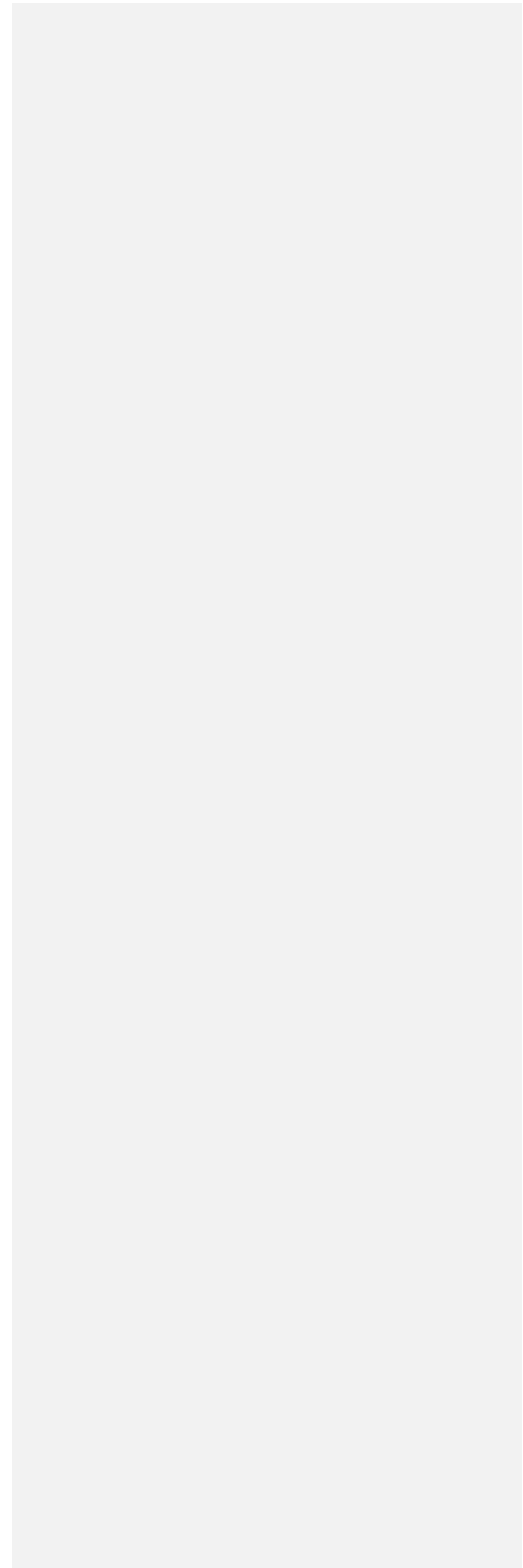
Cantilevered balconies extending along a portion of the façade shall be incorporated along the front elevation.

Figure 4-10
Examples of Monterey Architecture



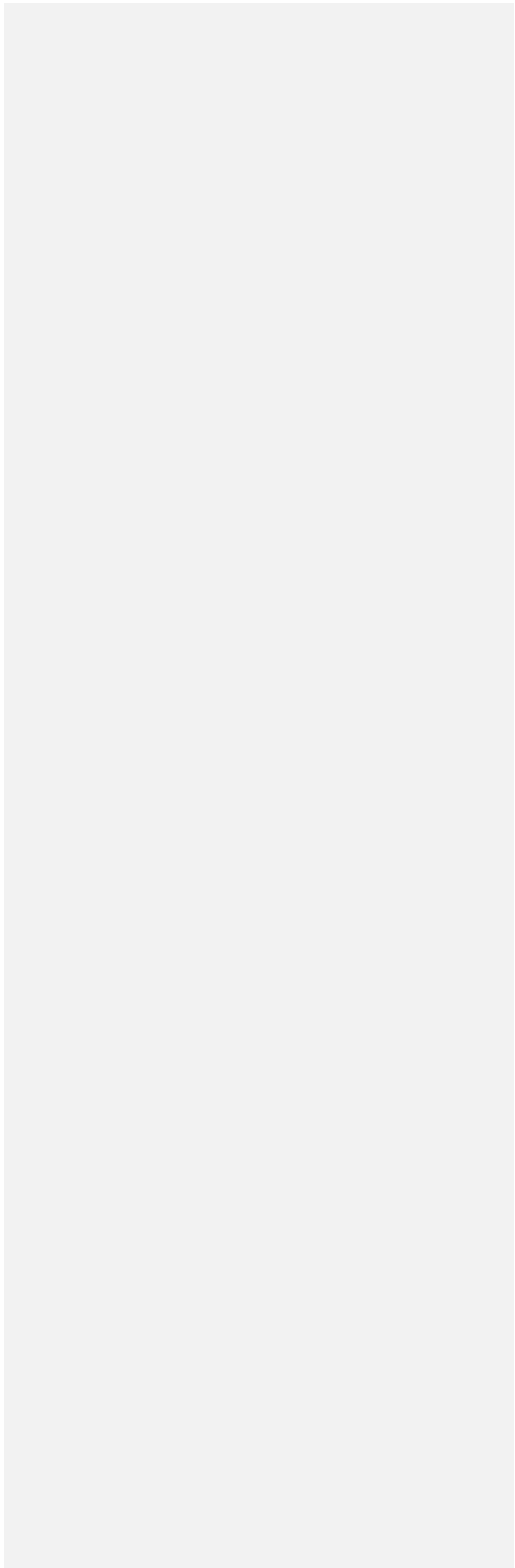
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Figure 4-11
Examples of Monterey Architecture



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Figure 4-12
Monterey Details



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E. SPANISH ECLECTIC

Historical Precedent

Because of the rich Spanish heritage of the early California settlers, along with the mild Mediterranean climate of the area, the Spanish Eclectic style of architecture was the preferred style of housing in Southern California during the early 1900's. The Spanish Eclectic style, also known as Spanish Colonial, is timeless and well suited to the Southern California lifestyle, and as a result has enjoyed several periods of renewed popularity throughout the years. The charm of this style lies in the directness, adaptability, and contrast of materials and textures.

The Spanish Eclectic style uses decorative details borrowed from the entire history of Spanish architecture. It is characterized by one-story and two-story building masses, which are often asymmetrical in form. Round or square towers are typical building accent forms, as are arched walkways leading to the front entrance or along a courtyard. Courtyards are typically simple with hanging pots, a flowering garden and sprawling shade trees. The roofs are typically low pitched with little eave overhang, and are covered with red "S" or barrel tile. Roof types commonly found in this style include side or cross-gabled, hipped, flat with parapet walls, or a combination of these. Use of stucco for walls, heavily textured wooden doors and highlighted ornamental ironwork are other architectural distinctions of this particular style.

Special features are used to further articulate and identify the Spanish Eclectic style of architecture. One or more prominent arches is/are commonly placed above a door or principal window, or beneath a porch roof. The entries are usually emphasized through the use of pilasters, columns, or patterned tiles, with the doors made of heavy wood panels. Many times there is one large focal window along the front facade, sometimes designed as a triple-arched shape. Decorative window grilles of wood or wrought iron are common, as are similar balustrades on porches and balconies.

Spanish Eclectic Design Characteristics

Spanish eclectic architectural offers many simple but distinct design features. The architecture is best understood by its design characteristics' significance on massing, scale, proportion, and building materials. These design characteristics are identified as:

- Exterior arches
- Round or square exterior columns
- Wrought iron accent gates
- Balcony railings
- Accent drain tiles
- Entry courtyard walls and gates
- Wooden front door
- Red barrel tile roofs

Further details of these design concepts are included as follows:

Roofs:

Roofing material shall consist of barrel tile or concrete 'S' tile in deep terracotta.

Overhangs:

Overhangs shall have tight rakes and 12" eaves with exposed rafter tails as an accent.

Siding materials:

Stucco or plaster. Wood should be limited to doors, shutters, and trim around windows and doors.

Windows:

Shutters may be used on front and side building elevations as accents. Windows will be trimmed out with fire-rated wood or stucco trim at the top and bottom of the window. On home product types, one or two accent windows may be recessed on the front elevation to create depth. The style of windows shall be compatible with the architectural style of the building. The use of many different styles of windows on one building plane shall be avoided. The size and proportion of panes shall correspond to the overall proportioning of the elevation. Although the use of mirrored or highly reflective glass is not permitted, energy efficient windows are encouraged.

Entry:

The entry shall be covered and be part of the porch and courtyard layout. The entry should be articulated as a focal point of the building's front elevation through appropriate usage of room elements, columns, porticos, recesses or projections, windows or other architectural features. Doors will be recessed and have stucco or wood trim surround along with articulated sidelights.

Color Palettes:

Wood/stucco trim may utilize a darker contrasting color, if desired. Typically, the stucco fascia will be similar in color to the main building. Where used, shutters will have more of a contrasting range with olives, aqua, blue, ochre, red, and other colorful accent colors.

Chimneys:

If provided, chimney will typically be constructed of sculptured stucco with an articulated cap detail. Exposed metal flues are not permitted.

Porches:

If provided, porches should be designed as an integral part of the front elevation to provide visual interest and should function as an extension of interior spaces. Porches may be accented by detailed columns, walls, and gates. Porches should be designed so as to not resemble a poorly conceived add-on element. Porch railings should compliment the building's architectural style.

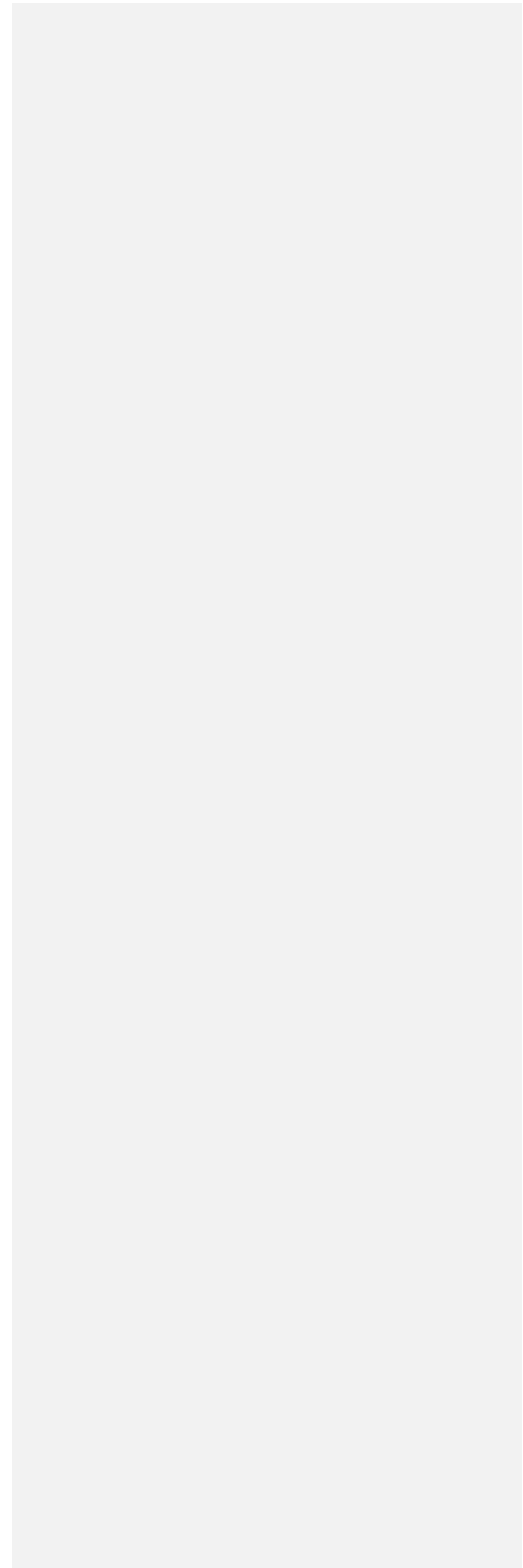
LYTLE CREEK RANCH
SPECIFIC PLAN

Balconies:

Balconies shall be an integral part of both the front and rear elevations with the traditional cantilevered massing on the front elevation. Balconies may be either functional or designed as decorative elements rather than usable balconies depending on the design intent of the architect. Balconies will project out over building planes to break up the front mass and be articulated with wood or wrought iron details.

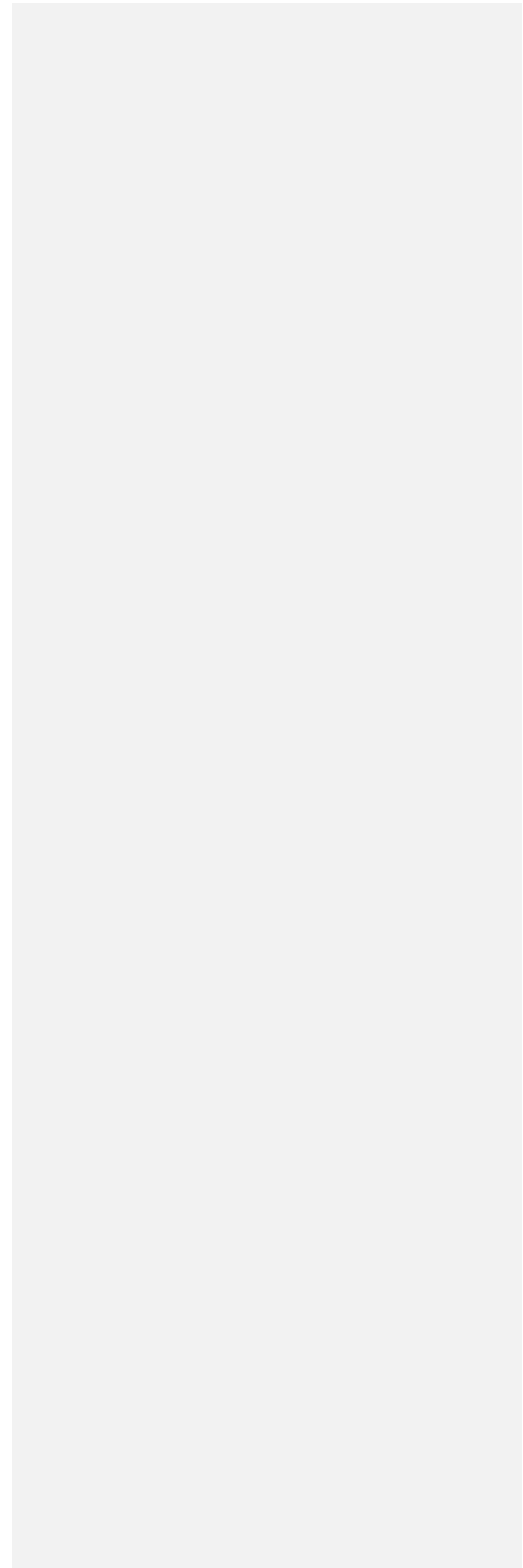
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Figure 4-13
Examples of Spanish Eclectic Architecture



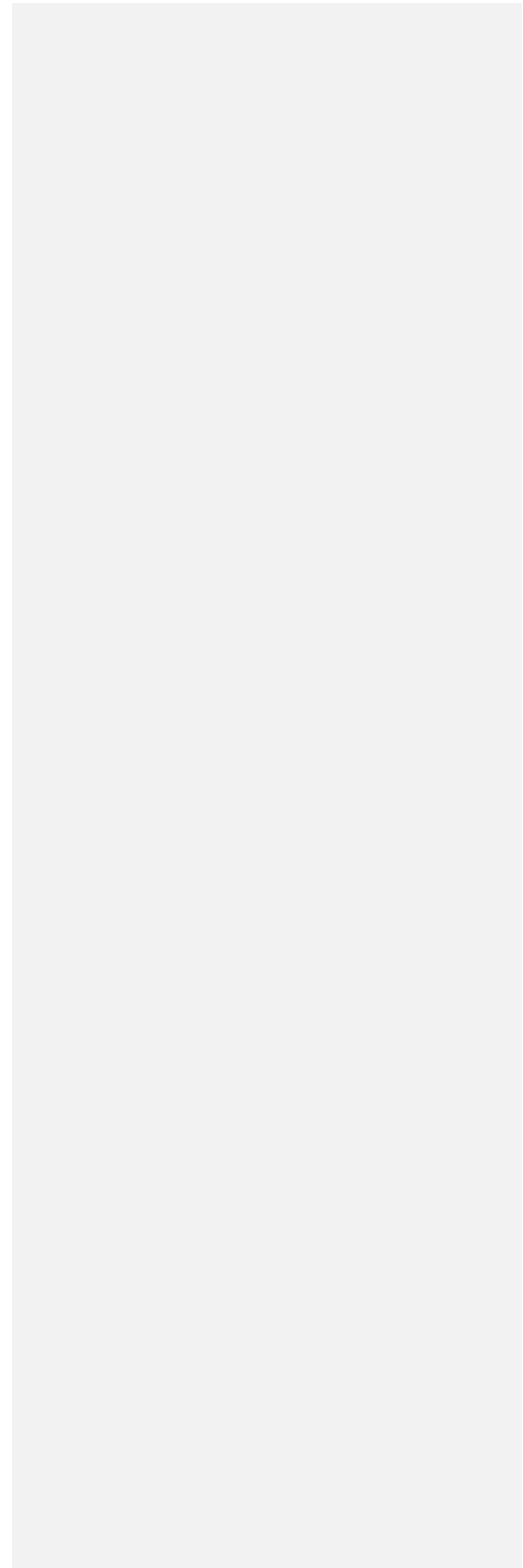
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Figure 4-14
Examples of Spanish Eclectic Architecture



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Figure 4-15
Spanish Eclectic Architecture Details



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F. TUSCAN

Historical Precedent

This style of architecture gradually developed with large homes in the countryside of Tuscany. It is known for its simplicity of design, with extensive use of materials such as stone, marble, and terracotta tiles. It is particularly suited to California's climate, with elements such as deep-set windows and outdoor spaces, and its popularity has grown in recent decades.

Design Characteristics

Tuscan architecture can be defined by the following features:

- Informal arrangement of building forms
- Mostly hipped roofs with occasional gable or cross-gable
- Predominantly barrel-tile roofs
- Rustic character through extensive stonework
- Rich earthy color tones
- Windows typically tall and narrow

Further details of these design concepts include:

Roofs:

Clay, concrete tile, or "S" tiles. Primarily hipped with secondary cross-gables or hips.

Overhangs:

Overhangs may vary in size and depth. Typical overhangs may extend from 12 to 24 inches.

Siding Materials:

Fieldstone or manufactured stone is typically used as an accent element on the front elevation with most of the façade being stucco.

Windows:

Windows shall include standard shutters or bermuda shutters.

At least one principal window treated in one of the following ways is required:

- Minimum 12" recess or surround
- Minimum 12" pot shelf with roof element and corbel
- Overhead trellis element projecting a minimum of 12"
- Decorative wrought iron window grille projecting forward of the wall plane.
- Full grid window mullion patterns

Entry:

If the front door is not covered, the front door shall have a minimum recess or surround of 12"

LYTLE CREEK RANCH
SPECIFIC PLAN

Color Palette:

Variety of rich “earthy” body colors with lighter or darker contrasting colors for trim and accent elements.

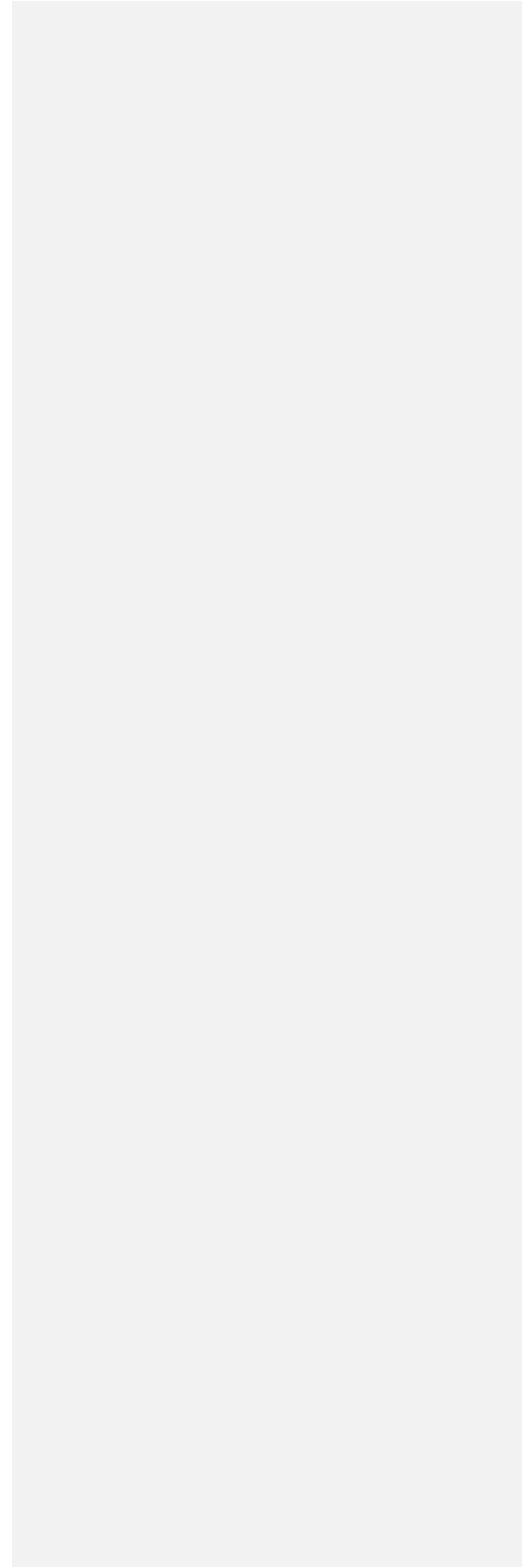
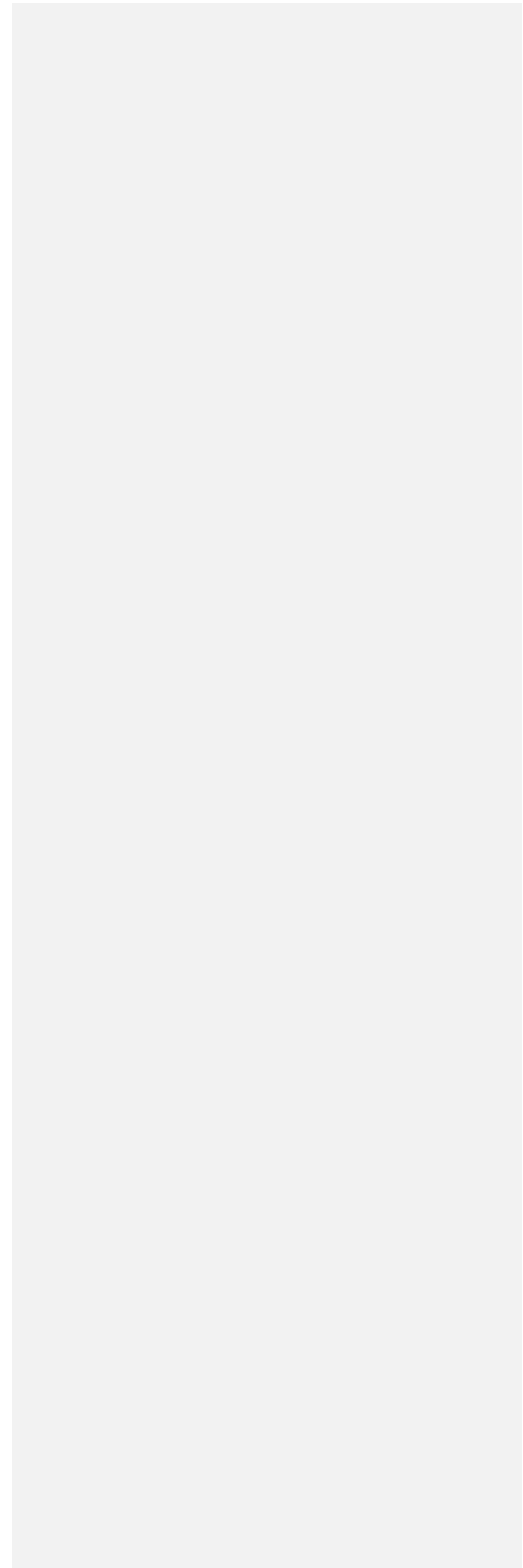
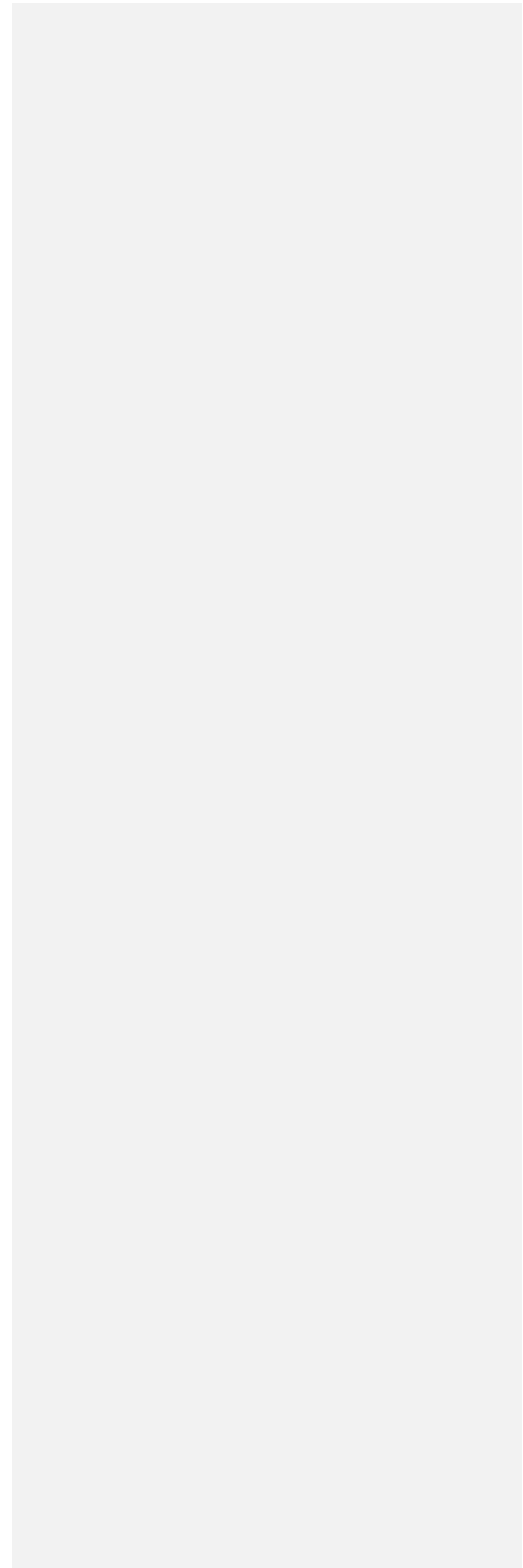


Figure 4-16
Examples of Tuscan Architecture



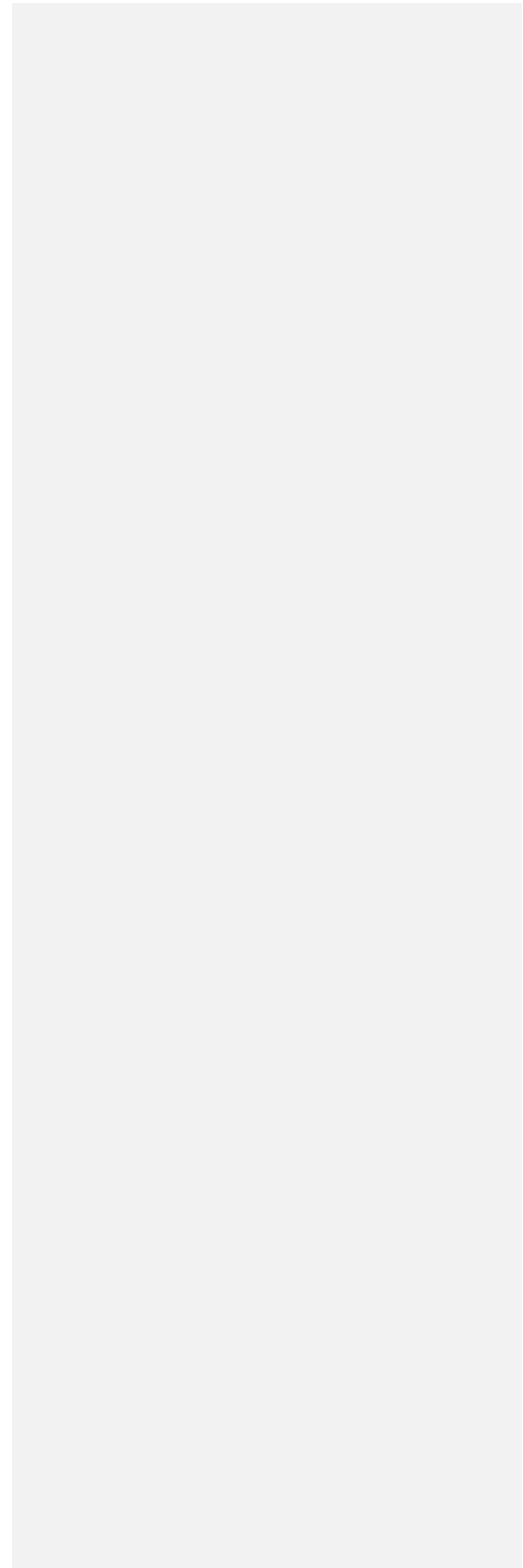
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Figure 4-17
Examples of Tuscan Architecture



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Figure 4-18
Tuscan Details



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G. ITALIANATE

Historical Precedent

This elegant, old-world style was meant to evoke the country villas of Tuscany and Umbria. The style remained popular through the second half of the 19th century. Italianate villas spread quickly from the Northeast throughout the Midwest to the West Coast. Homes are usually two or three stories in height.

Design Characteristics

Italianate architecture can be defined by the following features:

- Square and symmetric massing
- Almost flat, hipped roofs with deep overhanging eaves
- Incorporates corbels under the eaves
- Predominantly barrel-tile roofs
- Often incorporates a “belvedere,” or small tower, centered on the roof
- Incorporates wood, stone, or stucco
- Earth tone colors, beiges and tans with darker red and brown roofs
- Windows typically tall and narrow

Further details of these design concepts include:

Massing:

Houses are often simple, cubic building shapes.

Roofs:

Clay, concrete tile, or “S” tiles. Almost flat, hipped roofs with deep overhanging eaves.

Overhangs:

Overhangs may vary in size and depth. Typical overhangs may extend from 18 to 30 inches.

Siding Materials:

Stucco is used as primary material.

Windows:

Windows are usually formally and regularly spaced. Windows on first floor are tall and thin.

Doors:

Doors are often made of glass or wood. Sometimes the front entry will have double doors.

Balconies:

Where provided, balconies are usually constructed with wrought-iron railings or Renaissance balustrading.

LYTLE CREEK RANCH
SPECIFIC PLAN

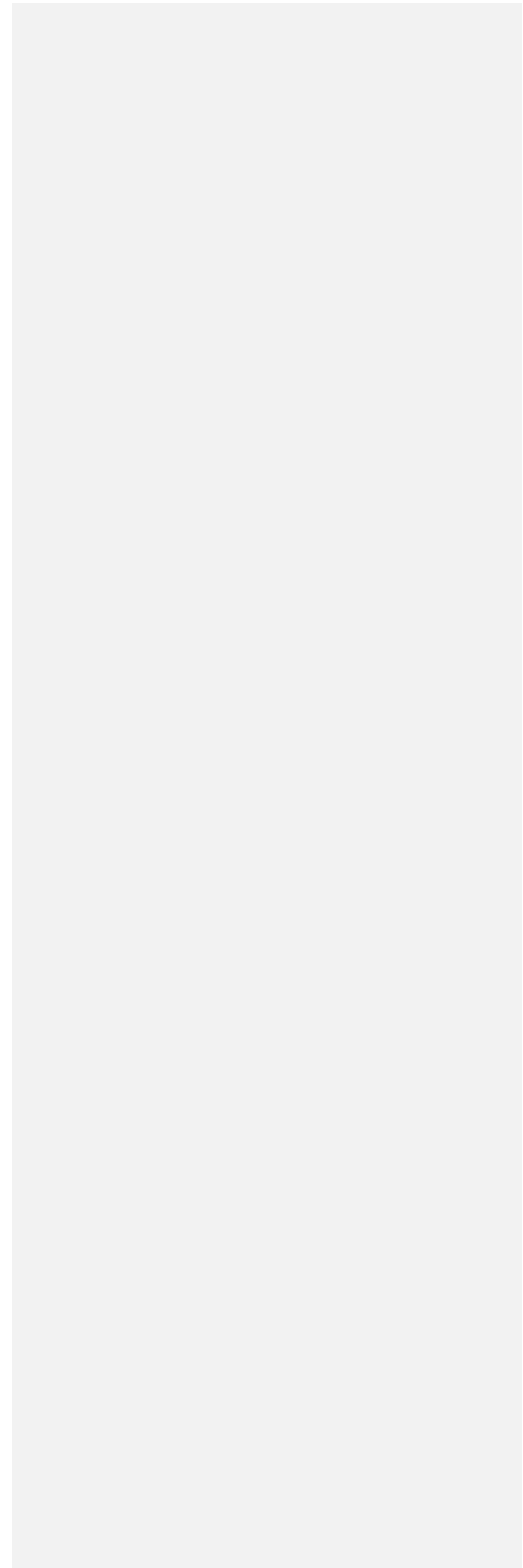
Entry:

If the front door is not covered, the front door shall have a minimum recess or surround of 12”.

Color Palette:

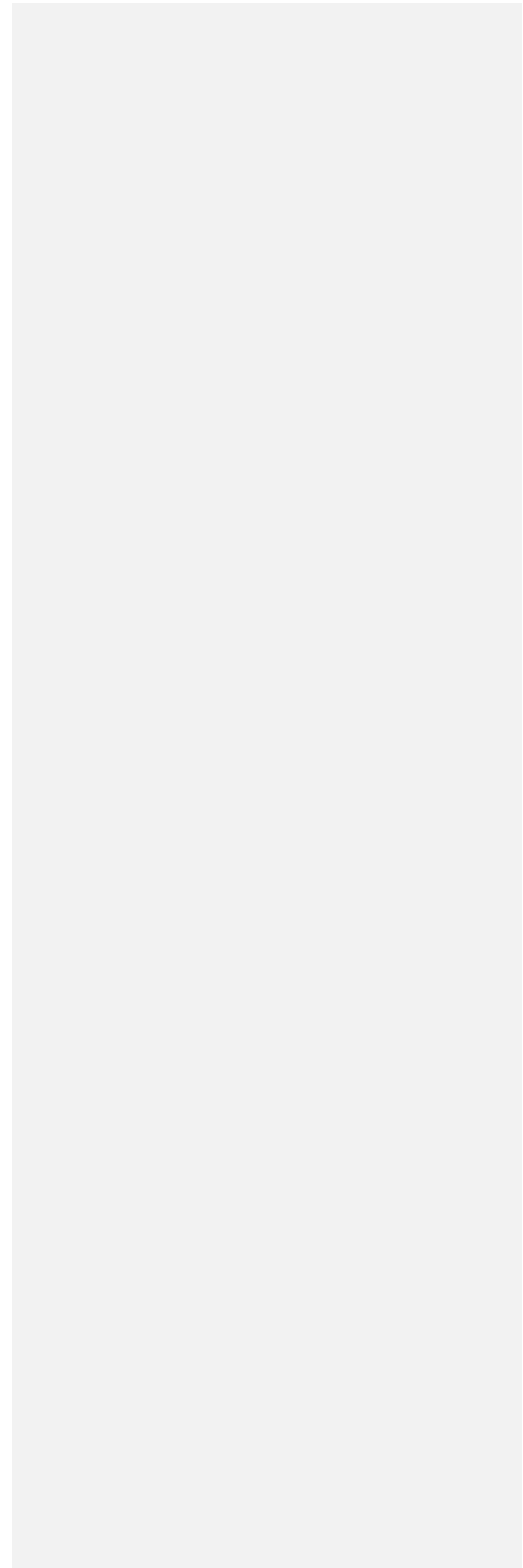
Variety of rich “earthy” body colors with lighter or darker contrasting colors for trim and accent elements.

Figure 4-19
Examples of Italianate Architecture



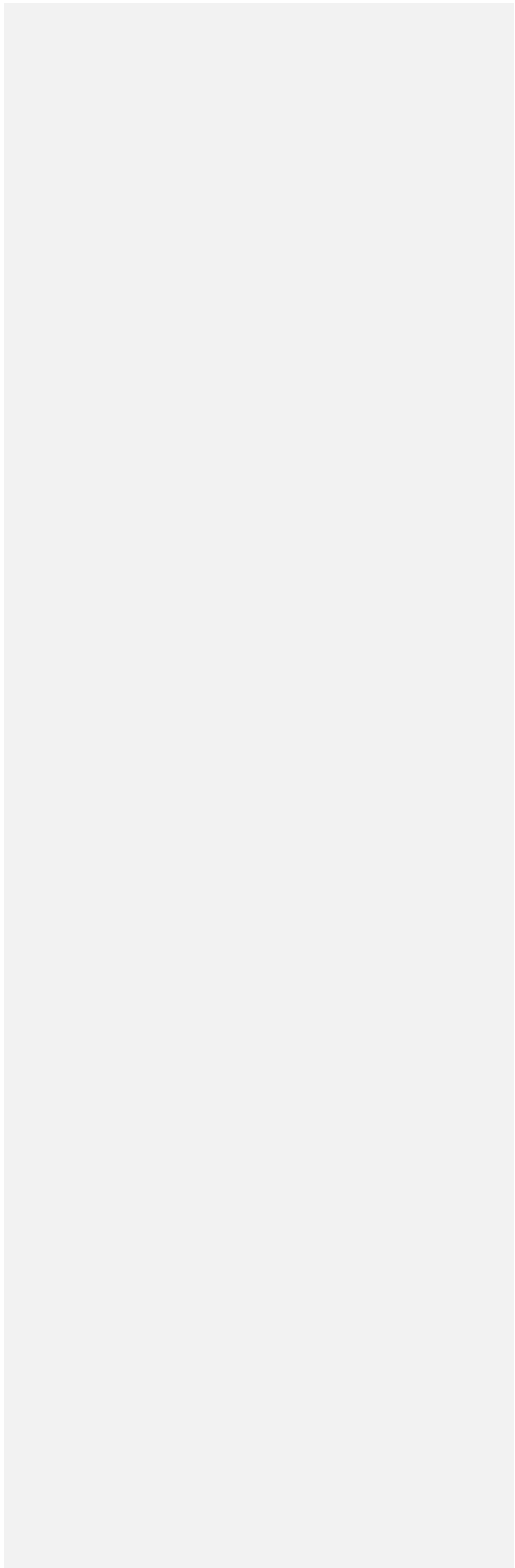
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Figure 4-20
Examples of Italianate Architecture



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Figure 4-21
Italianate Details



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4.2.2 APARTMENT HOMES

Lytle Creek Ranch will construct up to 4,325,989 apartment homes in the High Density Residential land use category on a total of approximately 45,433.4 acres. Each home will consist of one, two or three bedrooms and a garage. In addition, each home will be provided with energy saving appliances and private open space in the form of a patio, deck, or balcony. The photographs on the following pages depict the typical quality of construction anticipated for the apartments.



LYTLE CREEK RANCH
SPECIFIC PLAN



4.2.3 VILLAGE CENTER COMMERCIAL ARCHITECTURAL GUIDELINES

Purpose

Commercial/retail and office developments present certain architectural opportunities and limitations due to building massing, parking requirements, pedestrian and service access, and lighting. The design objective is to create an attractive Village Center Commercial environment, compatible in scale and aesthetics to the entire development. Some of the architectural styles selected for Lytle Creek Ranch will apply better to Village Center Commercial development than others. For example, Tuscan, Italianate, and Spanish Eclectic would be excellent architectural style choices for Village Center Commercial development in Lytle Creek Ranch. Styles like American Farmhouse, Monterey, Craftsman, and California Bungalow tend to work better on smaller structures with a more residential scale. The final determination as to which architectural style(s) to use for the Village Center Commercial areas will be determined by the master developer and/or builder(s).

Lists of general architectural guidelines for Village Center Commercial development within Lytle Creek Ranch are as follows:

Siting and Orientation

- A. Buildings should be designed using simple forms organized around a single element or group of elements. The objective is for these areas to be positively differentiated as quality Village Center Commercial environments.
- B. All design shall incorporate the combination of compatible architecture and landscape forms to ensure that this development achieves an image that is distinctive, clearly understandable, and unified.
- C. The architecture should incorporate elements of historic styles, while recognizing the needs of modern retail, commercial and office development.
- D. All designs shall appear as an integrated part of an overall site design concept. Details should be integrated into the building and not simply applied as an afterthought.
- E. To unify the site, common site design elements such as lighting and signage, enriched paving, and landscape treatments shall be required.
- F. Architectural design shall incorporate variations in front building elevations to avoid monotony and add distinctiveness to the building.
- G. Large complexes of buildings may be arranged to create and enclose a variety of outdoor spaces: plazas, squares, eating areas, usable open space, etc.

- H. To create visual interest, smaller buildings may vary in orientation from the larger buildings on-site and may be clustered to create areas of similar activities.
- I. Where feasible, guest and visitor parking should be located in proximity to main entrances. Employee parking and loading zones should be located further from the front of the building or on the side or rear of the buildings, and should be attractively screened from public streets with landscaping or other site design elements. Street parking shall be permitted.
- J. Vehicular and pedestrian circulation routes should be well separated and defined by landscape and site design elements.

Form, Scale, and Massing

- A. Buildings visible from the public right-of-way should be designed with articulated elevations (e.g., elevations with doors, windows, porches, balconies, dormers, trim and ~~mouldings~~moldings, roofline variations, or other architectural features), and with clearly defined entries.
- B. Scale and massing should be given careful consideration. Long, uninterrupted expanses of walls are discouraged.
- C. Interconnection and lapping of building forms and heights to break up long expanses of blank walls help relieve monotony and are desirable.
- D. On smaller “pad” buildings, all building sides should be treated architecturally.
- E. Buildings should provide architectural and decorative enhancements at main building entrances.

Architectural Features and Details

- A. Fixtures and finishes should be selected for their contribution to the overall theme of the development.
- B. Medium or high performance glass is preferred for use on Village Center Commercial buildings.
- C. At key locations on certain Village Center Commercial buildings, architectural elements such as towers, domes, cupolas, arcades, trellis structures, and other design elements may be incorporated to enhance the building architecture and create a “sense of place.”

Roof Forms and Materials

Roof forms and materials should reflect the selected architectural theme(s). Roofs should be designed to minimize the appearance of “tacked on” features. Flat roofs are permitted in all Village Center Commercial planning areas.

Details, Materials, and Colors

- A. Materials shall be durable, relatively maintenance free, and sympathetic in scale and aesthetics to the overall theme of the Village Center Commercial development.
- B. Building colors and materials should relate to the selected architectural theme(s).
- C. Limited use of brick or stone (real or manufactured) is permitted at key locations on buildings such as at building entrances or on arcades or tower elements.
- D. In general, building finishes should be non reflective.
- E. Street and plaza furniture within the Village Center Commercial development shall be designed to coordinate in design, style, and color with the principal architectural themes and/or architectural details of the primary structure(s) and building(s) in the development.

Walls and Fences

Decorative walls and/or walls screening yards, parking lot or enclosures shall be designed to integrate with the architecture of the building, as well as the landscape design.

Accessory Structures and Services

Any accessory buildings and/or enclosures, whether attached to the main building or not, shall be of similar design and materials.

Lighting

- A. Use of low, shielded walkway lighting.
- B. Incorporate energy-saving light fixtures, where feasible.
- C. Screen site lighting from direct view by adjacent residential uses.

Mechanical Equipment

- A. Use parapets or other architectural elements to screen rooftop equipment from ground level views.
- B. Disperse rooftop mechanical equipment on larger buildings and paint equipment to match rooftop. Roof-mounted mechanical equipment shall not be visible from ground level views.
- C. When screening mechanical equipment use screening materials similar or complementary to the external materials used in the building architecture.

4.3 LANDSCAPE DESIGN GUIDELINES

This section of the Specific Plan identifies the landscape design guidelines for Lytle Creek Ranch. All required landscape plans must be prepared by a licensed landscape architect.

4.3.1 MASTER LANDSCAPE PLAN DESCRIPTION

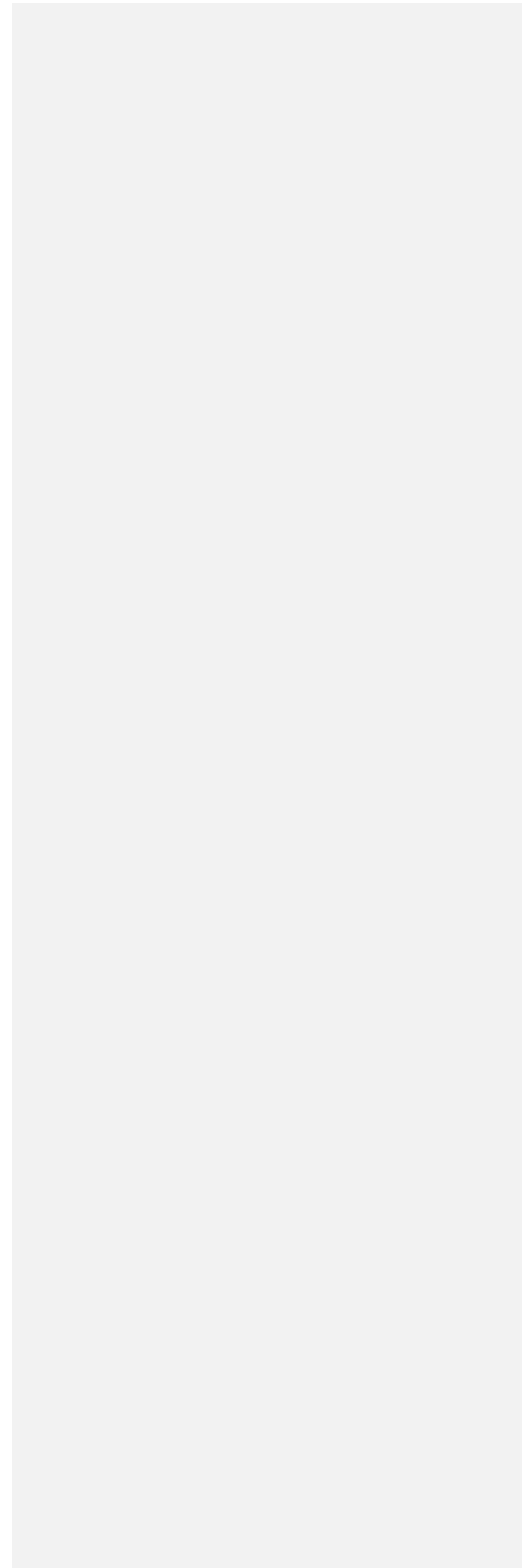
Figures 4-22 through 4-26 depict the Conceptual Master Landscape Plan for Lytle Creek Ranch. The Conceptual Landscape Plan depicts the location of “Welcome to Rialto” signage, community entries, and streetscape treatments. Special streetscape treatments for the primary streets within Lytle Creek Ranch are contained in this Specific Plan for the following streets:

- Riverside Avenue
- Country Club Drive (off-site)
- Country Club Drive (on-site)
- Entry Streets (Neighborhood II)
- Entry Streets (Neighborhood III)
- Collector Street (Neighborhood III)

Figure 4-22
Conceptual Master Landscape Plan – Neighborhoods I and IV

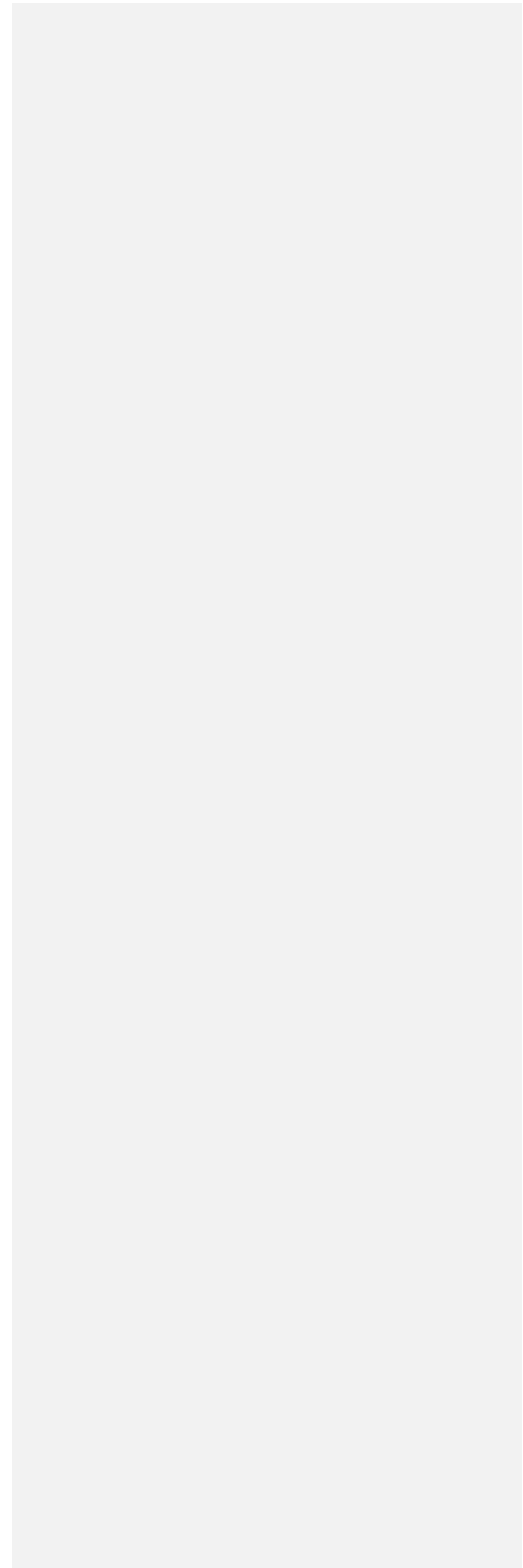
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Figure 4-23
Conceptual Master Landscape Plan – Neighborhood II-a



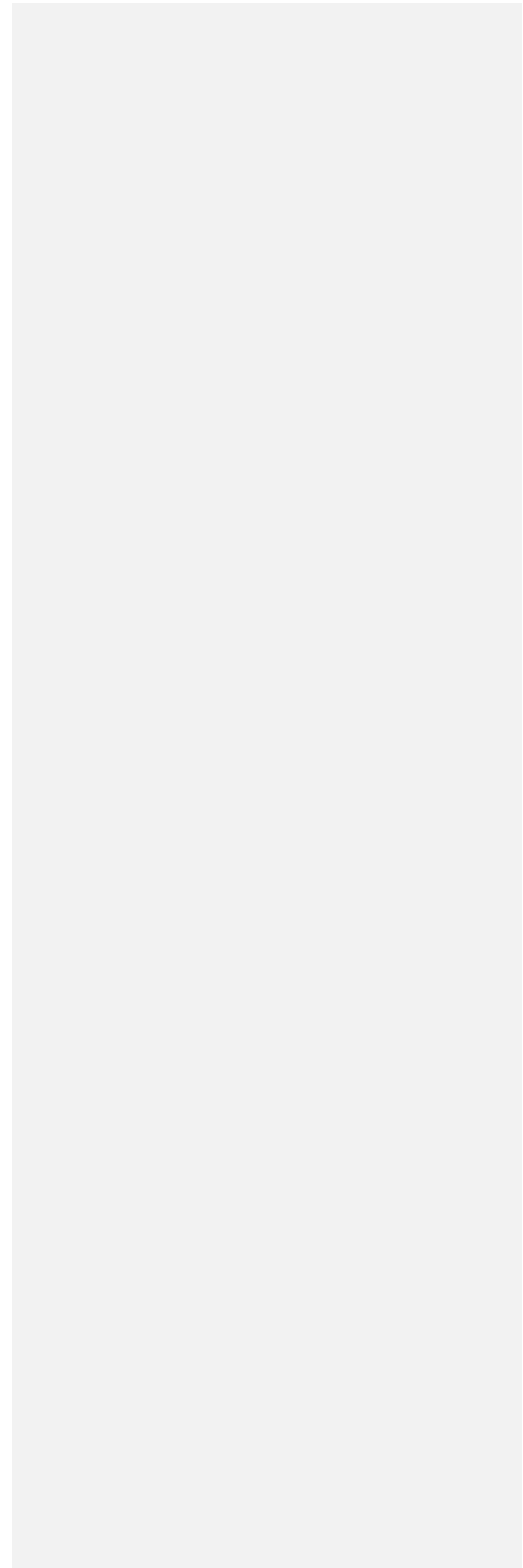
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Figure 4-24
Conceptual Master Landscape Plan – Neighborhood II-b



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Figure 4-25
Conceptual Master Landscape Plan – Neighborhood III-a

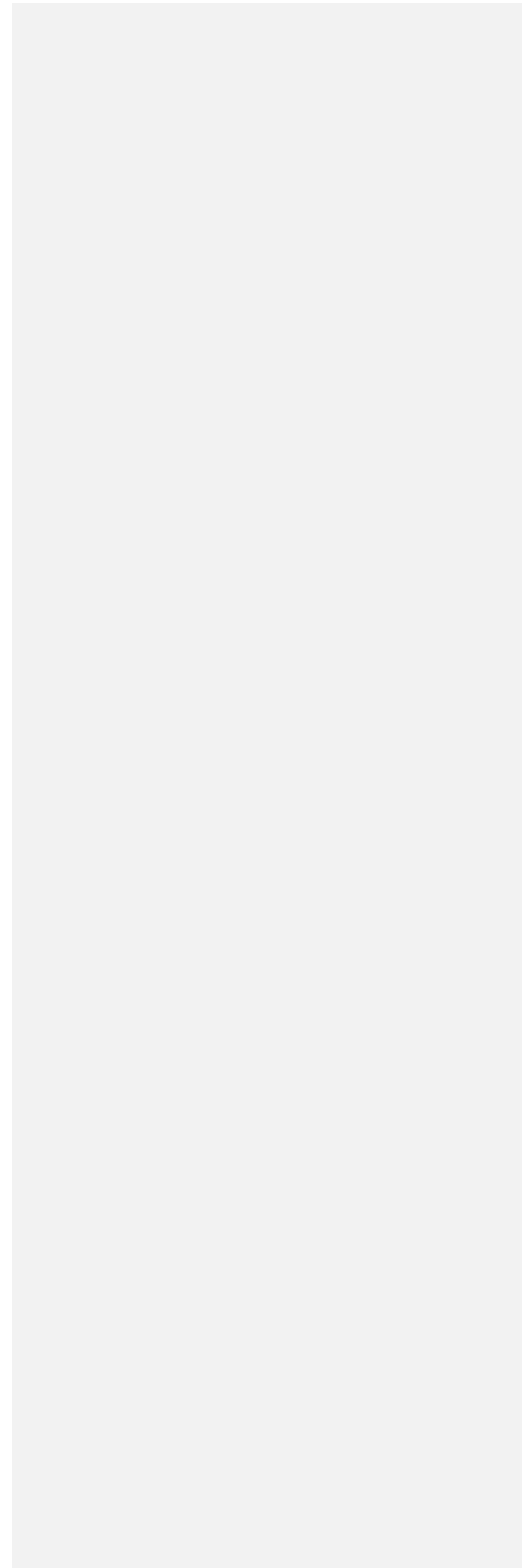


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Figure 4-26
Conceptual Master Landscape Plan – Neighborhood III-b

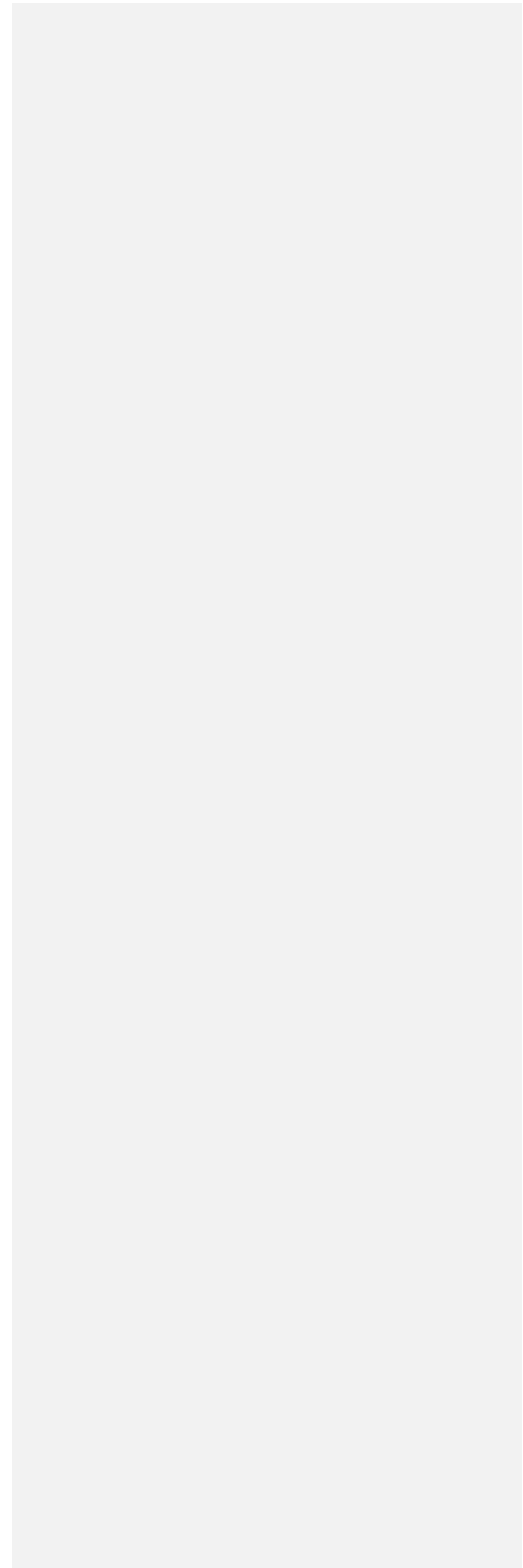
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Figure 4-27
Conceptual Riverside Avenue



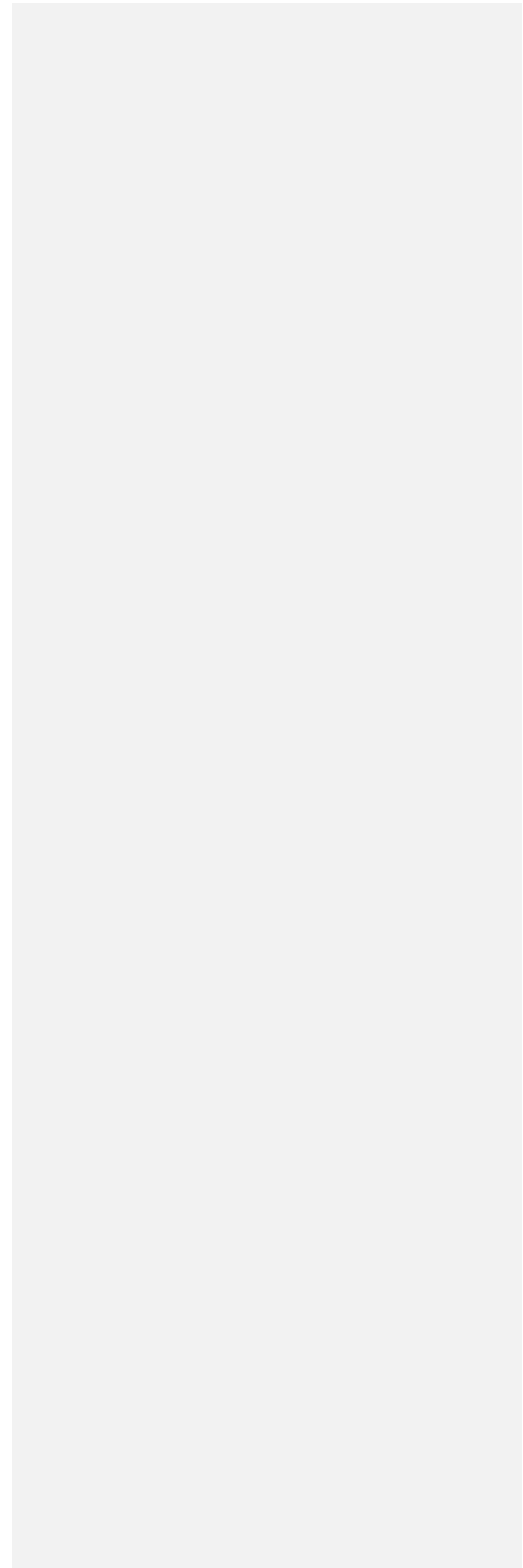
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Figure 4-28
Conceptual Country Club Drive (off-site)



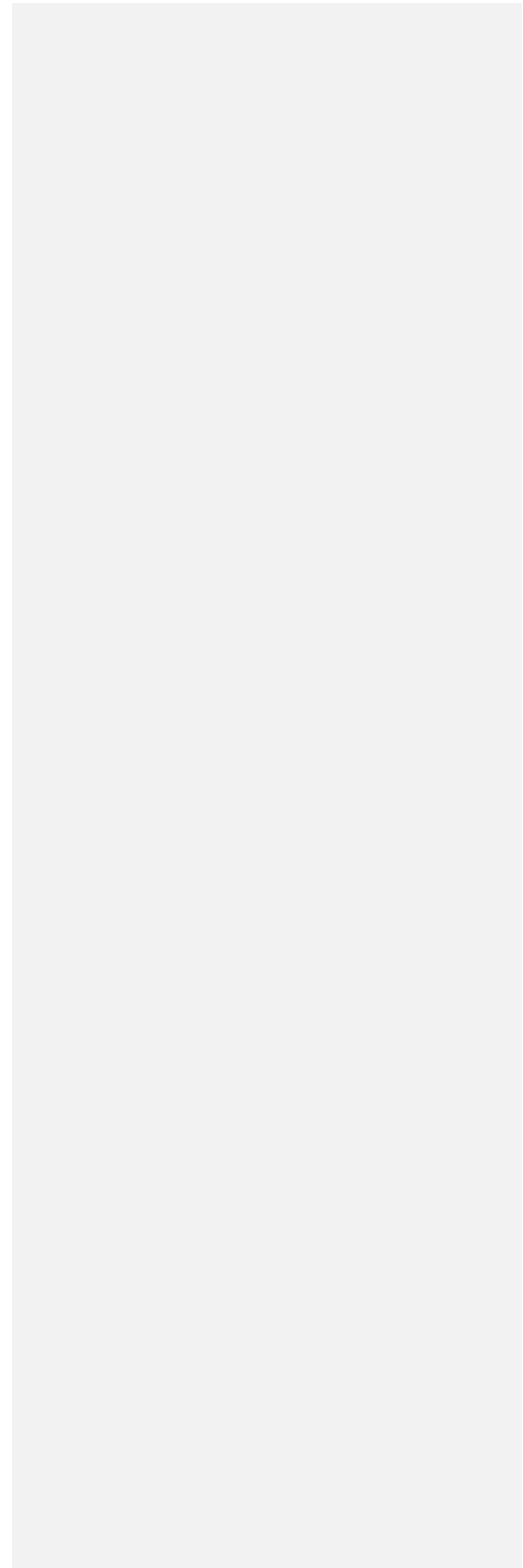
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Figure 4-29
Conceptual Country Club Drive (on-site)



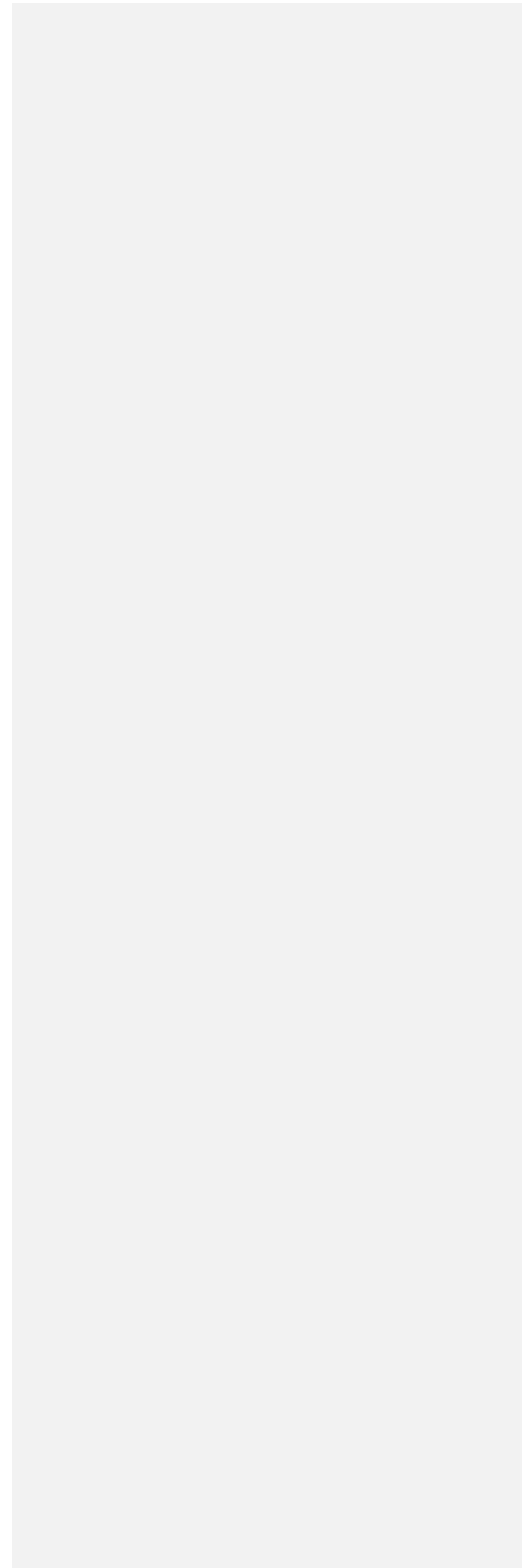
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Figure 4-30
Conceptual Entry Street – Neighborhood II



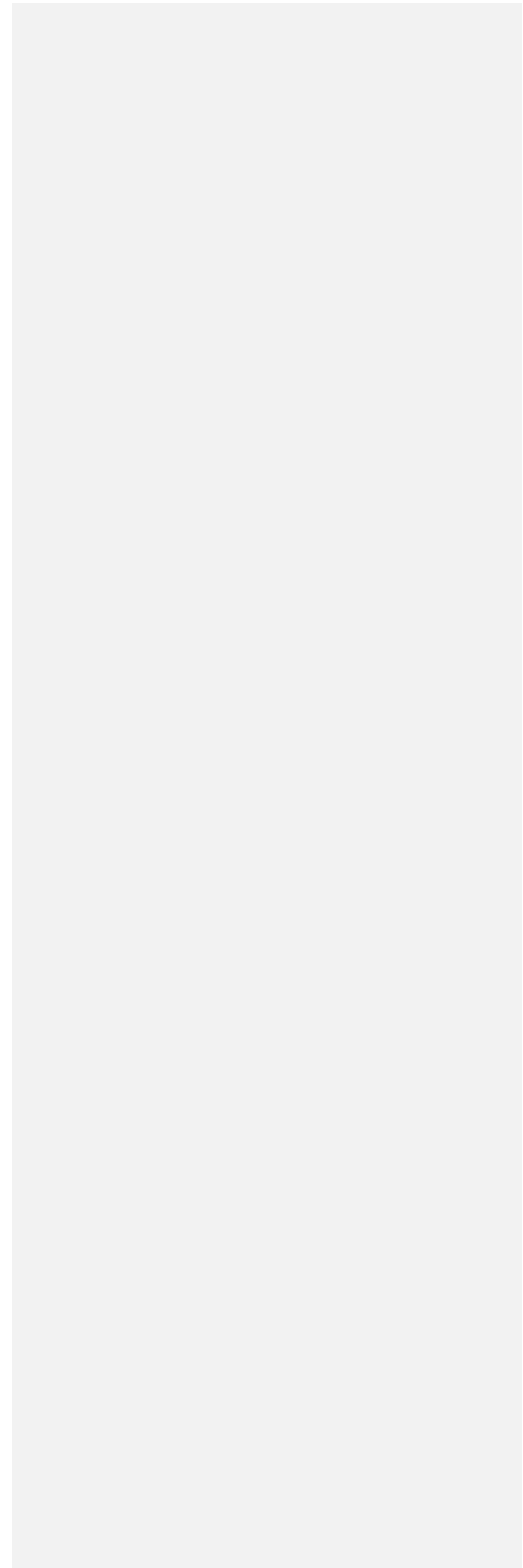
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Figure 4-31
Conceptual Entry Street – Neighborhood III



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Figure 4-32
Conceptual Collector Road – Neighborhood III



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4.3.2 ENTRY MONUMENTATION

City of Rialto entry monuments and features, community entries, neighborhood entries, and Village Center Commercial entries will consist of a thematic blend of construction features, landscaping, signage, and specialty lighting, which will serve as area landmarks, while reinforcing the distinctiveness of Lytle Creek Ranch. City of Rialto entry monuments will be owned and maintained by the City. All project entry monumentation will be privately maintained and located outside of all City maintained areas and the public rights-of-way.

A hierarchy of community theme entries are planned and will consist of the following:

- Major Community Entries
- Residential Planning Area Entries
- Village Center Commercial Entries

Because the design of the neighborhood entries and the Village Center Commercial entries will vary for each planning area, only the designs for the community entries are identified in this Specific Plan.

Northern City Gateway Feature (City of Rialto “Bridge” Structure)

Lytle Creek Ranch proposes a gateway element within the street right-of-way near the intersection of Sierra Avenue and Riverside Avenue. This entry feature will contain a simplified, modern interpretation of the Rialto Bridge, an existing historic bridge located in the city of Venice, Italy, and which appears on the City of Rialto’s seal. A conceptual illustration of the bridge-like structure is depicted in Figure 4-33. This structure will be designed to allow pedestrians to actually walk over the bridge. This “bridge-like” structure is not intended to be an exact replica of the Rialto Bridge, but should be designed to incorporate massing and styling similar to the existing bridge in Venice, Italy. This structure will function as the City’s northern gateway feature and will incorporate the words “Rialto” or “City of Rialto.” In addition, the City’s seal may be installed on the bridge. The final design of this gateway feature shall be determined by the Lytle Creek Ranch project master developer. In no event, shall this gateway feature exceed a height of fifteen (15) feet.

City of Rialto Monument Signs (See Figure 4-34)

A Rialto monument sign shall be installed in Neighborhood I near to the I-15 Freeway/Glen Helen Parkway interchange. This sign will include the wording, “Welcome to the City of Rialto,” as well as a version of the City of Rialto’s seal. The base of the wall either consist of real stone or manufactured stone or cobbles, and the remainder of the sign will be constructed of precast concrete. Cast lettering will be pinned to the sign. The sign will be accented by shrubs and groundcover plantings.

Commented [JG1]: Are you going to have a second monument anywhere else?

Community Entries (See Figure 4-35)

Community entries are planned at several locations within Lytle Creek Ranch including at Riverside Avenue/Redwood Avenue, Riverside Avenue/N. Live Oak Avenue, Riverside Avenue/N. Alder

Avenue, Riverside Avenue/N. Locust Avenue, and Riverside Avenue/Linden Avenue intersections. Each entry will contain a raised median containing landscaping and a large specimen tree. On either side of the entry there will be low stone retaining walls, wooden “ranch” fencing, perimeter theme wall fencing, and clusters of trees. There will also be limited turf areas. Each entry will contain enhanced paving on the entry street to help create a sense of arrival. Community signage containing the community name and logo will be provided at each entry.

Gated Entries (Neighborhood II) (See Figure 4-36)

At the discretion of the master developer, Neighborhood II may have gated entries. Gates may be card-operated or controlled by a guard. Each entry will contain a raised median containing landscaping and a guardhouse. On either side of the entry there will be stone walls and clusters of trees. A monument pilaster will be placed in the median adjacent to the gates, marking a formal entry into the project. The roadway will be expanded at each entry to permit vehicle u-turns, and enhanced paving will be utilized on the entry street to help create a sense of arrival. Community signage containing the community name and logo will be provided at each entry. Unrestricted pedestrian access will be available along the adjacent sidewalk.

Residential Planning Area Entries

At the discretion of the project master developer, each residential planning area may contain signage. Where provided, this signage will identify the name of the development within the planning area. The intent of this Specific Plan is to allow flexibility in the design of these residential planning area entries in order to create interest and promote diversity. Signs shall conform to the City of Rialto signage standards, which are contained in Section 18.102 of the City’s Zoning Code.

Village Center Commercial Entries

Lytle Creek Ranch Village Center Commercial entry monuments occur at key entrances into the Village Center Commercial planning areas. These entries will reinforce the overall landscape concept of Lytle Creek Ranch. Each Village Center Commercial entry monument will be designed by the master developer and/or builder(s) and submitted to the City for Design Review.

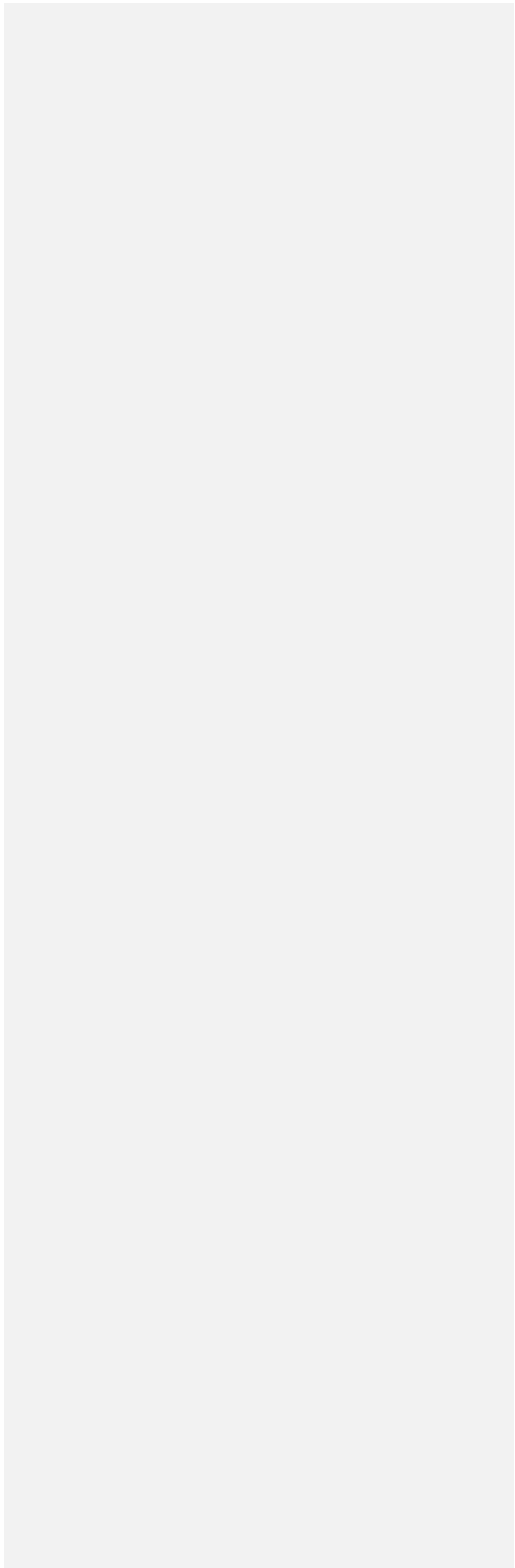
Roundabouts (See Figure 4-37)

Two landscaped roundabouts are included in Neighborhood II, and three are planned in Neighborhood III. These roundabouts will serve as important iconic elements that will help to create a unique identity for the project circulation system. River rock and enhanced concrete paving will be utilized. To keep maintenance requirements to a minimum, each roundabout will contain trees and plant materials, while minimizing the use of turf and other high-maintenance plantings. A low stone veneer planter wall and large specimen tree will be placed in the center of the roundabout. To help prevent distractions to vehicular traffic circulating through the roundabouts, no potentially distracting features such as fountains, sculptures, community signage, or other similar elements will be permitted within the central island in each of the roundabouts. Traffic-related signage shall be permitted as needed anywhere within the roundabouts.

Figure 4-33
Conceptual City of Rialto Monument Signs

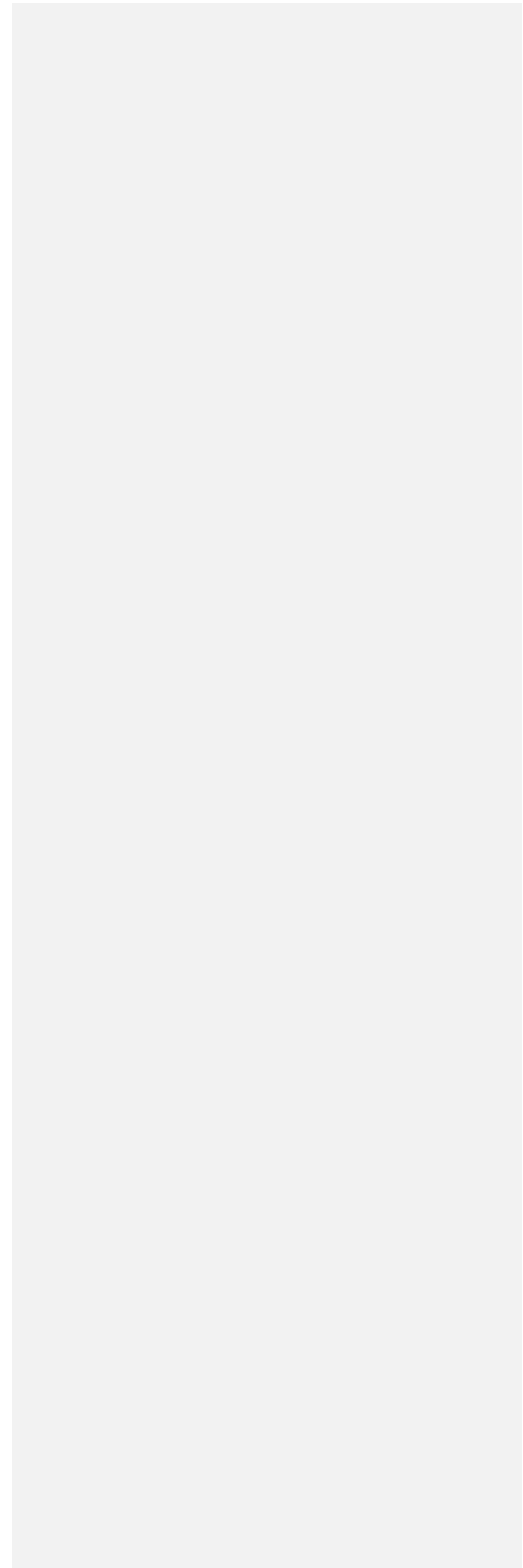
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Figure 4-34
Conceptual Community Entries



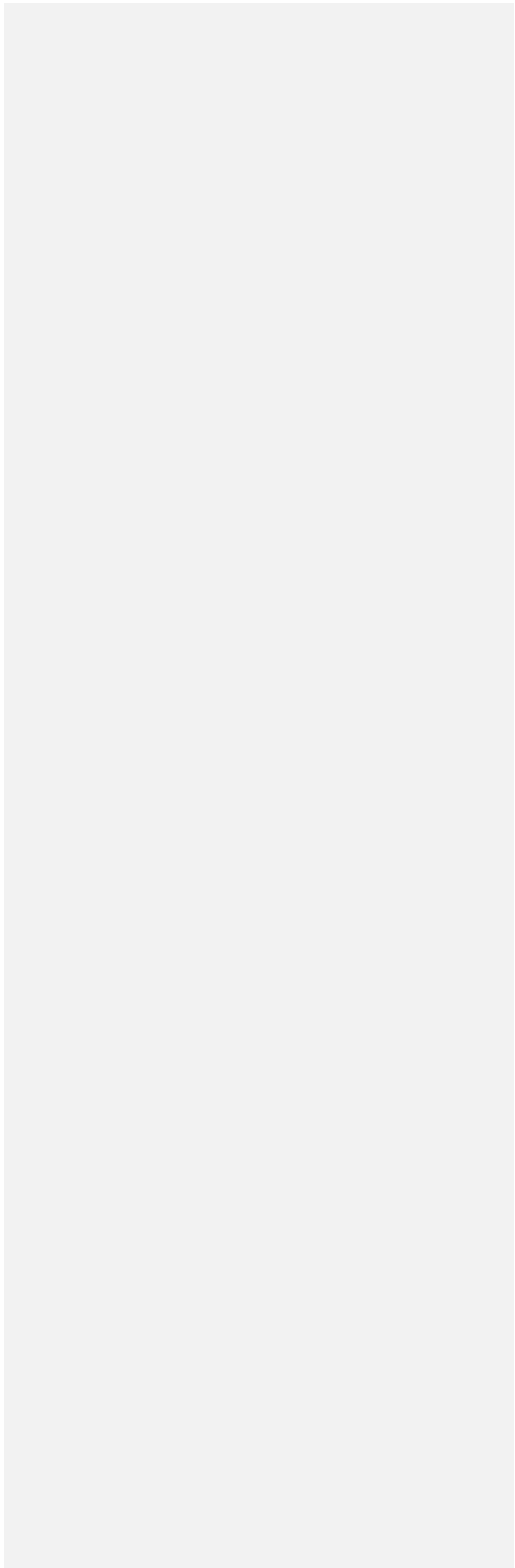
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Figure 4-35
Conceptual Gated Entries at Neighborhood II



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Figure 4-36
Conceptual Roundabouts



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4.3.3 PLANT MATERIALS GUIDELINES

In general, landscaping within Lytle Creek Ranch will be designed to reflect the area's historic agrarian theme, as well as the native vegetation and habitat of Lytle Creek. This Specific Plan restricts landscape plantings to non-invasive plant species for common areas adjacent to open space in order to minimize potential indirect effects to vegetation within these open space areas. Where appropriate, landscaping will consist of drought-tolerant, native species.

It is the intent of these guidelines to provide flexibility and diversity in plant material selection, while maintaining a cohesive plant palette in order to give greater unity and thematic identity to the community. The plant material lists have been selected for their appropriateness to the project theme, climatic conditions, soil conditions and concern for maintenance. Wherever possible, overall plant material selection for given project areas, will have compatible drought resistant or water wise characteristics. Irrigation programming can then be designed to minimize water application for the entire landscape setting. Plants used are to be reviewed and approved by the City during the conceptual drawing phase prior to preparing construction drawings. Plant installation will be provided per City standards.

In creating planting plans for Lytle Creek Ranch, consideration should be given to selecting plant materials for their color, texture, form (shape), and size (fine, medium, coarse) characteristics. At least one of the inherent characteristics should remain constant in each planting area to avoid a haphazard appearance to the plants and promote a sense of unity. For example, if a planting area contains a mixture of colored plants of various sizes and shapes, the textures of the plant materials used in this area should all be consistent to ensure some consistency to the plantings.

4.3.4 DROUGHT-TOLERANT PLANT MATERIALS AND WATER CONSERVATION

The conservation and efficient use of water and the use of drought tolerant and native plant materials is important to the landscape design for Lytle Creek Ranch. The following landscape standards shall apply to new development within Lytle Creek North, where appropriate. The following landscape standards shall apply to all new development:

- A. All landscaped areas shall be provided with automatic irrigation and shall be maintained at all times.
- B. Irrigated turf areas shall not exceed 40 percent of the each planning area or development area's total (parks, playgrounds, recreation areas, schools, and private residential lots are excluded from this requirement); provided, however, that the Planning Commission may allow larger turf areas, where special water conservation measures are implemented.
- C. Irrigated turf areas shall not exceed 60 percent of each private residential lot or pad.

- D. Consider using water saving turf varieties or turf substitutes (e.g., ground cover), where appropriate.
- E. Turf shall not be used in narrow planters, on raised planters, and other relatively small planters.
- F. Turf planting on slopes exceeding 15 percent causes excessive irrigation runoff and shall not be allowed.
- G. Plants shall be selected appropriate according to their suitability to experience the hot, dry Inland Empire climate.
- H. Protection and preservation of native species in natural open space areas is encouraged.
- I. "Gray water" or recycled water should be used for irrigation purposes, as available and as feasible.
- J. Plant selection should incorporate use of "water wise" plant materials, where feasible.
- K. Most plants need to be irrigated to survive and look their best. Even "water wise" plant materials require regular water to become established. An appropriate irrigation system might include sprinklers, bubblers, a drip system and hose bibs, for example. The system must be designed for efficient conservative use of water.

4.3.5 PLANT PALETTE

Below is a list of approved plant materials for use within the Lytle Creek Ranch Specific Plan area. Additional plant materials not listed below may be allowed by the City's Development Services Department on a case-by-case basis during review of the Precise Plan of Design. Final approval of plants in City-maintained Landscape Maintenance Districts (LMDs) shall be subject to approval by the Department of Public Works. Turf is permitted in all areas of Lytle Creek Ranch.

Botanical name

Common name

TREES

Liquidambar styraciflua

Schinus molle

Alnus rhombifolia

Platanus racemosa

Lagerstroemia indica

Prunus cerasifera K. Vesuvius

Platanus acerifolia

Rhus lancea

Eriobotrya deflexa

Sweet gum

California pepper

White alder

California sycamore

Crape myrtle

Purple leaf plum

London plane tree

African sumac

Bronze loquat

Botanical name

Common name

TREES (continued)

<i>Populus fremontii</i>	Western cottonwood
<i>Robinia idahoensis</i>	Idaho locust
<i>Koelreuteria paniculata</i>	Goldenrain tree
<i>Pinus halepensis</i>	Aleppo pine
<i>Pinus canariensis</i>	Canary Island pine
<i>Pinus eldarica</i>	Afghan pine
<i>Pinus radiata</i>	Monterey pine
<i>Pyrus calleryana 'aristocrat'</i>	Evergreen pear
<i>Quercus ilex</i>	Holly oak
<i>Quercus agrifolia</i>	Coast live oak
<i>Tristania conferta</i>	Brisbane box

SHRUBS

<i>Pittosporum tobira</i>	Tobira, Green, Variegated, Wheelers Dwf.
<i>Rhapiolepis spp.</i>	India hawthorn
<i>Trachelospermum jasminoides</i>	Star jasmine
<i>Dietes bicolor</i>	African iris
<i>Hemerocallis hybrid</i>	Daylily, several colors
<i>Pyracantha Santa Cruz</i>	Firethorn low growing
<i>Nandina domestica</i>	Heavenly bamboo, low and regular
<i>Xylosma congestum</i>	Xylosma, regular and low growing
<i>Ligustrum texanum</i>	Texas privet
<i>Viburnum tinus</i>	Laurastinus
<i>Photinia fraseri</i>	Photinia
<i>Agapanthus africanus</i>	Lily of the Nile, white and blue
<i>Leucophyllum frutescens</i>	Texas sage, 'White cloud'
<i>Cistus purpureus</i>	Orchid rockrose
<i>Rosa spp.</i>	Roses as ground cover and low shrubs
<i>Rosemarinus prostratus</i>	Rosemary as ground cover
<i>Euonymus japonicus variagata</i>	Variegated euonymus
<i>Escallonia fradesi</i>	Escallonia
<i>Myoporum pacificum</i>	Myoporum to 2 ft high

GROUND COVERS

<i>Myoporum parvifolium</i>	Myoporum 3" to 6" high
<i>Hedera helix</i>	Hahn's ivy

VINES

<i>Rosa Banksii Alba</i>	White climbing rose
<i>Bignonia spp.</i>	Lavender, yellow, red vines

4.3.6 PLANTING SCHEDULE

The installation of plant materials during the coldest winter months (December through March) and the hottest summer/fall months (July through September) can be difficult and should be avoided to

the extent feasible. Container plant materials not acclimated to the area can easily suffer from damage or sun exposure resulting in partial or entire foliage loss, even though such materials are perfectly suited to the temperature ranges once established.

4.3.7 PLANTING GUIDELINES

Planting areas must be integrated into each development area and with the design of the buildings. Plant materials should be selected to enhance the appearance and enjoyment of the project and soften the effect of the buildings and paving. Landscaping should consist of a mix of trees, shrubs, ground cover, and turf.

30,000 Trees Planted

A total of 30,000 trees (minimum 15 gallon) will be being planted within the Lytle Creek Ranch Specific Plan area. These trees will be provided as follows:

- A. A minimum of two (2) tree saplings will be offered to each single family residence by the Master Developer.
- B. The remaining trees will be planted within parkways, parks and recreation areas, school sites and joint-use sites, the Grand Paseo, the golf course and clubhouse facility, Village Center Commercial areas, and multi-family (MFR and HDR) and single family residential areas (SFR-2 and SFR-3) within the Lytle Creek Ranch Specific Plan area.

Street Parkway

Parkways, defined as the space between the curb and the sidewalk, are a key element in the overall community street scene. Guidelines for landscaping in this area are:

- A. A minimum of 10 percent 36" box trees are to be planted at designated community entries into Neighborhoods II and III.
- B. Street trees on streets within the Lytle Creek Ranch project shall be 24" box minimum.
- C. Street trees may be either informally or formally spaced, but shall average no more than 30' spacing on center.

Common Area Landscaping

- A. Most trees are to be a minimum 24" box in size. Up to 20% of the trees may be 15 gallon trees.
- B. Turf is appropriate for larger areas of active recreation such as parks, greenbelts, joint-use parks/schools, and the Grand Paseo.

- C. Shrub areas are to be planted with 60 percent 5 gallon and 40 percent 1 gallon.
- D. All buildings are to have a continuous foundation shrub planting.

Alley Drive Plantings

- A. All shrub pockets are to be planted with at least one 15 gallon vertical shrub along with ground cover and smaller shrubs at the base.
- B. 15 gallon vines with trellises or vine supports on the sides and top of the garage are to be provided on at least 50 percent of garages.
- C. Trees may be provided where space allows (minimum 24" box size). Trees in alleys are optional at the discretion of the builder or master project developer and shall not be required as part of project approvals.

Private Drive Plantings

- A. All trees are to be a minimum of 24" box size.
- B. 15 gallon vines with trellises or vine supports on the sides and top of the garage are to be provided on at least 50 percent of the homes on a block.
- C. Remaining shrub pockets are to receive a 15 gallon vertical shrub, with additional shrubs at the base adjacent to each garage.
- D. Shrub areas are to be planted with 60 percent 5 gallon and 40 percent 1 gallon.

Paseo and Trail Plantings

- A. Most trees are to be a minimum 24" box in size. Up to 25% of the trees may be 15 gallon trees.
- B. Shrub areas are to be planted with no smaller than 50 percent 5 gallon and 50 percent one gallon shrubs.

Parking Area Landscaping

Parking lot landscaping for all Village Commercial Center parking areas shall be required in agreement with a landscaping plan to be prepared by a licensed landscape architect, as follows:

- A. A minimum of ten percent of the required gross off-street parking area shall be landscaped, exclusive of setbacks. The parking area shall include access drives, aisles, stalls, maneuvering areas and required landscape setbacks around the perimeter of the parking facility.
- B. Landscape materials shall include plants consistent with the plant palettes contained in this Specific Plan and shall emphasize the use of drought-resistant ground covers, shrubs and trees to the extent feasible. At least one fifteen gallon tree shall be installed and maintained for every five parking stalls in the parking facility. Such trees may be clustered or grouped.
- C. An automatic irrigation system shall be installed and maintained in working order.
- D. Landscaping shall be continuously maintained and replanted as necessary. Landscaped areas shall be kept free of debris and litter.
- E. Landscaped areas shall be separated from vehicle parking and circulation areas by concrete curbs not less than six inches in height.
- F. Screen walls, gates, trellises, shrubs and vines, or espaliers are to be used to screen trash enclosures, where feasible.

Slope Landscaping

- A. All areas required to be landscaped will be planted with turf, groundcover, shrubs, or tree materials selected from the plant palette contained in these guidelines.
- B. Planting on slopes will commence as soon as the slopes are completed on any portion of the site and will provide for rapid short term coverage of the slope as well as long-term establishment cover per City of Rialto standards.

4.3.8 BIOFILTRATION SWALES

Lytle Creek Ranch incorporates the use of biofiltration swales to improve water quality on-site, particularly in the Grand Paseo in Neighborhood III and the golf course in Neighborhood II. Biofiltration swales are flow-through vegetated channels with a slope similar to that of standard storm drains channels (less than 6%), but are wider and shallower to maximize flow residence time and promote pollutant removal by filtration through the use of properly selected vegetation and settling. Some adsorption and uptake of dissolved pollutants also occurs. For biofiltration, it is important to maximize water contact with vegetation and the soil surface. The soils at the site should support vegetation growth. Biofilters should generally not receive construction-site runoff; if they do, presettling of sediments should be provided. Such biofilters should be evaluated for the need to remove sediments and restore vegetation following construction.

Maintenance of the infiltration/biofiltration areas is of vital importance for long-term operation. Maintenance can consist of sediment removal, vegetation cutting, replanting, changing types of plantings, etc. Long-term maintenance responsibility for infiltration/biofiltration areas will be provided by the Master Homeowners Association or other entity acceptable to the City of Rialto.

4.3.9 FENCES AND WALLS

Please refer to Figures 4-38 through 4-40 for conceptual wall and fence plans, and to Figure 4-41 for conceptual wall and fence details.

Perimeter Theme Wall and Pilasters

The perimeter theme wall will consist of a 6' tall colored precision block wall with a 4" colored precision block cap. Stone veneer pilasters with precast concrete caps will be constructed approximately every 100 feet along the wall.

Neighborhood Walls and Fences

Where provided, neighborhood fences and walls will be designed as integral components and extensions of building designs and surrounding landscapes. Periphery fences and walls may be integrated into adjacent structures and extended into the landscape areas to help integrate buildings into their environments. Fences and walls will be constructed of durable materials, colors, and textures that are similar and harmonious with the architecture. Wrought iron or tubular steel fencing, half block wall/glass or equivalent, vinyl fencing, colored precision block walls, split-face brick walls, manufactured stone and stone walls, brick and simulated brick walls, and other types of walls acceptable to the City, are all permitted types of walls/pilasters and fencing within Lytle Creek Ranch. Wood fencing is not permitted in Lytle Creek Ranch (excludes wooden gates, which are permitted).

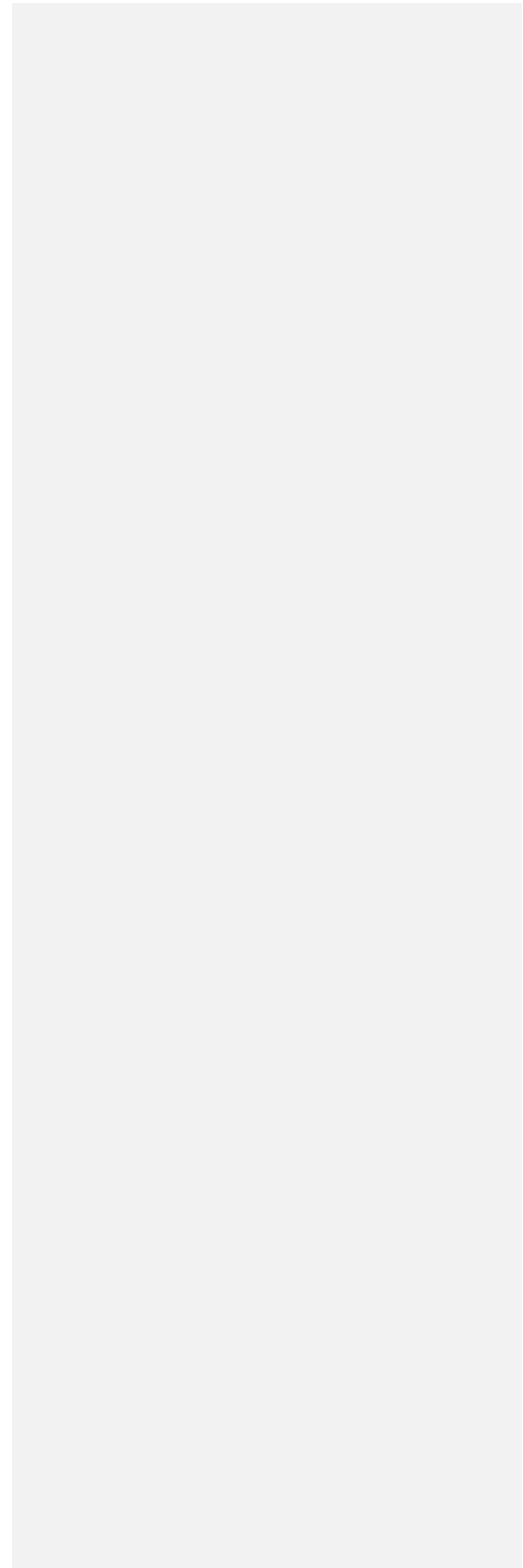
Particular importance will be given to railing and cap details. Fences and walls may be offset occasionally to avoid visual monotony. Fencing and walls will be used to define the limits of property ownership, as well as for the creation of exterior privacy and to promote the public's health and safety.

Side and Rear Yard Walls

All new single-family residential development will be required to install minimum five and one-half (5½) foot block walls, wrought iron or tubular steel fencing, vinyl fencing, or combination walls and fences (including walls with glass inserts to allow for views) along the side, rear and street side of the property line, except for alley loaded products or where other design considerations make constructing a wall impractical, unnecessary, or undesirable. Wherever a question arises as to whether or not a wall shall be provided on side or rear yards, the builder and City shall reach a consensus during Design Review as to whether or not a wall shall be required.

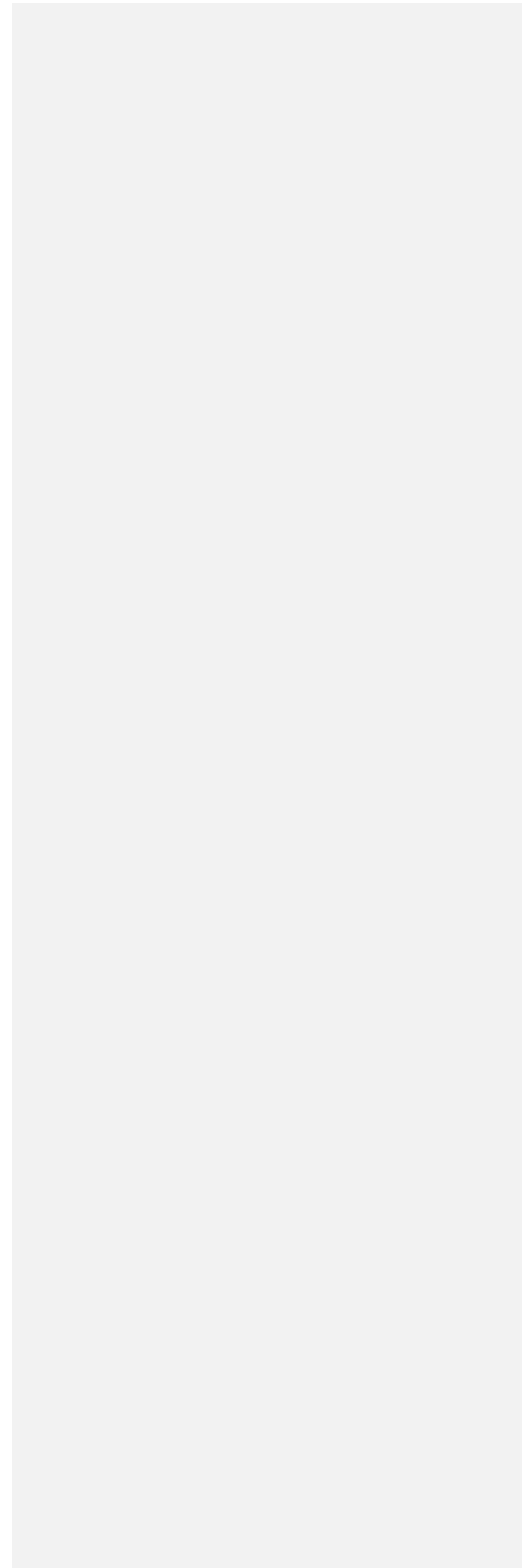
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Figure 4-37
Conceptual Community Wall and Fence Plan – Neighborhoods I and IV



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Figure 4-38
Conceptual Community Wall and Fence Plan – Neighborhood II

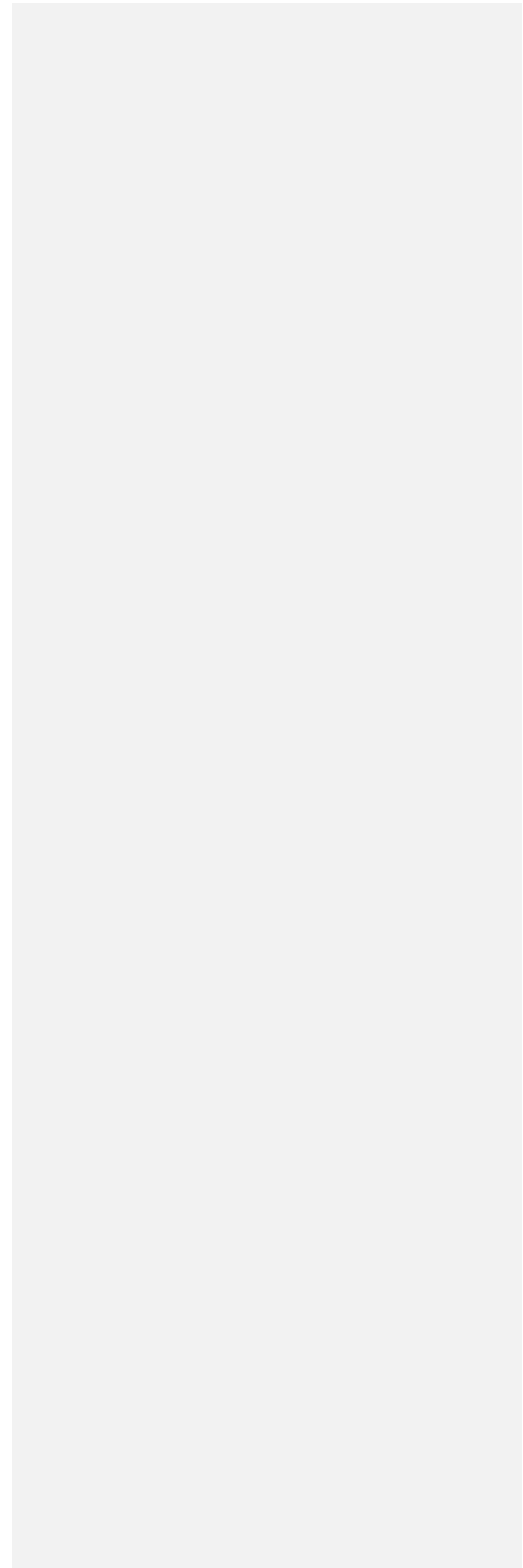


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Figure 4-39
Conceptual Community Wall and Fence Plan – Neighborhood III

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Figure 4-40
Conceptual Wall and Fence Details



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4.4 SUSTAINABLE DESIGN STRATEGIES

The way we plan the physical layout, or land use, of new communities is fundamental to sustainable design. Two basic land use practices over the past several decades have converged to generate haphazard, inefficient, and unsustainable urban sprawl: These practices are:

- Zoning ordinances that isolate employment locations, shopping and services, and housing locations from each other, and
- Low-density growth planning aimed at creating automobile access to increasing expanses of land.

Sustainable practices can lessen the environmental impacts of development with techniques that include compact development, reduced pervious surfaces and improved water detention and conservation, preservation of habitat areas, mixing of land uses (e.g., homes, offices, retail); and improved pedestrian and bicycle amenities that reduced reliance on smog-generating emissions from vehicles.

Because the concept of sustainability is a relatively recent concept that is still evolving, it is anticipated that new sustainable strategies will be continually developed during the build-out period of the Lytle Creek Ranch community. This Specific Plan encourages the implementation of realistic sustainable design strategies into the project design as the community continues to evolve and build-out over time. Below is a sampling of sustainable design strategies that may be utilized in Lytle Creek Ranch.

4.4.1 SITE PLANNING

- A. Provide physical linkages between land uses that promote walking and bicycling and provide alternatives to automobile use.
- B. Encourage compact development that concentrates residential areas close to public amenities such as schools, parks, retail, golf, recreation centers, etc.
- C. Include a range of housing types and/or densities within each Neighborhood (i.e., Neighborhoods I, II, III, and IV) in Lytle Creek Ranch.
- D. Create an interconnected street network that has a high level of connections with cul-de-sacs that include pedestrian or bicycle through connections.
- D. Incorporate “green” practices in developing buildings and infrastructure, particularly for stormwater runoff (e.g., bioswales).

- E. Encourage design of landscape areas that capture and direct stormwater runoff, particularly in open space, parks, paseos, and Village Center Commercial areas.
- F. Stabilize slopes to limit erosion as part of the Stormwater Management Plan and erosion control plan.
- G. Minimize the amount of paved areas for roads, parking, and patios, particularly in residential areas, where feasible, or consider using porous or permeable pavement.
- H. Provide an electric vehicle charging station in one of the planning areas designated Village Commercial (VC).

4.4.2 ENERGY EFFICIENCY

Most buildings can reach energy efficiency levels far beyond California 2010 Title 24 standards, yet most only strive to meet the standard. It is reasonable to strive for energy reduction in excess of that required by 2010 Title 24 standards. Therefore, development within the Lytle Creek Ranch Specific Plan shall be designed and implemented so as to exceed 2010 Title 24 standards by at least 15%. The builder will commit to a minimum of three of the following strategies:

- A. Passive design strategies can dramatically affect building energy performance. These measures include building shape and orientation, passive solar design, and the use of natural lighting.
- B. Develop strategies to provide natural lighting. Studies have shown that natural lighting has a positive impact on productivity and well being.
- C. Incorporate the use of Low-E windows or use Energy Star windows.
- D. Install high-efficiency lighting systems with advanced lighting controls. For non-residential buildings, include motion sensors tied to dimmable lighting controls. Task lighting reduces general overhead light levels.
- E. Where feasible, incorporate motion sensors or timers on exterior fixtures to reduce energy usage.
- F. Use a properly sized and energy-efficient heat/cooling system in conjunction with a thermally efficient building shell. Consider utilizing light colors for roofing and wall finish materials; install high R-value wall and ceiling insulation.
- G. Minimize the electric loads from lighting, equipment, and appliances.

- H. Individual developments within Lytle Creek Ranch are encouraged to implement some of the strategies of the EnergyStar program, which is an energy performance rating system developed by the U.S. Department of Energy and the Environmental Protection Agency, which certifies products and buildings that meet strict energy-efficiency guidelines. Involvement in the EnergyStar program will be completely optional at the discretion of each individual developer/builder.
- I. For retail, commercial, office, and light industrial/manufacturing uses, promote the use of light colored roofing with a high solar reflectance in order to reduce the heat island effect from roofs.
- J. In retail, commercial, and office developments, provide a limited number of preferred parking spaces for hybrid vehicles, fuel cell vehicles, electric vehicles, and other fuel efficient vehicles.

4.4.3 MATERIALS EFFICIENCY

The builder will commit to a minimum of three of the following strategies:

- A. Select sustainable construction materials and products by evaluating several characteristics such as reused and recycled content, zero or low off gassing of harmful air emissions, zero or low toxicity, sustainably harvested materials, high recyclability, durability, longevity, and local production. Such products promote resource conservation and efficiency. Using recycled-content products also helps develop markets for recycled materials that are being diverted from California's landfills, as mandated by the Integrated Waste Management Act.
- B. Encourage the use of low VOC paints and wallpapers within Lytle Creek Ranch.
- C. Encourage the use of low VOC Green Label carpet within Lytle Creek Ranch.
- D. Use dimensional planning and other material efficiency strategies. These strategies reduce the amount of building materials needed and cut construction costs. For example, consider designing rooms on four foot multiples to conform to standard-sized wallboard and plywood sheets.
- E. Consider using recycle base, crushed concrete base, recycle content asphalt, shredded tires in base and asphalt in roads, parking areas, and drive aisles, if feasible and economically viable. Re-using materials keeps materials out of landfills and costs less.
- F. Require plans for managing materials through deconstruction, demolition, and construction.

- G. Design with adequate space to facilitate recycling collection and to incorporate a solid waste management program that prevents waste generation.
- H. Establish a construction waste recycling program with a local waste management company to recycle up to 30% of the construction waste.

4.4.4 WATER EFFICIENCY

- A. Minimize wastewater by using ultra low-flush toilets, low-flow shower heads, and other water conserving fixtures.
- B. Use recirculating systems for centralized hot water distribution.
- C. Use a water budget approach that schedules irrigation using the California Irrigation Management Information System data for landscaping.
- D. Meter the landscape separately from buildings. Use micro-irrigation (which excludes sprinklers and high-pressure sprayers) to supply water in non-turf areas.
- E. Use state-of-the-art irrigation controllers and self-closing nozzles on hoses.

4.4.5 OCCUPANT HEALTH AND SAFETY

- A. Recent studies reveal that buildings with good overall environmental quality can reduce the rate of respiratory disease, allergy, asthma, sick building symptoms, and enhance performance. Choose construction materials and interior finish products with zero or low emissions to improve indoor air quality. Many building materials and cleaning/maintenance products emit toxic gases, such as volatile organic compounds (VOC) and formaldehyde. These gases can have a detrimental impact on occupants' health and productivity.
- B. Provide adequate ventilation and a high-efficiency, in-duct filtration system. Heating and cooling systems that ensure adequate ventilation and proper filtration can have a dramatic and positive impact on indoor air quality.
- C. Prevent indoor microbial contamination through selection of materials resistant to microbial growth.
- D. Provide effective drainage from the roof and surrounding landscape.
- E. Install adequate ventilation in bathrooms.
- F. Allow proper drainage of air-conditioning coils.

- G. Design building systems to control humidity.
- H. Establish criteria for the delivery and storage of absorptive materials, and the ventilation of spaces once the materials are installed to prevent mold.

4.4.6 LANDSCAPE DESIGN

- A. Encourage the use of low water use and native plant materials throughout Lytle Creek Ranch and minimize turf areas. Areas that may include larger areas of turf include community entries, parks, joint-use park/school facilities, schools, paseos and greenbelts, golf courses, sports fields, turf play areas, and other high-use outdoor activity areas.
- B. Provide plant materials that are well-suited depending on the solar orientation and shading of homes.
- C. Provide for a low water use irrigation system and for zoning of the irrigation system.
- D. Use green waste mulch and soil amendments to retain soil moisture.
- E. Incorporate locally native vegetation into the plant palette for Lytle Creek Ranch.
- F. Encourage the use of colored hardscape materials to reduce glare and/or reflect heat in outdoor plazas and gathering areas.
- G. Consider the use of low-growing plant material in parkways instead of turf.

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5.0 DEVELOPMENT STANDARDS

5.1 PURPOSE AND INTENT

The regulations contained in this Chapter are intended to provide for development of all properties located within the Lytle Creek Ranch Specific Plan area. This Chapter establishes the permitted uses and physical development standards for the proposed development in Lytle Creek Ranch.

The standards contained in this Chapter of the Specific Plan supersede those of the Rialto Municipal Code, unless otherwise stated herein. Where the language in this Specific Plan is undefined, unclear, or vague, then the final interpretation and determination shall be made by the Director of Development Services. At his or her discretion, the Director of Development Services may forward an item requiring interpretation to the Planning Commission for determination. In addition, any decision by the Planning Commission may be appealed to the City Council for final determination. All decisions by the City Council shall be deemed final.

In instances of conflicting regulations and standards, the standards and regulations contained in the Specific Plan shall take precedence over the Municipal Code. If this Specific Plan is silent on an issue, then the standards in the Rialto Municipal Code or other applicable city, state, or federal code shall apply, as appropriate. The provisions in this chapter are not intended to interfere with, abrogate, or annul any easement, covenant, or other agreement between parties.

5.2 DEFINITIONS

For the purposes of this chapter, definitions shall be the same as described in Chapter 18.04 of the Rialto Municipal Code, except as otherwise defined in this Specific Plan.

5.3 ZONING CATEGORIES

The Lytle Creek Ranch Specific Plan and the City of Rialto Zoning Map, as amended, designate the entire project site as "Specific Plan Zone." Development within Lytle Creek Ranch is governed by the Land Use Plan (see Figure 3-1 in the Specific Plan). Within the "Specific Plan Zone," there are eight different zoning categories, as follows:

RESIDENTIAL

There will be five separate categories of residential development within the Lytle Creek Ranch Specific Plan. Each of the residential uses will be a separate category in the permitted uses table.

Single-Family Residential One (SFR-1). This category will only include single-family detached residential development ranging in density from 2 to 5 dwelling units/acre (du/ac).

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Single-Family Residential Two (SFR-2). This category will include single-family detached and attached residential development ranging in density from 5 to 8 du/ac.

Single-Family Residential Three (SFR-3). This category will include a combination of single-family detached and attached residential product types at densities ranging from 8 to 14 du/ac.

Multi-Family Residential (MFR). This category will include only attached housing products such as, but not limited to, townhomes, attached row homes, condominiums, stacked flats, garden courts, motorcourts, and apartments with densities ranging from 14 to 28 du/ac.

High Density Residential (HDR). This category will include only high density residential products such as, but not limited to, condominiums, stacked flats, podium units, and apartments with densities ranging from 25 to 35 du/ac.

VILLAGE CENTER COMMERCIAL (VC)

This category allows for retail and commercial development including shopping centers, freestanding retail and commercial buildings, medical/dental uses, and office and business park uses. The intent of this category is to provide uses that are sales tax generating uses. Approximately 41.1 acres of land will continue its current use and not change, or the land has already changed.

OPEN SPACE / RECREATION (OS/R)

The Land Use Plan (Figure 3-1) identifies planning areas that may develop as “Open Space/Recreation.” These areas will consist of a mix of recreation types including, but not limited to, ~~an 18-hole golf course~~, neighborhood parks, mini parks, private recreation centers, and trails and walkways. The permitted uses and the development standards for the OS/R category apply to all planning areas designated as “Open Space/Recreation.”

OPEN SPACE / JOINT-USE (OS/JU)

The Land Use Plan (Figure 3-1) identifies planning areas that may develop as “Open Space/Joint-Use.” These areas will consist of joint-use parks/schools. The permitted uses and the development standards for the Open Space/Joint Use category apply to all planning areas designated as “Open Space/Joint-Use.”

OPEN SPACE (OS)

The “Open Space” areas in Lytle Creek Ranch are intended to stay preserved in their existing, natural state. Land within this category is designed to protect important natural resources located within the Lytle Creek Ranch Specific Plan.

5.4 OVERLAY DISTRICTS

The Lytle Creek Ranch Specific Plan includes two separate zoning overlays as identified below. If a portion of a planning area is developed using the overlay district standards, then the entire planning area must be developed under the same overlay in order to prevent potentially incompatible uses from locating adjacent to one another.

5.4.1 SINGLE-FAMILY RESIDENTIAL (SFR) OVERLAY

Portions of the areas designated as “Open Space/Recreation” (approximately three acres), as well as all areas designated as “Elementary School,” “Elementary/Middle School,” and “Open Space/Joint-Use” (approximately 41 acres) shall be overlain by a “Single-Family Residential Overlay” as depicted in Figure 3-1, Land Use Plan. This residential overlay district allows for the development of single-family residential homes at densities ranging from 2 to 14 du/ac; provided, however, that a Site Plan and Tentative Tract Map shall be submitted to the City for review and approval by the Planning Commission. Removal of open space and recreational areas and replacing them with homes will result in a need for a proportional increase in the amount of open space to be provided for those residences and/or payment of fees or combination thereof. In no event shall the dwelling unit cap of ~~8,407~~8,039 dwelling units be exceeded in the Specific Plan area. In addition, the gross density for the entire Specific Plan area shall not exceed 3.5 du/ac.

The uses permitted by right in the “Single-Family Residential Overlay” include the following (see Table 5-1 for additional permitted uses):

- Residential, single-family detached and attached (2 to 14 du/ac)
- Model homes
- Sales and leasing offices and trailers
- Nursing homes and convalescent facilities
- Independent living and assisted living residential facilities
- Home occupations

If an area overlain by the “Single-Family Residential Overlay” is developed with residential uses, then the development standards of the SFR-1 zone, SFR-2 zone, or SFR-3 zone shall apply, as follows: (1) for developments with residential densities of 2 to 5 du/ac, the SFR-1 zone development standards shall apply (see Table 5-2, Summary Development Standards for SFR-1 Zone); (2) for developments with residential densities of 5 to 8 du/ac, the SFR-2 zone development standards shall apply (see Table 5-3, Summary Development Standards for SFR-2 Zone); and (3) for developments with residential densities of 8 to 14 du/ac, the SFR-3 zone development standards shall apply (see Tables 5-4 to 5-6, Summary Development Standards for SFR-3 Zone).

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5.4.2 HIGH DENSITY RESIDENTIAL (HDR) OVERLAY

This overlay will include only high density residential products such as, but not limited to, condominiums, stacked flats, podium units, and apartments with densities ranging from 25 to 35 du/ac. See Table 5-1 for permitted uses for the High Density Residential Overlay (listed under the High Density Residential Zone). The development standards in Table 5-7, Summary Development Standards for MFR Zone, shall apply to all areas being developed under the High Density Residential Overlay with the following differences:

- 1) The density range in all HDR Overlay areas shall be 25 to 35 dwelling units per acre; and
- 2) The maximum building height in all HDR Overlay areas shall not exceed 55 feet.

In no event shall the dwelling unit cap of ~~8,407~~7,039 dwelling units be exceeded in the Specific Plan area. In addition, the gross density for the entire Specific Plan area shall not exceed 3.5 du/ac.

The High Density Residential Overlay applies to Planning Areas 89, 90 and 91. A Site Plan and Tentative Tract Map shall be submitted to the City for review and approval by the Planning Commission for any planning area seeking to implement the High Density Residential Overlay.

5.5 PERMITTED USES

This section of the Specific Plan document sets forth the uses permitted in each zone within the Lytle Creek Ranch Specific Plan area. The permitted uses listed below in Table 5-1, below, are grouped into the following categories of uses:

- Accessory Uses and Structures
- Places of Assembly
- Automotive and Vehicle Uses
- Dining, Drinking, and Entertainment
- General Services
- Industrial and Research Uses
- Office Uses and Health Services
- Public and Semi-Public Uses
- Recreation Uses
- Retail Uses
- Residential, Lodging, and Child Care Uses
- Temporary Uses
- Warehousing and Commercial Uses
- Other Uses

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5.6 DEVELOPMENT STANDARDS

5.6.1 SINGLE-FAMILY RESIDENTIAL ONE (SFR-1)

The following development standards shall apply in the Single-Family Residential One (SFR-1) in all neighborhoods (i.e., Neighborhoods I, II, III, and IV). Table 5-2 contains the SFR-1 development standards.

**TABLE 5-2
SUMMARY DEVELOPMENT STANDARDS FOR SFR-1
(SINGLE-FAMILY RESIDENTIAL ONE) ZONE**

Residential Development Standards	SFR-1
Minimum development area	3 acres
Minimum lot area per dwelling unit	5,500 sf
Minimum average lot area per dwelling unit	6,000 sf
Maximum dwelling units per net acre	5 du/ac
LOT DIMENSIONS	
Minimum lot width	50 feet
Minimum lot width for a flag lot, cul-de-sac, or knuckle at front property line (flag lots permitted for detached dwellings only)	25 feet
Minimum lot depth	90 feet
MAXIMUM LOT COVERAGE	60%
SETBACKS	
Minimum front yard (to main building façade)	10 feet
Minimum front yard (to front porch or deck)	10 feet
Minimum garage front yard setback	18 feet from back of sidewalk
Minimum side-entry garage setback	10 feet
Minimum corner side yard	10 feet*
Minimum interior side yard	5 feet*
Minimum rear yard	15 feet (useable space)
MAXIMUM HEIGHT	3 stories, not to exceed 40 feet in height
MINIMUM DWELLING UNIT SIZE	1,600 sf

*Ground level architectural encroachments shall not be permitted into the required side yards with the exception of roofs, eaves, cornices and other similar features located above the first floor, which may encroach into the side yards a maximum of two (2) feet.

- A. The City's Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan, excepting modifications to minimum lot size, lot dimensions, and garage setback requirements, which may not be reduced.
- B. Parking. A minimum of two (2) enclosed spaces shall be provided per unit/lot. Side-entry and tandem garages shall be permitted. Enclosures shall comply with all yard requirements. For second units, one additional off-street parking space shall be provided; parking space may be uncovered.

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C. Fences and walls. Fences and walls shall be permitted within the front, side, and rear yard setback areas, except as provided below:

1. Fences may not be erected within either public or private street rights-of-way.
2. Wherever fencing is visible from public view, the finished side of the fencing shall be exposed to public view.
3. No fence or wall shall exceed six (6) feet in height, unless a higher wall is specifically required for sound attenuation purposes. The height of the fence or wall shall be measured from the highest ground level immediately adjacent to the base of the wall.
4. In instances where a side or rear yard fence or wall is located adjacent to a public right-of-way, then the minimum fence height shall be six (6) feet as measured on the public right-of-way side.
5. Privacy walls, if provided in side and rear yards, shall be a minimum of five and one-half (5½) feet in height.
6. In front yard setback areas, solid fences and walls shall not exceed 42 inches in height. Pilasters and columns may be provided up to six (6) feet in height.
7. All pool enclosure fencing shall conform to applicable State of California or City of Rialto pool code fencing requirements, whichever is more stringent.

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5.6.2 SINGLE-FAMILY RESIDENTIAL TWO (SFR-2)

The following development standards shall apply in the Single-Family Residential Two (SFR-2) in all neighborhoods (i.e., Neighborhoods I, II, III, and IV). Table 5-3 contains the SFR-2 development standards. Exhibits 5-1a to 5-1c depict typical product types permitted in the SFR-2 zone. Additional products types are permitted in the SFR-2 zone provided the maximum density does not exceed 8 dwelling units per acre.

**TABLE 5-3
SUMMARY DEVELOPMENT STANDARDS FOR SFR-2
(SINGLE-FAMILY RESIDENTIAL TWO) ZONE
(APPLIES TO BOTH ACTIVE & NON-ACTIVE ADULT HOUSING)**

Residential Development Standards	SFR-2 Single-Family Detached (Except Motorcourts, Garden Courts and Alley Loaded Detached)	Motorcourt And Garden Court Homes	SFR-2 Alley Loaded Detached
Minimum development area	3 acres	3 acres	3 acres
Minimum lot area per dwelling unit	3,750 sf	3,750 sf	3,750 sf
Maximum dwelling units per net acre	8 du/ac	8 du/ac	8 du/ac
LOT DIMENSIONS			
Minimum lot width	35 feet	30 feet	35 feet
Minimum lot width for a flag lot, cul-de-sac, or knuckle at front property line (flag lots permitted for detached dwellings only)	25 feet	25 feet	25 feet
Minimum lot depth	80 feet	65 feet	80 feet
MAXIMUM LOT COVERAGE	60%	70%	70%
SETBACKS			
Minimum setback from public or private street right-of-way	none required	5 feet	5 feet
Minimum front yard (to porch or main living area)	10 feet	none required	none required
Minimum garage front yard setback	5 feet or less or 18 feet or greater	none required	none required
Minimum side-entry garage setback	10 feet	none required	none required
Minimum garage setback (for alley-loaded garages or motorcourt or garden court homes)	none required	0 feet	0 feet
Minimum corner side yard	10 feet	none required	10 feet
Minimum interior side yard	0 feet*	0 feet	0 feet*
Minimum rear yard	10 feet	none required	none required
MINIMUM SPACING BETWEEN MAIN BUILDINGS	10 feet	10 feet	10 feet
MINIMUM SPACING BETWEEN GARAGES ON ALLEY			
Distance Between Opposite Main Buildings on Alleys with Facing Garages	none required	none required	28 feet
Distance Between Opposite Main Buildings on Alleys without Facing	none required	none required	26 feet

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Residential Development Standards	SFR-2 Single-Family Detached (Except Motorcourts, Garden Courts and Alley Loaded Detached)	Motorcourt And Garden Court Homes	SFR-2 Alley Loaded Detached
Garages			
MAXIMUM HEIGHT	3 stories, not to exceed 40 feet in height	3 stories, not to exceed 40 feet in height	3 stories, not to exceed 40 feet in height
MINIMUM PRIVATE YARD AREA	not applicable	100 square feet with a minimum depth of 10 feet	100 square feet with a minimum depth of 10 feet
MINIMUM DWELLING UNIT SIZE	1,000 sf	1,000 sf	1,000 sf

*The minimum interior side yard shall be zero (0) feet; provided, however, that the minimum spacing between primary buildings on adjacent lots is five (5) feet. No ground level architectural encroachments shall be permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into the required side yard a maximum of two (2) feet.

- A. The City’s Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan, excepting modifications to minimum lot size, lot dimensions, and garage setback requirements, which may not be reduced.
- B. Parking. A minimum of two (2) enclosed spaces shall be provided per unit/lot. Tandem garages shall be permitted as the third enclosed parking space only. Side-entry garages shall not be permitted on lots narrower than 50 feet in width. Enclosures shall comply with all yard requirements.
- C. Fences and walls. Fences and walls shall be permitted within the front, side, and rear yard setback areas, except as provided below:
 - 1. Fences may not be erected within either public or private street rights-of-way.
 - 2. Wherever fencing is visible from public view, the finished side of the fencing shall be exposed to public view.
 - 3. No fence or wall shall exceed six (6) feet in height, unless a higher wall is specifically required for sound attenuation purposes. The height of the fence or wall shall be measured from the highest ground level immediately adjacent to the base of the wall.
 - 4. In instances where a side or rear yard fence or wall is located adjacent to a public right-of-way, then the minimum fence height shall be six (6) feet as measured on the public right-of-way side.
 - 5. Privacy walls, if provided in side and rear yards, shall be a minimum of five and one-half (5½) feet in height.

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6. In front yard setback areas, solid fences and walls shall not exceed 42 inches in height. Pilasters and columns may be provided up to six (6) feet in height.
7. All pool enclosure fencing shall conform to applicable State of California or City of Rialto pool code fencing requirements, whichever is more stringent.

Alley Loaded Detached

Land Use: Single-Family Residential 2
Type: Single-Family Detached
Density: 5-8 du/ac

Typical Building Plotting

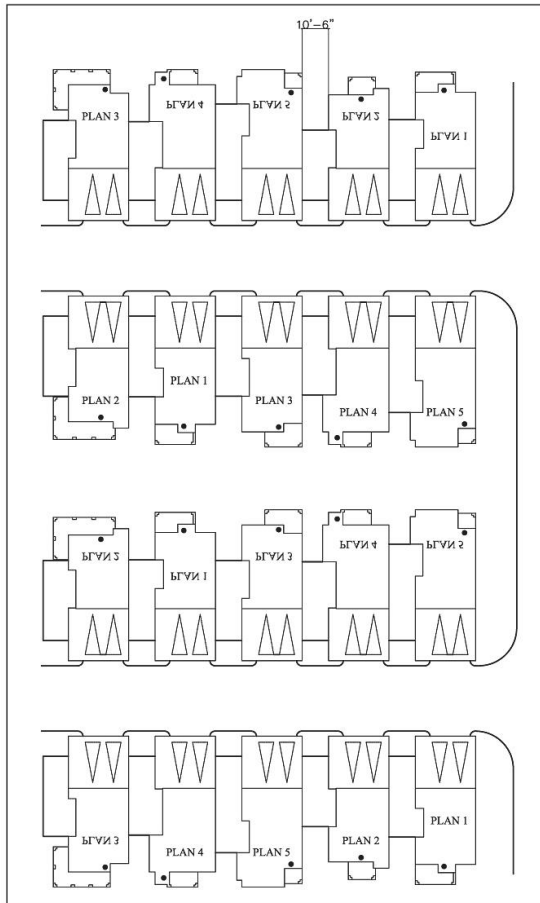


Figure 5-1a
Typical Alley Loaded Detached

LYTLE CREEK RANCH
SPECIFIC PLAN

Garden Court Detached

Land Use: Single-Family Residential 2
Type: Single-Family Detached
Density: 5-8 du/ac

Typical Building Plotting

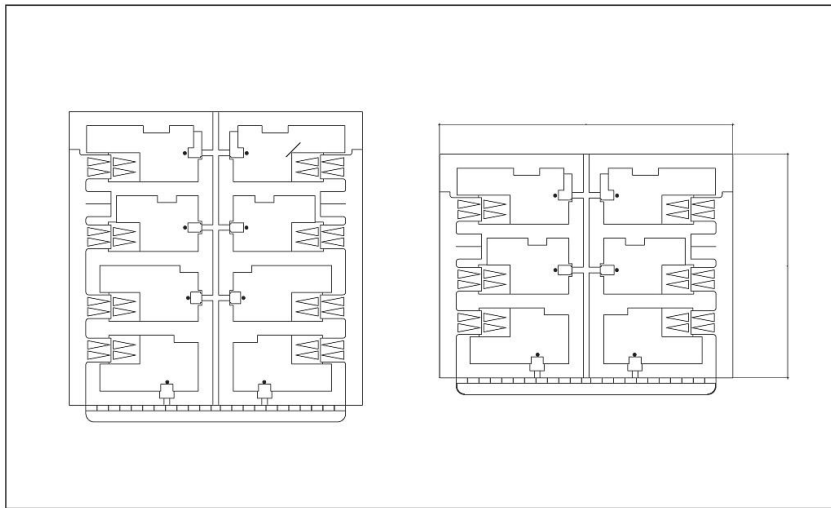


Figure 5-1b

Typical Garden Court Detached

Motor Courts

Land Use: Single-Family Residential 2
Type: Single-Family Detached
Density: 5-8 du/ac

Typical Building Plotting

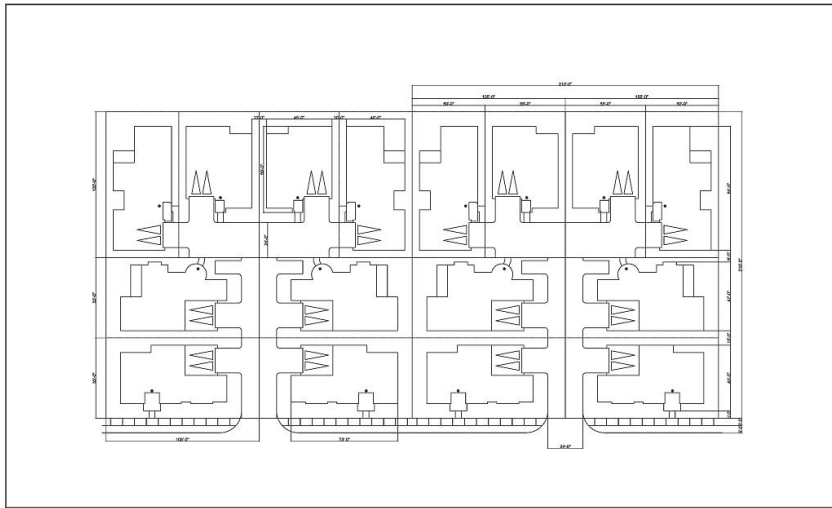


Figure 5-1c
Typical Motor Courts

LYTLE CREEK RANCH
SPECIFIC PLAN

5.6.3 SINGLE-FAMILY RESIDENTIAL THREE (SFR-3) – NEIGHBORHOODS I, III & IV

Development in this zone will consist of various type of detached and attached products including zero lot line housing. The following development standards shall apply in the Single-Family Residential Three (SFR-3) zones in Neighborhoods I, III, and IV. Tables 5-4 and 5-5 contain the SFR-3 development standards for Neighborhoods I, III, and IV. Exhibits 5-2a to 5-2d depict the typical product types permitted in the SFR-3 zone. Additional products types are permitted in the SFR-3 zone provided the maximum density does not exceed 14 dwelling units per acre.

**TABLE 5-4
SUMMARY DEVELOPMENT STANDARDS FOR SFR-3
(SINGLE-FAMILY RESIDENTIAL THREE) ZONE – NEIGHBORHOODS I, III & IV**

Residential Development Standards	SFR-3 Single-Family Detached	SFR-3 Alley Loaded Detached or Reverse Cluster Home*
Minimum development area	3 acres	3 acres
Minimum lot area per dwelling unit	2,000 sf	2,000 sf
Maximum dwelling units per net acre	14 du/ac	14 du/ac
LOT DIMENSIONS		
Minimum lot width	30 feet	30 feet
Minimum lot width for a flag lot, cul-de-sac, or knuckle at front property line (flag lots permitted for detached dwellings only)	25 feet	not applicable
Minimum lot depth	65 feet	65 feet
MAXIMUM BUILDING COVERAGE	80%	80%
SETBACKS		
Minimum front yard (to main building façade)	10 feet	5 feet
Minimum garage front yard setback	5 feet or less, or 18 feet or greater	not applicable
Minimum garage rear yard setback (for alley-loaded products)	not applicable	not applicable
Minimum corner side yard	5 feet**	5 feet**
Minimum interior side yard	5 feet / 0 feet ***	5 feet / 0 feet **
Minimum rear yard	none required	none required
MINIMUM SPACING BETWEEN MAIN BUILDINGS	5 feet	5 feet
MINIMUM PRIVATE OUTDOOR SPACE REQUIRED	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet
MAXIMUM HEIGHT	3 stories, not to exceed 45 feet in height	3 stories, not to exceed 45 feet in height
MINIMUM DWELLING UNIT SIZE	1,000 sf	1,000 sf

*A reverse cluster home development includes units with garages oriented to both streets and alleys.

**For corner lots, the minimum interior side yard shall be zero (0). No ground level encroachments are permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into the required corner side yard a maximum of two (2) feet; provided, however that the side yard is no less than five (5) feet in width.

LYTLE CREEK RANCH
SPECIFIC PLAN

***The minimum interior side yard shall be five (5) on one side and zero (0) feet on the other side; provided, however, that the minimum spacing between primary buildings on adjacent lots is no closer than five (5) feet. No ground level encroachments are permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into the required side yard a maximum of two (2) feet.

**TABLE 5-5
SUMMARY DEVELOPMENT STANDARDS FOR SFR-3
(SINGLE-FAMILY RESIDENTIAL THREE) ZONE – NEIGHBORHOODS I, III & IV**

Residential Development Standards	SFR-3 Duplexes or Triplexes	SFR-3 Attached (Excluding Duplexes & Triplexes)
Minimum developable area	3 acres	3 acres
Minimum lot area	not applicable	not applicable
Maximum dwelling units per net acre	14 du/ac	14 du/ac
LOT DIMENSIONS		
Minimum lot width	85 feet	100 feet
Minimum lot width for a cul-de-sac, or knuckle at front property line	25 feet	none permitted
Minimum lot depth	90 feet	not applicable
MAXIMUM BUILDING COVERAGE		
	80%	75%
SETBACKS		
Minimum yard adjacent to street right-of-way (to main living area)	5 feet	10 feet
Minimum yard adjacent to street right-of-way (to front porch or deck)	0 feet	5 feet
Minimum garage setback	5 feet or less or 18 feet minimum from curb	not applicable
Minimum garage rear yard setback (for alley-loaded products)	not applicable	not applicable
Minimum corner side yard	5 feet*	not applicable
Minimum interior side yard	0 feet**	not applicable
Minimum rear yard	0 feet	not applicable
MINIMUM PRIVATE OUTDOOR SPACE REQUIRED	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet
MINIMUM BUILDING SPACING		
Minimum Spacing Between Main Buildings	10 feet	10 feet
Buildings Built with Front Entry to Front Entry	not applicable	20 feet
Building Side to Building Side	not applicable	10 feet
Buildings Built with Front Entry to Rear Entry	not applicable	20 feet
Buildings Built with Front Entry to Building Side	not applicable	20 feet***
Buildings Built with Rear Entry to Rear Entry	not applicable	20 feet
Minimum Distance Between Main and Accessory Buildings	10 feet	10 feet
MINIMUM SPACING BETWEEN GARAGES ON ALLEY		
Distance Between Opposite Main Buildings on Alleys with	28 feet	28 feet

LYTLE CREEK RANCH
SPECIFIC PLAN

Residential Development Standards	SFR-3 Duplexes or Triplexes	SFR-3 Attached (Excluding Duplexes & Triplexes)
Facing Garages		
Distance Between Opposite Main Buildings on Alleys without Facing Garages	28 feet	28 feet
MAXIMUM HEIGHT	3 stories, not to exceed 45 feet in height	3 stories, not to exceed 45 feet in height
MINIMUM DWELLING UNIT SIZE	1,000 sf	1,000 sf

*For corner lots, the minimum interior side yard may be zero (0). No ground level encroachments are permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into the required corner side yard a maximum of two (2) feet.

**No interior side yard is required.

***Porches and decks on front entry elevations may encroach up to 10 feet into required 20 foot spacing between buildings.

- A. The City's Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan, excepting modifications to minimum lot size, lot dimensions, minimum dwelling unit size, and garage setback requirements, which may not be reduced.
- B. Parking. A minimum of two (2) parking spaces shall be provided per unit/lot. At least one (1) of the spaces shall be enclosed in a garage. Tandem garages shall be permitted. Enclosures shall comply with all yard requirements.
- C. Guest parking. One guest parking space shall be provided for every five (5) dwelling units, or portion thereof. On-street parking for guest parking shall be permitted.
- D. Fences and walls. Fences and walls shall be permitted within the front, side, and rear yard setback areas, except as provided below:
 - 1. Fences may not be erected within either public or private street rights-of-way.
 - 2. Wherever fencing is visible from public view, the finished side of the fencing shall be exposed to public view.
 - 3. No fence or wall shall exceed six (6) feet in height, unless a higher wall is specifically required for sound attenuation purposes. The height of the fence or wall shall be measured from the highest ground level immediately adjacent to the base of the wall.
 - 4. In instances where a side or rear yard fence or wall is located adjacent to a public right-of-way, then the minimum fence height shall be six (6) feet as measured on the public right-of-way side.
 - 5. Privacy walls, if provided in side and rear yards, shall be a minimum of five and one-half (5½) feet in height.

LYTLE CREEK RANCH
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6. In front yard setback areas, fences and walls shall not exceed 42 inches in height. Pilasters and columns may be provided up to six (6) feet in height.
7. All pool enclosure fencing shall conform to applicable State of California or City of Rialto pool code fencing requirements, whichever is more stringent.

LYTLE CREEK RANCH

SPECIFIC PLAN

Reverse Cluster

Land Use: Single-Family Residential 3
Type: Single-Family Detached
Density: 8-14 du/ac

Typical Building Plotting

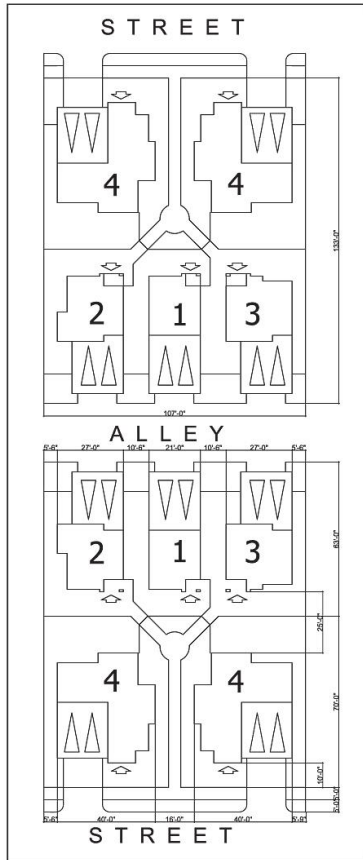


Figure 5-2a
Typical Reversed Cluster

Duplexes

Land Use: Single-Family Residential 3
Type: Single-Family Attached
Density: 8-14 du/ac

Typical Building Plotting

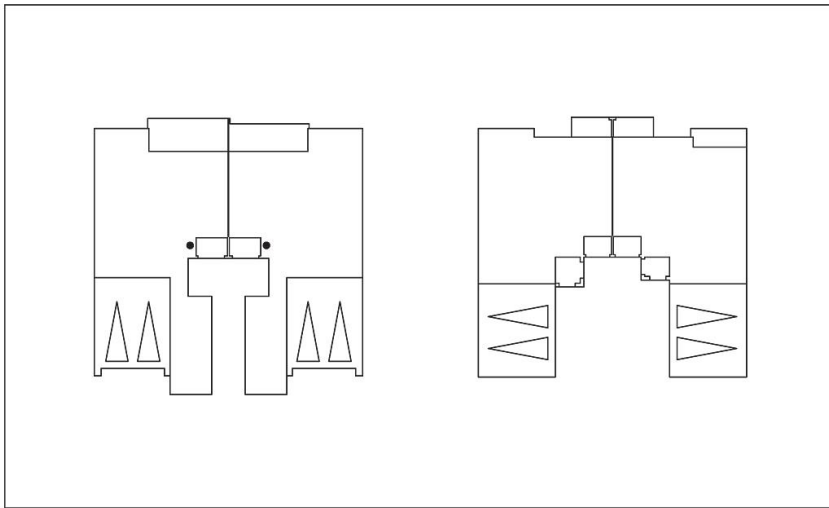


Figure 5-2b
Typical Duplexes

LYTLE CREEK RANCH
SPECIFIC PLAN

Manor Homes

Land Use: Single-Family Residential 3
Type: Single-Family Attached
Density: 8-14 du/ac

Typical Building Plotting

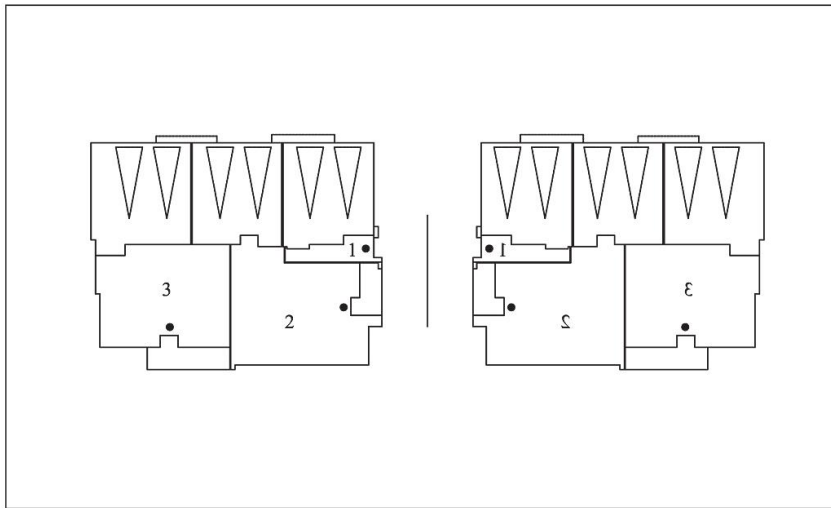


Figure 5-2c
Typical Manor Homes

Flats

Land Use: Single-Family Residential 3
Type: Single-Family Attached
Density: 8-14 du/ac

Typical Building Plotting

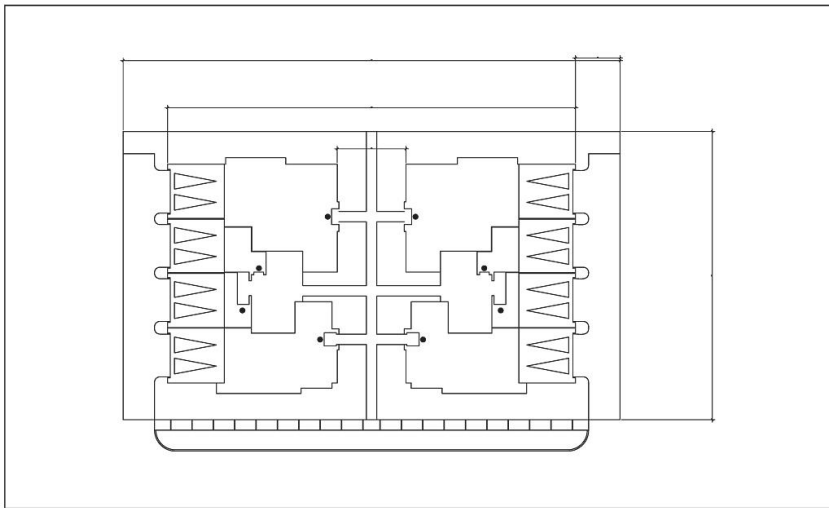


Figure 5-2d
Typical Flats

LYTLE CREEK RANCH
SPECIFIC PLAN

5.6.4 SINGLE-FAMILY RESIDENTIAL THREE (SFR-3) – NEIGHBORHOOD II ONLY

Development in this zone will consist of various types of detached and attached products including zero-lot-line housing. The following development standards shall apply in the Single-Family Residential Three (SFR-3) for Neighborhood II (Active Adult) only. Table 5-6 contains the SFR-3 Active Adult development standards for Active Adult uses. Exhibits 5-2a to 5-2d in this Specific Plan depict the typical product types permitted in the SFR-3 zone for Neighborhood II. Additional products types are permitted in the SFR-3 zone in Neighborhood II provided the maximum density does not exceed 14 dwelling units per acre.

**TABLE 5-6
SUMMARY DEVELOPMENT STANDARDS FOR SFR-3
(SINGLE-FAMILY RESIDENTIAL THREE) ZONE – NEIGHBORHOOD II ONLY**

Residential Development Standards	SFR-3 (Detached)	SFR-3 (Attached)
Minimum size of development area phase	3 acres	3 acres
Maximum dwelling units per net acre	14 du/ac	14 du/ac
LOT DIMENSIONS		
Minimum lot width	30 feet	not applicable
Minimum lot width for a flag lot, cul-de-sac, or knuckle at front property line (flag lots permitted for detached dwellings only)	20 feet	not applicable
Minimum lot depth	65 feet	not applicable
MAXIMUM BUILDING COVERAGE	80%	80%
SETBACKS		
Minimum setback from public or private street right-of-way	5 feet	5 feet
Minimum front yard (to main building façade)	5 feet	not applicable
Minimum garage front yard setback	5 feet or less or greater than 18 feet	not applicable
Minimum garage rear yard setback (for alley-loaded products only)	0 feet	not applicable
Minimum corner side yard	5 feet*	not applicable
Minimum interior side yard	5 feet / 0 feet **	not applicable
Minimum rear yard	not applicable	not applicable
Required Setback Adjacent to Off-site Residential Uses	15 feet	15 feet
MINIMUM BUILDING SPACING		
Minimum Spacing Between Main Buildings	10 feet	not applicable
Buildings Built with Front Entry to Front Entry	not applicable	20 feet
Building Side to Building Side	not applicable	10 feet
Buildings Built with Front Entry to Rear Entry	not applicable	20 feet
Buildings Built with Front Entry to Building Side	not applicable	20 feet***
Buildings Built with Rear Entry to Rear Entry	not applicable	20 feet
Minimum Distance Between Main and Accessory Buildings	not applicable	10 feet
MINIMUM SPACING BETWEEN GARAGES ON ALLEY		
Distance Between Opposite Main Buildings on Alleys with Facing Garages	28 feet	28 feet
Distance Between Opposite Main Buildings on Alleys without Facing Garages	28 feet	28 feet

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Residential Development Standards	SFR-3 (Detached)	SFR-3 (Attached)
MINIMUM PRIVATE OUTDOOR SPACE REQUIRED	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet	100 sf on the ground floor; 60 sf above ground floor with a minimum width of 6 feet
MAXIMUM HEIGHT	2 stories, not to exceed 30 feet in height	2 stories, not to exceed 30 feet in height
MINIMUM DWELLING UNIT SIZE	1,000 sf	850 sf

*For corner lots, the minimum interior side yard may be zero (0) feet. No ground level encroachments are permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into any required corner side yard a maximum of two (2) feet.

**The minimum interior side yard shall be five (5) feet on one side and zero (0) feet on the other side; provided, however, that the minimum spacing between primary buildings on adjacent lots is no closer than five (5) feet. No ground level encroachments are permitted. Roofs, eaves, cornices and other similar features located above the first floor may encroach into the required side yard a maximum of two (2) feet.

***Porches and decks on front entry elevations may encroach up to 10 feet into required 20 foot spacing between buildings.

- A. The City's Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan; provided, however, that lot sizes, lot dimensions, minimum dwelling unit size, and garage setbacks shall not be modified.
- B. Parking. A minimum of one (1) enclosed space shall be provided per unit/lot. One (1) additional space, either enclosed or unenclosed, shall be required per dwelling unit. This additional space may be in a driveway; provided, however, that the driveway extends a minimum of 18 feet from any sidewalk. Enclosures shall comply with all yard requirements. Tandem garages shall be permitted.
- C. Guest parking. One guest parking space shall be provided for every five (5) dwelling units, or portion thereof. On-street parking for guest parking shall be permitted.
- D. Fences and walls. Fences and walls shall be permitted within the front, side, and rear yard setback areas, except as provided below:
 - 1. Fences may not be erected within either public or private street rights-of-way.
 - 2. Wherever fencing is visible from public view, the finished side of the fencing shall be exposed to public view.
 - 3. No fence or wall shall exceed six (6) feet in height, unless a higher wall is specifically required for sound attenuation purposes. The height of the fence or wall shall be measured from the highest ground level immediately adjacent to the base of the wall.

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4. In instances where a side or rear yard fence or wall is located adjacent to a public right-of-way, then the minimum fence height shall be six (6) feet as measured on the public right-of-way side.
5. Privacy walls, if provided in side and rear yards, shall be a minimum of five and one-half (5½) feet in height.
6. In front yard setback areas, fences and walls shall not exceed 42 inches in height. Pilasters and columns may be provided up to six (6) feet in height.
7. All pool enclosure fencing shall conform to applicable State of California or City of Rialto pool code fencing requirements, whichever is more stringent.

5.6.5 MULTI-FAMILY RESIDENTIAL (MFR)

The multi-family residential zoning district is intended to provide for the development of attached residential developments. Typical housing types may include, but are not limited to, townhouses, stacked flats, motorcourts, courtyard homes, podium units, and apartments, with a density range of 14 to 28 dwelling units per acre. Table 5-7 provides a listing of the MFR development standards. Exhibits 5-3a to 5-3c depict typical product types permitted in the MFR zone. Additional products types are permitted in the MFR zone provided the maximum density does not exceed 28 dwelling units per acre.

**TABLE 5-7
SUMMARY DEVELOPMENT STANDARDS FOR MFR
(MULTI-FAMILY RESIDENTIAL) ZONE**

Residential Development Standards	MFR
Minimum size of development area phase	2 acres
Maximum number of dwelling units per net acre	28 du/ac
LOT DIMENSIONS	
Minimum lot width at front property line	not applicable
Minimum lot depth	not applicable
MAXIMUM BUILDING COVERAGE	
	70%
SETBACKS	
Required Setback Adjacent to Local, Collector or Arterial Street	5 feet
Required Setback Adjacent to Off-site Open Space	10 feet
Required Setback Adjacent to Off-site Residential Uses	15 feet
Required Setback Adjacent to Off-site Commercial, Office, or Light Industrial Use	25 feet
MINIMUM PRIVATE OUTDOOR SPACE (required for each dwelling unit)	
Ground Level (patio, deck or porch) – min. 8 foot width & min. 6 foot depth	60 sf
Outdoor Space Above Ground Level (balcony) – min. 6 foot width & min. 4 foot depth (Note: This standard applies to useable balconies only; there are no minimum dimensions for decorative balconies.)	48 sf
LANDSCAPE SETBACK	
Minimum Landscape Setback Adjacent to Local, Collector or Arterial Street	5 feet
Minimum Landscaped Setback When Located Adjacent to Off-site	5 feet

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Residential Development Standards	MFR
Residential Use	
MINIMUM BUILDING SPACING	
Buildings Built with Front Entry to Front Entry	20 feet
Building Side to Building Side	10 feet
Buildings Built with Front Entry to Rear Entry	20 feet
Buildings Built with Front Entry to Building Side	20 feet**
Buildings Built with Rear Entry to Rear Entry	20 feet
Minimum Distance Between Main and Accessory Buildings	10 feet
MINIMUM SPACING BETWEEN GARAGES ON ALLEY	
Distance Between Opposite Main Buildings on Alleys with Facing Garages	28 feet
Distance Between Opposite Main Buildings on Alleys without Facing Garages	28 feet
MINIMUM AVERAGE FLOOR AREA OF EACH DWELLING UNIT	
Bachelor and Studios	550 sf average
One Bedroom	650 sf average
Two Bedroom	850 sf average
Three Bedroom	1000 sf average
Four Bedroom	1200 sf average
MAXIMUM HEIGHT	3 stories, not to exceed 45 feet in height*

*Special architectural features, including, but not limited to, chimneys, towers, domes, cupolas, elevator penthouses, etc. shall be permitted up to 55 feet in height, and shall not exceed 15 percent of the roof area.

**Porches and decks on front entry elevations may encroach up to 10 feet into required 20 foot spacing between buildings.

- A. The City's Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan; provided, however, that lot sizes, lot dimensions, and garage setbacks shall not be modified.
- B. Either centralized laundry facilities, including washers and dryers, shall be installed in one or more central locations within each multi-family complex and hookups to accommodate washers and dryers shall be installed in each dwelling unit, or washers and dryers shall be installed in each unit. The project master developer or builder shall determine which option to implement on a case-by-case basis.
- C. Because this Specific Plan provides substantial public/common open space in proximity to the Multi-Family Residential (MFR) areas on-site in the form of parks, greenbelts, and the Grand Paseo, the amount of common open space required within each MFR planning area has been reduced accordingly from the existing City standard. The intent is to provide common open space within Lytle Creek Ranch where it is most useable and accessible to the public at-large. Exhibit A, below, depicts this approach. Therefore, each development shall provide a minimum of 100 square feet of common usable outdoor living area per residential dwelling unit (excludes

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private balconies, patios, and yards). Public or private driveways, parking areas, required trash areas, or other areas designed for operational functions are not considered open space. The minimum dimensions for open space areas shall be twenty-five feet in each direction. Open space areas may include, but are not limited to, turf areas, landscaped areas, hardscaped areas (excluding parking areas and public/private driveways), gardens, sitting areas, game courts, swimming pools, spas, sauna baths, tennis courts, basketball courts, play lots, bocce ball courts, outdoor cooking areas, lawn bowling and other recreational uses. The gradient or slope of all required outdoor living space shall not be greater than five percent in any direction except when grade variations are used as landscape features which do not interfere with proper drainage of the site.

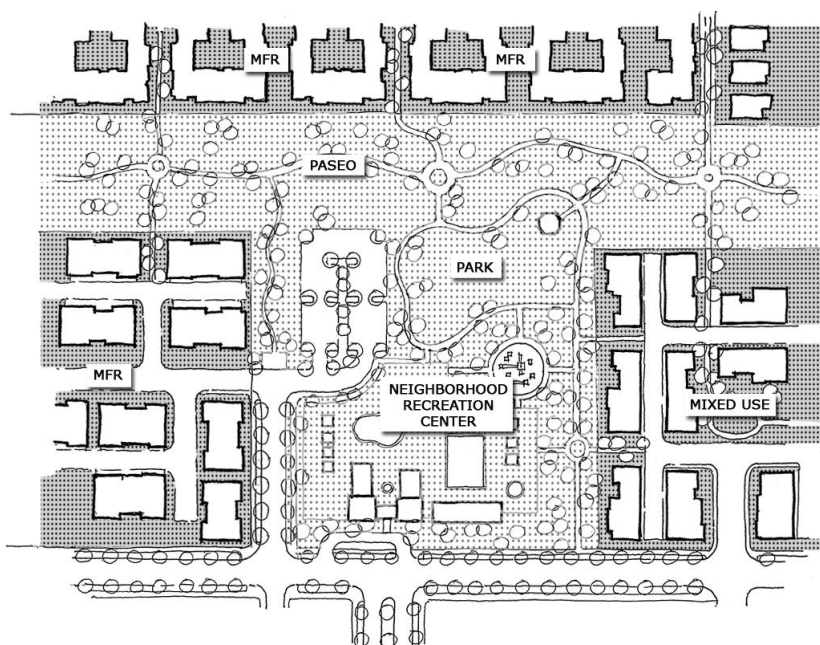


Exhibit A –Illustration Depicting Typical Relationship between Common Open Space in Private Residential Complexes and Public Open Space Areas within Lytle Creek Ranch

- D. The gradient or slope of all required outdoor living space shall not be greater than five percent in any direction except when grade variations are used as landscape features that do not interfere with proper drainage of the site.
- E. Open spaces created pursuant to the provisions of this chapter shall remain open and available for such use during the life of the development.

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- F. A pedestrian circulation system shall be incorporated into the development design for the purpose of providing direct access to and from all individual dwelling units, trash storage areas, parking areas, recreational facilities, and all other outdoor areas. This system shall be developed with a combination of the following development standards:
1. A public sidewalk system shall be developed adjacent to all public streets and installed in accordance with City standards.
 2. The interior walkway system shall include pedestrian walks or paths. The minimum width of the interior pedestrian system shall be four feet. Walkway systems shall utilize materials such as concrete, brick, flagstone or other materials approved by the City.
- G. Trash collection areas should, in general, be located within two hundred feet of the furthest residential unit they are to serve. Consideration should be given to siting trash collection areas for convenient access, but with care given to avoid impacting important design features such as, but not limited to, entries, recreation areas, leasing offices, and clubhouses. Such units shall be constructed to City standards and situated so as to reduce noise and visual intrusion on adjacent units and properties.
- H. Parking. A minimum of 2.1 spaces shall be provided per unit/residential lot. A minimum of one parking space shall be provided in an enclosed garage. Tandem garages shall be permitted. Enclosures shall comply with all yard requirements.
- I. Lighting and Security Devices.
1. All exterior lighting shall be adequately controlled and shielded to prevent glare and undesirable illumination to adjacent properties or streets.
 2. The use of energy-conserving and vandal-resistant fixtures or lighting systems shall be given primary consideration.
 3. Each unit shall be provided with a solid core entry door(s) and equipped with a wide-angle peephole and deadbolt lock attached to the construction on studding.
- J. Fences and walls. Fences and walls shall be permitted within the front, side, and rear yard setback areas, except as provided below:
1. Fences may not be erected within either public or private street rights-of-way.
 2. Wherever fencing is visible from public view, the finished side of the fencing shall be exposed to public view.
 3. No fence or wall shall exceed six (6) feet in height, unless a higher wall is specifically required for sound attenuation purposes. The height of the fence or wall shall be measured from the highest ground level immediately adjacent to the base of the wall.

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4. In instances where a side or rear yard fence or wall is located adjacent to a public right-of-way, then the minimum fence height shall be six (6) feet as measured on the public right-of-way side.
5. Privacy walls, if provided in side and rear yards, shall be a minimum of five and one-half (5½) feet in height.
6. In front yard setback areas, solid fences and walls shall not exceed 42 inches in height. Pilasters and columns may be provided up to six (6) feet in height.
7. All pool enclosure fencing shall conform to applicable State of California or City of Rialto pool code fencing requirements, whichever is more stringent.

Motor Court Townhomes

Land Use: Multi-Family Residential
Type: Multi-Family Attached
Density: 14-28 du/ac

Typical Building Plotting

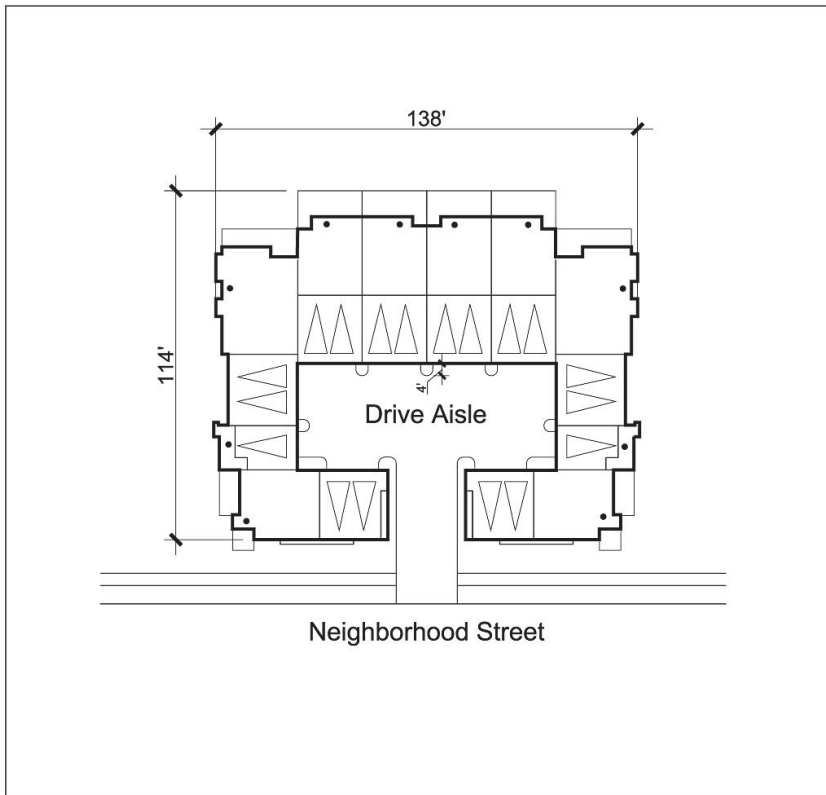


Figure 5-3a
Typical Motor Court Townhomes

LYTLE CREEK RANCH
SPECIFIC PLAN

Mansionette Condos

Land Use: Multi-Family Residential
Type: Multi-Family Attached
Density: 14-28 du/ac

Typical Building Plotting

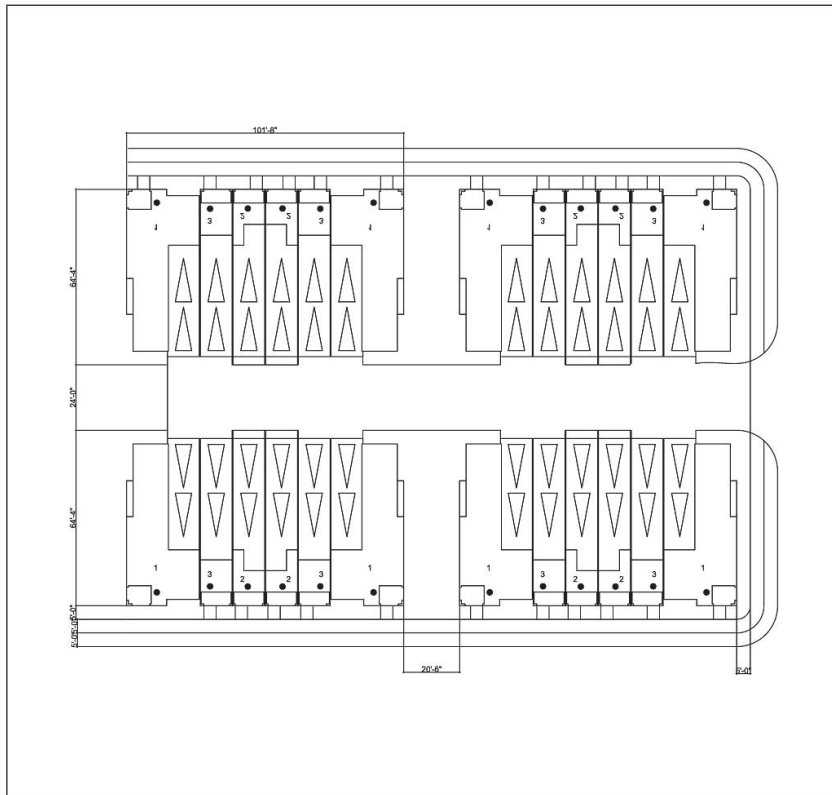


Figure 5-3b
Typical Mansionette Condos

11-Plex Garden Court

Land Use: Multi-Family Residential
Type: Multi-Family Attached
Density: 14-28 du/ac

Typical Building Plotting

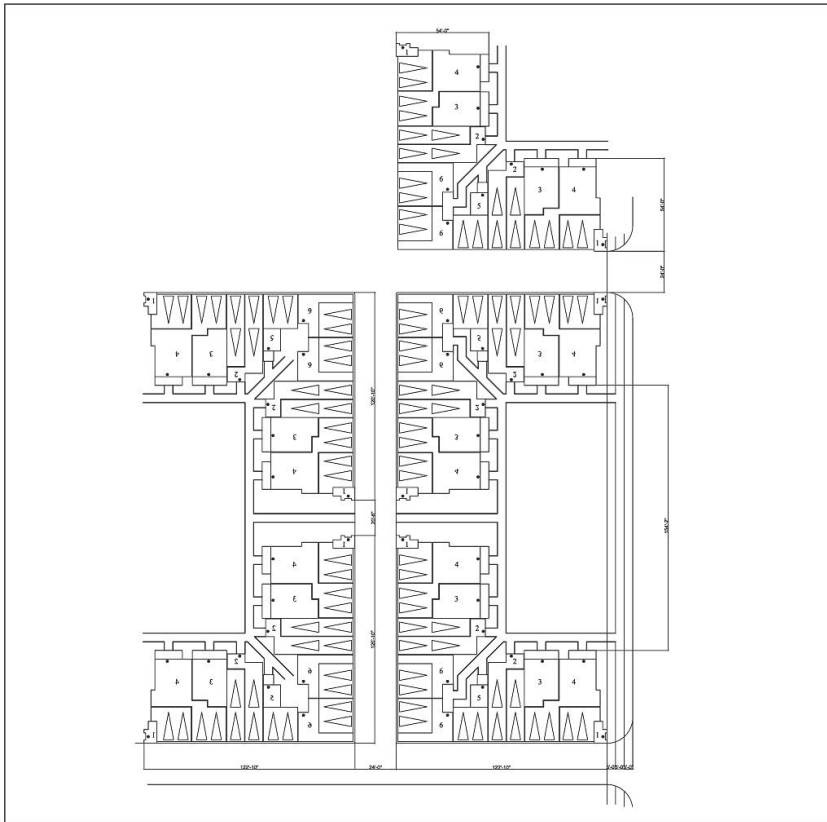


Figure 5-3c

Typical 11-Plex Garden Court

LYTLE CREEK RANCH
SPECIFIC PLAN

5.6.6 HIGH DENSITY RESIDENTIAL (HDR)

The High Density Residential zoning district standards shall be the same as the standards for the Multi-Family Residential zoning district contained in Table 5-7, with the following differences:

- 1) The density range shall be 25 to 35 dwelling units per acre; and
- 2) The maximum building height shall be 55 feet.

Typical housing types may include, but are not limited to, condominiums, stacked flats, podium units, and apartments.

5.6.7 VILLAGE CENTER COMMERCIAL (VC)

The Village Center Commercial zoning district allows a mixture of uses including, but not limited to, retail commercial, office and business park, and medical/dental uses. No residential development is permitted within the VC zoning district. Table 5-8 contains the VC development standards.

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**TABLE 5-8
SUMMARY DEVELOPMENT STANDARDS FOR VC
(VILLAGE CENTER COMMERCIAL) ZONE**

Village Center Commercial Development Standards	VC
Minimum size of development area phase	2 acres
Maximum floor area ratio	1.5 FAR*
LOT DIMENSIONS	
Minimum lot width at front property line	not applicable
Minimum lot depth	not applicable
MINIMUM LANDSCAPE SETBACK	
Minimum landscape setback along public streets (measured from right-of-way)	10 feet
MINIMUM BUILDING SPACING	
Buildings Built with Front Entry to Front Entry	20 feet
Building Side to Building Side	10 feet
Buildings Built with Front Entry to Rear Entry	20 feet
Buildings Built with Front Entry to Building Side	20 feet
Buildings Built with Rear Entry to Rear Entry	20 feet
Minimum Distance Between Main and Accessory Buildings	10 feet
MAXIMUM HEIGHT	3 stories, not to exceed 55 feet in height; provided that building heights up to 60 feet are permitted in Planning Area 78 only**

*FAR calculations do not include structured parking.

**Special architectural features, including, but not limited to, chimneys, towers, domes, cupolas, elevator penthouses, etc. shall be permitted up to 65 feet in height, and shall not exceed 15 percent of the roof area.

- A. The City's Development Review Committee (DRC) may approve modifications to these standards up to 15 percent for innovative and quality designs that meet the intent of the provisions of this Specific Plan.

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- B. Parking. Off-street parking shall be required pursuant to Section 18.58 of the Rialto Zoning Code, unless a shared parking analysis is prepared by a qualified traffic engineer and approved by both the Director of Development Services and the City Traffic Engineer, in which case reductions in the amount of required parking may be allowed. Any shared parking analysis may be subject to CEQA.
- C. Lighting and Security Devices.
1. All exterior lighting shall be adequately controlled and shielded to prevent glare and undesirable illumination to adjacent properties or streets.
 2. The use of energy-conserving and vandal-resistant fixtures or lighting systems shall be given primary consideration.

5.6.8 SINGLE FAMILY RESIDENTIAL (SFR) OVERLAY

The development standards for the Single-Family Residential Overlay shall be as follows:

- 1) For developments with residential densities of 2 to 5 du/ac, the SFR-1 zone district development standards shall apply (see Table 5-2, Summary Development Standards for SFR-1 Zone);
- 2) For developments with residential densities of 5 to 8 du/ac, the SFR-2 zone district development standards shall apply (see Table 5-3, Summary Development Standards for SFR-2 Zone); and
- 3) For developments with residential densities of 8 to 14 du/ac, the SFR-3 zone district development standards shall apply (see Tables 5-4 to 5-6, Summary Development Standards for SFR-3 Zone).

5.6.9 HIGH DENSITY RESIDENTIAL (HDR) OVERLAY

The development standards for the High Density Residential Overlay shall be the same as the standards for the Multi-Family Residential zoning district contained in Table 5-7, with the following differences:

- 1) The density range shall be 25 to 35 dwelling units per acre; and
- 2) The maximum building height shall be 55 feet.

Typical housing types may include, but are not limited to, condominiums, stacked flats, podium units, and apartments.

5.6.10 OPEN SPACE AND RECREATION (OS/R)

The Open Space and Recreation (OS/R) Zone is intended to promote a wide range of public and private recreational uses in the Lytle Creek Ranch community. The development standards in this zone apply to both “Open Space/Recreation” and “Open Space/Joint-Use” zones. These uses include community facilities, recreation centers and buildings, ~~golf courses~~, health clubs, public parks and recreation areas, sports parks, swimming pools, and other outdoor athletic facilities and similar recreational uses. In addition, this zone allows for low intensity, passive recreational purposes and related uses such as trails, picnic areas, bicycle paths, gardens, and sitting areas.

5.6.11 OPEN SPACE (OS)

Open Space Zone (OS). The Open Space Zone is intended for those lands that should remain in a natural state as much as feasible without intrusions from active recreational uses. Improvements may be made to these areas to allow for safe limited public access or for control of erosion, geologic stability, flood control, habitat enhancement, or other public safety purposes. The construction of buildings or other structures is not permitted.

5.6.12 DEVELOPMENT STANDARDS FOR WATER WELLS

The following development standards shall apply to all water well sites located or constructed within the Lytle Creek Ranch Specific Plan area:

- A. A minimum six foot high solid block wall shall be constructed on all sides of the water well site with a gate provided on one side for access. The gate shall be constructed of wrought iron or other decorative metal.
- B. Well sites shall be covered with concrete tile roofs.
- C. All utilities shall be installed underground.
- D. The construction of each structure shall incorporate feasible and appropriate sound attenuation measures to mitigate potential noise impacts.
- E. The well facilities shall be designed and constructed to match or complement the architectural styles of adjacent development.

5.6.13 SIGNAGE AND ADVERTISING STRUCTURES REQUIREMENTS (ALL DISTRICTS AND ZONES)

Signs and advertising structures shall be regulated by Chapter 18.102 of the Rialto Municipal Code; provided, however, that the following signs shall be permitted in addition to the signs permitted by Chapter 18.102 of the Municipal Code:

- A. One freestanding up to 90 feet in height shall be allowed in each of the following planning areas: Planning Areas 23, 25, 27, 31, and 33. Additional height may be allowed

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by the City subject to a “hang test.” Each sign may be double-sided. The signage portion of the structure shall not exceed 2,000 square feet in area (total, both sides). The sign may be internally or externally illuminated or a combination of both internally and externally lighting. These additional signs shall be reviewed and approved by the City of Rialto’s Development Review Committee.

- B. Billboards and electronic billboard-type signs shall be permitted within 400 feet of any State Route, Interstate Highway, or Freeway, subject to issuance of a Conditional Development Permit by the City of Rialto pursuant to the provisions of Chapter 18.66 of the City’s Zoning Code.

5.6.14 WALL AND FENCE STANDARDS (ALL ZONES)

- A. Chain link fencing is prohibited in Lytle Creek Ranch, except as temporary construction fencing or if used on school sites, sports fields, or playgrounds. Walls and fences around parks, schools, and playgrounds are permitted by right up to a height of eight (8) feet; provided that wall and fence heights up to twelve (12) feet may be permitted on a case-by-case basis subject to approval by the Parks Department and Community Development Department (no Variance or Conditional Development Permit required).
- B. Fences along Lytle Creek shall be constructed of wrought iron or tubular steel and shall be a minimum of six feet in height. The purpose of this fence is to discourage humans and animals from entering the Lytle Creek Wash. Actual height of the fences along Lytle Creek Wash shall be determined in consultation with the City of Rialto and the appropriate resource agencies.
- C. All new single-family residential development will be required to install minimum five and one-half (5½) foot block walls, wrought iron or tubular steel fencing, vinyl fencing, or combination walls and fences (including walls with glass inserts to allow for views) along the side, rear and street side of the property line, except for alley loaded products or where other design considerations make constructing a wall impractical, unnecessary, or undesirable. Wherever a question arises as to whether or not a wall shall be provided on side or rear yards, the builder shall make the final determination as to whether or not a wall shall be required.
- D. This Specific Plan permits, but does not require, residential walls and fences to extend up to ten (10) in height where such walls abut mining operations or other light industrial, manufacturing, warehousing, or similar use. The builder and project master developer shall determine the final wall height.

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- E. Where required for sound attenuation purposes, residential walls anywhere within Lytle Creek Ranch may exceed the permitted maximum heights, subject to a sound attenuation study.

5.6.15 OFF-STREET PARKING REQUIREMENTS (APPLIES TO ALL DISTRICTS AND ZONES WITHIN THE LYTLE CREEK RANCH SPECIFIC PLAN)

Except as otherwise provided for in this chapter, parking shall be required pursuant to Section 18.58.010 of the Rialto Municipal Code; provided, however, that a reduction in the number of required parking spaces may be approved for any Village Center Commercial development by the Director of Development Services and the City Traffic Engineer pending approval of a shared parking analysis by a qualified Traffic Engineer. CEQA review may be required. Good site design can minimize the need for large parking lots and expansive areas of parking.

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6.0 ADMINISTRATION AND IMPLEMENTATION

6.1 PURPOSE AND INTENT

The City of Rialto shall administer the provisions of the Lytle Creek Ranch Specific Plan in accordance with the State of California Government Code, Subdivision Map Act, the Rialto General Plan, the City of Rialto Municipal Code, and other applicable State and City regulations. The Specific Plan development procedures, regulations, standards, and specifications shall supersede the relevant provisions of the City's Municipal Code, as they currently exist or may be amended in the future. Any development regulation and building requirement not addressed in the Specific Plan shall be subject to the City's adopted codes and regulations. Where there is a question of interpretation, the City's Planning Division shall make a determination as to the intent of any disputed clause, paragraph, section, or development standard. Said determination shall be judged to be final.

6.2 DEVELOPMENT PHASING

The project will be built-out in ~~four-three~~ phases or neighborhoods (Neighborhoods ~~I, II, III, and IV~~), with build-out occurring by 2030 or as required by an approved development agreement. It is anticipated that construction will begin ~~in Neighborhood I, followed by development~~ in Neighborhoods II and III. Neighborhood IV will likely be the final neighborhood to be developed. These phases may occur either sequentially or concurrently with one another and the phasing is subject to change in response to market conditions and demands. Please refer to Figure 12-1, Development Phasing.

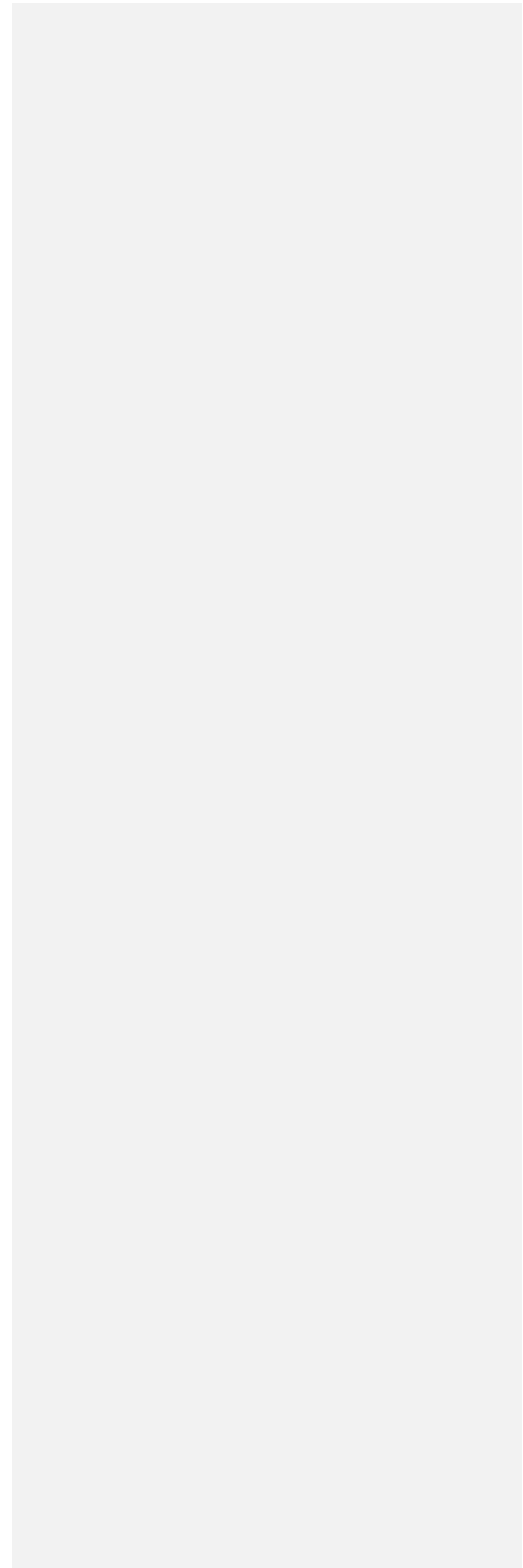
The project master developer shall have the right to alter the project phasing program at any time; provided, however, that notice of the phasing change shall be provided in writing to the City's Planning Division within thirty (30) calendar days of the change. In addition, the project phasing may be altered subject to approval by the affected City departments and revisions to the Development Agreement as deemed necessary or appropriate.

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Figure 6-1
Phasing Plan



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6.3 FINANCING

Several types of financing strategies and tools are available for financing master planned communities such as Lytle Creek Ranch. It is anticipated that the project will build-out using a variety of these strategies and tools including, but not limited to, the following:

6.3.1 MELLO-ROOS COMMUNITY FACILITIES ACT OF 1982

The Mello-Roos Act enables cities, counties, special districts, and school districts to establish community facilities districts and to levy special taxes to fund a variety of facilities and services required by a specific plan. A Mello-Roos tax can be applied to the planning and design work directly related to the improvements being financed and may also fund services on a pay-as-you-go basis including police and fire protection, ambulances, flood protection, recreational programs, parks, and schools.

6.3.2 SPECIAL ASSESSMENT DISTRICTS

Special assessment districts, such as the Lighting and Landscape Maintenance Act of 1972, the Municipal Improvement Act of 1913 and the Improvement Bond act of 1915, provide methods of leveraged financing whereby a public entity determines an area in which the provision of facilities will benefit real property. One or more special assessment districts may be created for the Lytle Creek Ranch project to cover improvements such as landscaping and lighting. This financing tool can be used for public improvements that directly benefit specific properties that are assessed to pay for the improvements at no risk to public agency general funds.

6.3.3 IMPACT FEES AND EXACTIONS

Impact fees and exactions are another tool for paying for new development resulting from increased population or demand for services. The master developer for Lytle Creek Ranch will negotiate with the City of Rialto to determine appropriate fees and exactions, which shall be identified in a Development Agreement.

6.3.4 DEVELOPER FUNDING

In certain instances, funding for on-site facilities may be tied directly to the Lytle Creek Ranch project. The developer may pay a fair share portion of the facility in exchange for development rights. On-site local streets, utility connections from the main trunk lines, and drainage facilities are typical examples of facilities that may be funded by the developer. Such improvements will usually be required concurrent with the project development.

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6.3.5 INFRASTRUCTURE FINANCING

- A. The local storm drain system shall be funded and constructed by the developer. The cost of the local system shall be borne by the developer without fee credits.
- B. The regional storm drain system and flood control improvements associated with Lytle Creek shall be funded and constructed by a Community Facilities District or other similar mechanism.
- C. The backbone water facilities and infrastructure shall be owned, operated, and serviced by the West Valley Water District (West San Bernardino County Water District). The fair share cost of designing and constructing the water system shall be borne by the developer.
- D. The backbone sewer facilities and infrastructure shall be owned and operated by the City of Rialto. The fair share cost associated with designing and constructing the sewer system shall be borne by the developer. The package treatment plant constructed as part of the Rosena Ranch project is expected to handle the waste from this proposed project as well.
- E. Telephone, electricity, gas lines, and cable television lines shall be installed and maintained by the appropriate utility companies.
- F. Roadway and parking lot improvements (the timing and responsibility for construction / funding of which shall be negotiated between the City of Rialto and the project master developer), shall occur in accordance with the adopted Development Agreement between the City of Rialto and project master developer.
- G. The Master Homeowner's Association or other private association or Landscape and Lighting District shall be responsible for installation, maintenance, and upkeep of all common landscape areas, hardscape areas, and irrigation systems within the Specific Plan area.
- H. All regional trails shall be the responsibility of the City of Rialto or other public entity to design, fund, construct, and maintain.
- I. All bicycle trails shall be the responsibility of the project master developer to design, fund, construct, and maintain.
- J. All necessary infrastructure (e.g., roads, sewers, water lines, storm drains, drainage improvements, etc.) shall be phased and installed concurrently with development.

6.4 MAINTENANCE RESPONSIBILITIES

Successful operation of maintenance districts and associations are important in ensuring that the project area is well-maintained. Maintenance responsibilities for parks, right-of-way, open space, landscape areas, street lighting, and common project facilities will be divided among a Master Homeowners Association, possible Neighborhood Associations, Community Services District, a Mello-Roos Community Facilities District, Landscape and Lighting Districts, or similar financing mechanism. Decisions regarding this joint assessment program will be made at a future state of project design and reviewed in concert with City agencies.

6.4.1 MASTER HOMEOWNERS ASSOCIATIONS

If not included within a Community Development District, Community Facilities District, Landscape and Lighting District, or other similar public maintenance mechanism, common areas identified in the Specific Plan shall be maintained by a permanent private master maintenance organization. Areas of responsibility shall include, but not be limited to, landscaped parkways, open space, parks, paseos, trails, mini parks, and private recreation areas.

6.4.2 RESIDENTIAL NEIGHBORHOOD ASSOCIATIONS

In certain residential areas of the project, smaller associations may be formed to assume maintenance responsibility for common areas and facilities that benefit only residents in those areas. Potential private recreation centers, common open space areas, and potential private roadways exemplify facilities that would come under the jurisdiction of a neighborhood association.

6.4.3 VILLAGE CENTER COMMERCIAL MAINTENANCE ASSOCIATIONS

Any Village Center Commercial planning areas may include their own private association(s).

6.4.4 OPEN SPACE AND PARKS MAINTENANCE ASSOCIATIONS

All open space, parks, mini parks, and recreation areas, which are not directly associated with a particular neighborhood, will be the responsibility of a Master Homeowners' Association, Community Facilities District, or other private entity. The joint-use parks within Lytle Creek Ranch shall be maintained by a Master Homeowner's Association, Community Facilities District, or the Rialto Unified School District.

6.4.5 PROJECT ROADWAYS MAINTENANCE

All public project roadways will be designed and constructed to standards acceptable to the City and will therefore be entered into the City of Rialto's system of roads for operation and

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maintenance. Private roads will be the responsibility of either the Master Homeowner's Association or other private maintenance association.

6.5 SPECIFIC PLAN ADMINISTRATION AND IMPLEMENTATION

This Specific Plan shall generally be enforced in a manner identical to the prevailing City of Rialto procedure(s) to enforce the provisions of the zoning and subdivision codes. The City of Rialto Development Services Department shall enforce the site development standards and design guidelines set forth herein, in accordance with the State of California Government Code, Subdivision Map Act, the Rialto General Plan, and the Rialto Municipal Code. The development procedures, regulations, standards, and specifications contained in this adopted Specific Plan shall supersede the relevant provisions of the City's Municipal Code, as they currently exist or may be amended in the future.

6.5.1 COMPLIANCE WITH THE ADOPTED SPECIFIC PLAN

The City of Rialto shall monitor compliance with the adopted Specific Plan and mitigation measures at these stages, as appropriate:

- A. During the review and approval of tentative tract maps, subsequent conditional development permits, and use permits.
- B. During the review of working drawings, and prior to the issuance of grading or building permits.
- C. Prior to the issuance of a certificate of occupancy for any building within the specific plan area.
- D. Prior to the recordation of any parcel map or final map within the Specific Plan boundaries.

6.5.2 IMPLEMENTING DEVELOPMENT APPLICATIONS

Table 6-1, Implementing Development Applications, is designed to clarify the process of entitlement through the City of Rialto for various applications and actions.

**Table 6-1
Implementing Development Applications**

Proposed Improvements	Action Required By				
	Transportation Commission	DRC	Director of Development Services	Planning Commission	City Council
Development Transfer Between Planning Areas		■	■		
Specific Plan – Ministerial Changes		■	■		
Specific Plan – Minor Adjustments		■	■	■	
Specific Plan – Major Amendment	■	■	■	■	■
Precise Plan of Design (Design Review)		■	■		
Development Agreement					■
Tentative Map(s) or Parcel Map(s)		■	■	■	

The following administrative standards apply to the implementation of future development applications (including plot plans, tract maps, parcel maps, conditional use permits, or variances) for projects within the Specific Plan area.

- A. No development shall occur or building permits issued within the adopted Specific Plan area until the proposed development is reviewed by the City’s Development Review Committee and found to be consistent with the adopted Specific Plan. Criteria for review and approval of proposed development shall include, but not be limited to the following:
1. Conformance with the land use designation;
 2. Conformance with the specific development standards, goals, and policies of the Specific Plan; and
 3. Conformance with the intended density of the zone of the site.

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- B. Future development within the Specific Plan area shall require individual project review and analysis including General Plan and Specific Plan consistency and environmental analysis, according to the provisions of the California Environmental Quality Act (CEQA) and the CEQA Guidelines.
- C. Future tentative or parcel maps and site plan review documents shall be consistent with the Specific Plan.
- D. Building permits for dwelling units shall be issued when a final subdivision map has been recorded. Permits may be issued for model units prior to final map recordation subject to the Subdivision Map Act and Development Code.
- E. Specific lotting designs and residential dwelling unit types for each planning area shall be determined at the time of individual implementing site design or subdivision proposals. Residential lot sizes, densities, and housing types may vary within each planning area so long as the number of units in the planning area does not exceed that allowed by this Specific Plan.
- F. The Rialto Unified School District is the lead agency for all environmental and entitlement processes related to schools within Neighborhood III of Lytle Creek Ranch. Any additional review and approval through the Specific Plan process shall not be required.

6.5.3 SPECIFIC PLAN INTERPRETATIONS

In instances where any section, subsection, sentence, clause, phrase, portion, or word contained within this specific plan is undefined, unclear, or vague, then the Director of Development Services shall make a determination as to its meaning and intent. All determinations shall be held to be final. At his or her discretion, the Director of Development Services may forward an item requiring interpretation to the Planning Commission for determination. In addition, any decision by the Planning Commission may be appealed to the City Council for final determination. All decisions by the City Council shall be deemed final.

6.5.4 SEVERABILITY

If any section, subsection, sentence, clause, phrase, or portion of this specific plan, or any future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this specific plan, or any future amendments or additions hereto. The City hereby declares that it would have adopted these requirements and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.

6.6 SPECIFIC PLAN MODIFICATIONS AND AMENDMENTS

6.6.1 DWELLING UNIT OR INTENSITY ADJUSTMENTS

This Specific Plan provides development flexibility by allowing for permitted shifts of dwelling units between planning areas and neighborhoods over the life of the Specific Plan. Any unused dwelling units or retail/commercial square footage within Lytle Creek Ranch may be transferred into other planning areas or neighborhoods within Lytle Creek Ranch; provided, however, the specific conditions are met. Transfer and adjustment of residential units and retail/commercial development shall be permitted to occur within the Specific Plan area as described in Section 3.2.4 of this Specific Plan.

6.6.2 MINISTERIAL CHANGES

Ministerial changes are modifications which are in substantial conformance with the Specific Plan as they relate to development standards or design guidelines. Ministerial changes shall be reviewed first by the City's Development Review Committee (DRC), then shall be forwarded to the Director of Development Services for final determination. Ministerial changes shall constitute the following:

- A. Minor revisions to the circulation plan related to ingress and egress locations.
- B. Minor modifications to the architectural or landscape design guidelines.
- C. Revisions to approved grading, water, sewer, or drainage plans.
- D. Revisions to phasing plans.

6.6.3 MINOR ADJUSTMENTS TO THE SPECIFIC PLAN

Minor adjustments to the plans, guidelines, regulations, and standards contained in the Lytle Creek Ranch Specific Plan may be approved at the discretion of the Director of Development Services; provided, however, that such deviations are deemed to be in substantial conformance with the Specific Plan and are not detrimental to the public health, safety, and welfare. Modifications to the adopted Specific Plan must be consistent with the purpose and intent of the originally approved Specific Plan.

The following modifications constitute "minor adjustments" to the approved Lytle Creek Ranch Specific Plan and may be approved without amending the Specific plan subject to a recommendation by the City's Development Review Committee (DRC) to the Director of Development Services with final determination made by the Planning Commission. Minor adjustments include modifications that do not change the meaning or intent of the Specific Plan.

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- A. Density or dwelling unit transfers between planning areas that are originally designated for residential land uses or with a residential overlay designation provided that the entire Lytle Creek Ranch Specific Plan area does not exceed 8,407,039 dwelling units.
- B. Minor changes to the design of the roadway cross-sections, provided that the streets have adequate capacity to handle the anticipated volumes of traffic and the design changes are recommended by the City's Traffic Engineer.

6.6.4 SPECIFIC PLAN AMENDMENTS

Specific Plan Amendments, also referred to as major amendments, are modifications or amendments that change the intent, provisions, or development standards of the Specific Plan.

- A. Major amendments shall constitute the following:
 - 1. Modifications to the Specific Plan boundaries.
 - 2. Amendments to any planning area that would change the originally approved land use designation.
 - 3. Amendments to the Specific Plan that would result in an amendment to the City's Adopted General Plan.
 - 4. Amendments to an approved Development Agreement.
- B. The project master developer, project owner, or any project merchant builder shall have the authority to initiate an amendment to the adopted Specific Plan at any time. No authorization by City staff, the Planning Commission, or the City Council shall be necessary to initiate a Specific Plan Amendment.
- C. Said amendment shall not require a concurrent General Plan Amendment unless it is determined by the City of Rialto that the proposed amendment would substantively affect the General Plan goals, objectives, policies, or programs for the Lytle Creek Ranch Specific Plan area.
- D. All Specific Plan Amendments shall be subject to the requirements of the California Environmental Quality Act (CEQA) of 1970 and any applicable City of Rialto Environmental Guidelines.

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- E. The Planning Commission and City Council shall each hold a public hearing on the proposed amendment of the Specific Plan. Any hearing may be continued from time to time as deemed appropriate and necessary by the Planning Commission and City Council.
- F. The Planning Commission shall review all proposed amendment to the adopted Specific Plan. Upon the close of the required public hearing, the Planning Commission shall act by resolution to adopt, reject, or modify the proposed Specific Plan Amendment and forward its recommendation and findings to the City Council for action.
- G. The City Council shall review the Planning Commission's findings and recommendations. Upon the close of the required public hearing, the City Council shall act by resolution and ordinance to adopt, reject, or modify the proposed Specific Plan Amendment. If approved by the City Council, any proposed Specific Plan Amendment shall be adopted by resolution, except for the Development Standards Chapter (Chapter 5.0), which shall be adopted by ordinance.
- H. Prior to approving or conditionally approving any Specific Plan Amendment, the following findings shall be made by the Planning Commission and City Council that the Specific Plan Amendment:
 - 1. Is consistent with the goal and policies of the General Plan and with its purposes, standards and land use guidelines;
 - 2. Will help to achieve a balanced community of all races, age groups, income levels, and ways of life;
 - 3. Results in development of desirable character which will be compatible with existing and proposed development in the surrounding neighborhoods;
 - 4. Contributes to a mix of land uses that will enable local residents to work and shop in the community in which they live; and
 - 5. Respects the environmental and aesthetic assets of the community consistent with economic realities.

6.7 TENTATIVE MAPS AND PARCEL MAPS

Implementing Tentative Tract Maps and Parcel Maps are expected to be processed through the City as part of implementing projects. These Maps will be processed according to the City's standard Tentative Map Review process and California's Subdivision Map Act. Tentative Tract Maps or Parcel Maps shall be reviewed first by the Director of Development Services, then by the Development Review Committee (DRC), and finally by the Planning Commission.

6.8 PRECISE PLAN OF DESIGN (DESIGN REVIEW)

This section is intended to provide for the review of those developmental qualities that are not subject to precise statement in this Specific Plan, in order to assure that yards, open space, structures, parking, loading facilities, landscaping, streets, and land uses will embody the overall character and intent of this Specific Plan.

Most implementing projects within Lytle Creek Ranch including, but not limited to, residential subdivisions, multi-family housing developments, and commercial/retail, office, business park, medical/dental, and light industrial developments will be required to go through a Precise Plan of Design as follows:

- A. The purpose of the Precise Plan of Design (PPD) is to promote an orderly and aesthetically pleasing environment within the City of Rialto and to ensure that development complies with all City ordinances and regulations. Because a Project EIR will be certified by the City in conjunction with approval of this Specific Plan, no further environmental studies shall be required for implementing projects.
- B. A PPD must be approved by the City's Development Review Committee prior to the issuance of any building permit for new construction of one or more residential units or for new commercial/retail, office, business park, medical/dental, and light industrial development within the Lytle Creek Ranch Specific Plan area.
- C. The Precise Plan of Design shall consider the provision of safe and convenient access to nearby public transportation, where feasible.
- D. When required, the PPD process shall be as follows:
 1. After receiving the completed application, the Planning Division will refer the PPD application to other members of the Development Review Committee (DRC), consisting of the Planning Division representative and members of the Engineering Division, Fire and Police Departments, the Building Division, Water Division, the Landscape Design Coordinator and the Redevelopment Agency.
 2. After determination by staff that the submittal package is complete, the completed applications shall be referred to the DRC within three (3) weeks. Applications will be notified by mail of staff's determination regarding their submittal.
 3. The DRC members will provide draft conditions of approval within three weeks from the date of distribution to the DRC members. The draft conditions of approval will be sent to the applicant/and or representatives and a DRC meeting will be scheduled the following

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week. This meeting will afford the opportunity for the applicant to discuss areas of concern or differences with each Department/Division's representative of the DRC.

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APPENDIX A GENERAL PLAN CONSISTENCY

This Appendix to the Specific Plan document contains an analysis of the consistency between the Lytle Creek Ranch Specific Plan and the goals and policies contained in the City of Rialto General Plan as required by Section 65454 of the California Government Code. Only those goals and policies that either relate directly to or have the potential to relate to the Lytle Creek Ranch project have been addressed. For the sake of brevity and clarity, those General Plan goals and policies that do not relate to new development or to the project have been omitted and are not addressed below. As evidenced by this consistency analysis, the Lytle Creek Ranch Specific Plan is consistent with the City of Rialto General Plan.

1.1 LAND USE ELEMENT

Goal 4.1.1 Encourage annexations which will demonstrate net benefit to the City before being considered for approval.

Policies

- 4.1.1.1 All large annexations to Rialto should be required to have an approved Specific Plan prior to annexation.
- 4.1.1.2 The City shall encourage, where appropriate, the preparation of Specific Plans on large annexations, to include a fiscal impact statement to insure that the City enjoys financial benefit from annexation of the subject land.
- 4.1.1.3 Based on the approved Specific Plan for large annexations, impact fees will be charged on new development sufficient to assure timely construction of public facilities and provision of expanded City services. Impact fees shall provide full mitigation of financial costs to the City, and protect its existing levels of services from deterioration.
- 4.1.1.4 Specific Plans for large annexations shall demonstrate compatibility of land uses both within and adjacent to the planned area.
- 4.1.1.5 Specific Plans for large annexations shall demonstrate protection of all resources valued by the cities of Rialto including, but not limited to: views, trees and other landscaping features, aquifers, surface water courses, historic buildings, etc. (Refer to Chapter X, the Conservation Element and Chapter IX, the Cultural and Historic Resources Element for policies to be applied to Specific Plan areas.)
- 4.1.1.6 Specific Plans for large annexations shall set aside land for community parks and other public facilities as appropriate to maintain the City's quality of life.

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Consistency Analysis

Prior to initiation of annexation procedures through LAFCO, the Lytle Creek Ranch Specific Plan must be adopted by the City of Rialto. A fiscal impact analysis was prepared for this project, which indicates that the project will have a positive financial contribution to the City. The Lytle Creek Ranch project will pay its fair share of impact fees as negotiated with the City. These fees will provide mitigation of financial costs to the City, while protecting existing City levels of services.

The Lytle Creek Ranch Specific Plan has been carefully designed to ensure a mix of compatible land uses, both within the project and between on-site uses and existing off-site uses. Where portions of the project abut potentially incompatible land uses such as the I-15 or the adjacent mining operations, then landscape buffers will be installed and, where warranted, walls or fencing.

A major component of the Lytle Creek project is the preservation of environmental resources on-site – resources that are valued both by the community and region. To this end, a minimum of ~~829-2769~~ acres of land will be preserved as natural open space, which will include Lytle Creek. There are no significant trees or historic buildings found on the project site.

Fifty ~~three~~ percent (~~5053~~%) of the Lytle Creek Specific Plan is devoted to open space and recreation uses. This includes several neighborhood parks, joint-use parks/schools, trails and walkways, ~~and an 18-hole public golf course with a new clubhouse.~~

Goal 4.1.2 Eliminate all negative impacts of mining activities on the citizens of Rialto while complying with the provisions of the California Mining and Reclamation Act (Refer to the Conservation Element, Chapter X.)

Policies

4.1.2.2 Allow the phasing of other planned land uses on large mineral resource sites on that part of the site on which mining is not anticipated, or on that part of the site on which mining is completed and reclamation has been established.

Consistency

Portions of the Lytle Creek Ranch project are designated as Mineral Resource Zone; however, no mining currently occurs on the property, nor has mining occurred on-site in the recent past. In addition, mining is not contemplated for the project site in the future. Therefore, the Specific Plan, in compliance with General Plan Policy 4.1.2.2, proposes to develop the project site with residential, recreation, retail and commercial, and other compatible land uses.

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Goal 4.1.3 Enhance Riverside Avenue to be the signature street of the City of Rialto.

Policies

- 4.1.3.1 Create a portal at the City's northwest entrance on Riverside Avenue (Refer to Chapter VIII, the Community Design Element.)
- 4.1.3.2 Provide planted median strips, parking planting and turning pockets on Riverside Avenue throughout the City. (Refer to the Community Design Element, Chapter VIII, Street Enhancement Program.)
- 4.1.3.3 Preserve and improve the northern section of Riverside Avenue as an enhancement to some of the City's finest neighborhoods.
- 4.1.3.4 Prevent strip commercial development and other inappropriate land uses on northern Riverside Avenue which is inconsistent with the goals and policies of the General Plan.
- 4.1.3.5 Route trucks and other through traffic between Riverside Avenue and Highland Avenue via Locust Avenue. (Refer to Chapter V, the Circulation Element.)
- 4.1.3.6 Encourage new and existing residential developments to provide ground signs and landscaping at their entrances to improve the identity and distinction of the City's neighborhoods. (Refer to Chapter VIII, the Community Design Element.)

Consistency Analysis

Lytle Creek Ranch will include the construction of an entrance portal into the City's northwest entrance within the Sierra Avenue or Riverside Avenue right-of-way, near to the I-15 Freeway (adjacent to Planning Area 33). This entry will be designed to include a representation of the Rialto Bridge, which is depicted on the City's seal. The entry will also include appropriate landscaping and possibly a water feature.

As part of improvements to Riverside Avenue, the street will be widened to an ultimate 127 foot wide right-of-way, including a 24 foot wide landscaped parkway on the northeast side of Riverside Avenue, adjacent to the project site. The landscape corridor will serve as the gateway to Neighborhoods II and III in the new master planned community of Lytle Creek Ranch.

The Lytle Creek Ranch land use plan designates several Village Center Commercial areas in key locations along the I-15 Freeway and Sierra Avenue/Riverside Avenue corridors. These areas are anticipated to include commercial and retail uses. However, no strip commercial development is planned. All commercial development will be concentrated into carefully designed shopping centers and developments.

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The project proposes to route trucks from the Cemex site along Locus Avenue between Highland Avenue and Riverside Avenue, as recommended in the City's General Plan.

New residential development in Lytle Creek Ranch will include ground signs and landscaping at key entrances, which will help to improve the identity and distinctiveness of the City's neighborhoods.

Goal 4.1.5 Develop, protect and enhance high quality residential and industrial land uses in Rialto.

Goal 4.1.7 Ensure that all developed areas of the City are adequately served with essential public services and infrastructure including, but not limited to, streets, water, surface drainage, sanitary sewers, law enforcement, fire protection and public schools.

Policies

- 4.1.7.1 The City will coordinate all development proposals with other affected public entities to ensure the provisions of adequate public facilities.
- 4.1.7.2 Proposals for new residential development will be referred to the affected school district(s) for advise and comment.
- 4.1.7.3 When reviewing proposals for residential development, the City will work closely with the affected school districts(s) in order to plan coordinated mitigation of any negative impacts upon the schools.

Consistency Analysis

The project site is located within the boundaries of three separate school districts. The applicable school districts include the Rialto Unified School District, the Fontana Unified School District, and the San Bernardino Unified School District. The project will pay its fair share of fees to each school district as required by California state law and/or the project master developer shall enter into a mitigation agreement with the school district. In response to the specific needs of the Rialto Unified School District, the project identifies a potential elementary school site and a K-8 school site in Neighborhood III.

Goal 4.2.2 Meet adopted City standards for the provision of park lands and open space. (Refer to the Open Space and Recreation Element, Chapter VII.)

Policies

- 4.2.2.2 School facilities, parks, and other activity nodes within residential districts shall be linked with Class II bicycle trails on neighborhood streets. Bicycle trails will be located on only one side of residential streets, leaving the other side free for residential parking. (Refer to Chapter V, the Circulation Element.)

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- 4.2.2.5 Require developers of the Lytle Creek Special Study Area to provide a Community Park within the project area.
- 4.2.2.10 Encourage proponents of development projects to provide parklands for residents and visitors.

Consistency Analysis

The project includes approximately ~~328.8282.5~~ acres of park and recreation land ~~and golf course~~ uses within its boundaries. Another 17 acres will be used for joint-use park/school sites in Planning Areas 48 and 74. In addition, a minimum of ~~829.2769~~ acres of the project will be preserved as natural open space. In total, fifty ~~three~~ percent (~~5053~~%) of the project will be preserved either as parkland, joint-use park/school, or open space. The project will link together parks and other activity nodes on-site via a 23.5-acre “Grand Paseo.” This paseo will vary in width from a minimum of 70 feet up to 110 feet. Three neighborhood parks will be provided in Neighborhood III (Planning Areas 40, 53, and 64). Also, a network of bicycle trails and lanes (either on Class I or II) are planned in Neighborhoods II and III. Where provided, Class II bicycle trails will be located on at least one side of the street.

The project proposes a series of neighborhood parks and joint-use parks/schools that will include amenities similar to a community park. These amenities will include athletic fields (in the joint-use park/school sites), gardens, swimming pools, tot lots, etc. The neighborhood parks and joint-use parks/schools and the Grand Paseo will be available for use by all citizens of Rialto. A portion of the neighborhood parks in Planning Areas 40, 53, and 64 are expected to contain private recreation centers for use by residents of Neighborhood III and their guests.

Goal 4.2.4 All streetscapes in Rialto shall support and enhance the City’s image as a desirable place in which to live or work.

Policies

- 4.2.4.1 Require landscaping in front of all barrier walls parallel to a street. (Refer to the Community Design Element, Chapter VIII.)
- 4.2.4.7 Stripe all collector streets with a center lane to facilitate residents’ entrances and exits between these streets and their neighborhood streets. (Refer to Chapter V, the Circulation Element.)

Consistency Analysis

Where barrier walls about a street, landscaping shall be installed to help screen and beautify the walls. The Collector street in Neighborhood III shall be constructed with a raised center median with turn pockets to facilitate residents’ entrances and exits between this street and local streets.

1.2 ECONOMIC DEVELOPMENT ELEMENT

Goal 1.1 Promote an economic base and positive business climate providing primary commercial services to the resident population.

Consistency Analysis

The project will help to provide an economic base and positive business climate in Rialto. Lytle Creek Ranch proposes approximately 95.6 acres of Village Center Commercial development, which will include at least one major shopping center, as well as smaller areas retail centers. The Specific Plan assumes up to 849,420 square feet of retail and commercial uses, which will provide important tax revenue to the City of Rialto, in addition to providing important services to the resident population.

1.3 CIRCULATION ELEMENT

Goal 3.2.3 Maintain Level of Service D or better on all Rialto arterial roadways.

Policies

3.2.3.2 New streets and improvements to existing streets made necessary by new development shall be provided concurrent with new development.

Consistency Analysis

The project will construct new streets and improvements to existing streets concurrently with new construction in order to ensure that Level of Service D is maintained on arterial roadways in the vicinity of the project.

Goal 3.2.4 Residential neighborhoods in Rialto shall be protected from the noise, pollution and danger of excessive vehicular traffic.

Policies

3.2.4.3 Residential areas border arterials shall be protected from traffic noise, pollution and danger by buffer walls bordering the arterial.

3.2.4.4 New residential driveways shall be permitted only on local streets and prohibited on arterials.

3.2.4.5 Collector streets shall be striped with left turn lanes in order to facilitate safe entrances and exits between local and collector streets.

Consistency Analysis

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Residential areas bordering Glen Helen Parkway, ~~Clearwater Parkway~~, Lytle Creek Road/Sierra Avenue, and Riverside Avenue will be protected from traffic noise, pollution and danger by buffer walls, as necessary and appropriate. These walls will be landscaped for aesthetic purposes. Walls internal to the project are optional and will be provided at the discretion of the master developer and builders.

New residential driveways will be constructed only on local streets, and will be prohibited on arterials.

The Collector Street in Neighborhood III of Lytle Creek Ranch will contain a raised landscaped median with turn pockets that will facilitate safe entrances and exits between local and collector streets.

Goal 7.1.2 Safe pedestrian access throughout Rialto.

Policies

7.1.2.1 Require sidewalks on at least one side of all streets in newly developed areas.

Consistency Analysis

Lytle Creek Ranch is designed with sidewalks on at least one side of all streets. These sidewalks will facilitate safe pedestrian movement throughout the project. Lytle Creek Ranch is designed to foster and promote walking between land uses.

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1.4 OPEN SPACE AND RECREATION ELEMENT

Goal 6.1 Optimal use of the flood plain, Alquist-Priolo Zone, and Rialto Municipal Airport Safety Zone II.

Policies

6.1.2 Investigate opportunities for dedication, acquisition or leasing of land in the Lytle Creek flood plan for appropriate use as City designated open space, parkland or recreational area.

Consistency Analysis

Lytle Creek Ranch is located within the flood plain of Lytle Creek. In addition, portions of the project are located within Alquist-Priolo Zones. Select areas within the Alquist-Priolo Zones will be preserved as open space areas ~~or used as golf course~~ and accessory uses. The project site is not located within the Rialto Municipal Airport Safety Zone II.

Goal 7.1 Meet adopted City standards for the provision of park lands and open space.

Policies

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- 7.1.1 The City shall acquire additional land for parks and open space.
- 7.1.4 The City shall apply, by ordinance, the provisions of the Quimby Act to ensure that adequate park and recreational facilities are available within or accessible to new residential developments.

Consistency Analysis

There will be approximately ~~328,828.5~~ acres of park and recreation land within Lytle Creek Ranch, ~~including an 18-hole public golf course~~. Except for the private recreation centers planned in Planning Areas 40, 53, and 64, these lands will be utilized as public parkland, which will be available for use by all City residents. The private recreation centers in Planning Areas 40, 53, and 64, will be for use by Neighborhood III residents and their guests only. In addition, the project ~~will~~ ~~may~~ provide an Active Adult recreation center in Planning Area 86, which is designed specifically to meet the recreation needs of Neighborhood II (Active Adult) residents.

Goal 9.1: Completion, maintenance and successful operation of a safe, attractive and effective network of recreational/circulation trails within the City.

Policies

- 9.1.4 Coordinate recreational trail plans with neighboring cities and with San Bernardino County to insure linkage of local trails across city boundaries, and linkage with regional trail systems.
- 9.1.5 Provide walkways parallel to bicycle paths in scenic areas such as the Lytle Creek Wash, or in pleasant, landscaped stretches of Class I bicycle trails.
- 9.1.7 Encourage the inclusion of internal walkways or greenways in residential subdivisions and PRD zones.

Consistency Analysis

The trails in Lytle Creek Ranch connect with existing trails within San Bernardino County and the San Bernardino National Forest.

A minimum eight foot wide multi-purpose trail will be constructed in the Grand Paseo, which traverses through most of Neighborhood III. In addition, bike lanes can be accommodated on the primary streets in Neighborhoods II and III

A series of internal walkways and greenways will be provided within Lytle Creek Ranch. The largest of these greenways will be the Grand Paseo in Neighborhood III. This generous greenway will range in width from a minimum of 70 feet up to 110 feet. The paseo will link together three of the Neighborhood Parks. In addition, the project proposes a 20 foot wide paved trail along Lytle Creek in Neighborhoods II, III, and IV. Also, the project proposes a comprehensive system of

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sidewalks along its streets. These walkways and greenways will facilitate pedestrian movement within and between residential subdivisions and throughout the project.

1.5 COMMUNITY DESIGN ELEMENT

Goal 1.2 Protect Rialto's rural, small town character.

Policies

1.1.3 All new development and renovations, adjacent to older residential neighborhoods, shall respect the scale, massing, and landscape of older residential neighborhoods. This includes development of landscape plans which complement neighborhood lots, buffer adjoining land uses, and soften variations in size, setbacks, or architectural character of buildings on nearby parcels; the relationship between the size and bulk of building parts; placement of windows and doors, setbacks, colors, materials, and detailing compatible with the existing neighborhood; and adopt demolition and infill ordinances, applying demolition and infill standards in all future Specific Plans within developed areas.

Consistency Analysis

Development within Lytle Creek Ranch will respect the scale, massing, and landscape of the nearby older residential areas, while establishing its own unique community identity.

Goal 1.3: Improve the quality of planned development in the City of Rialto.

Policies

1.1.3 Stagger the layout of units and/or buildings to maximize visual interest and individual identity.

Consistency Analysis

The layout of units and buildings to maximize visual interest and individual identity will be encouraged; provided, however, that consideration is provided to maximizing efficiency of unit layout.

Goal 1.4: Improve the architectural quality of development within Rialto to achieve harmony without monotony in the built environment.

Policies

1.4.1 Although common themes for neighborhoods are to be encouraged, incentives for residential, commercial and industrial developers to vary design, setbacks, driveways, rooflines, materials, colors, landscape treatments, etc. should be developed to ensure variation of individual units within large development projects.

1.1.3 The following neighborhoods shall be addressed separately by specific policies and/or documents as identified below: . . .

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- Sycamore Flats (Sycamore Flats Specific Plan)
- Lytle Creek Area (Lytle Creek Specific Plan)

In these areas, design shall conform to the separate design standards found in their respective Specific Plan documents. The City shall encourage the timely completion of design components within these areas.

Consistency Analysis

This Specific Plan includes standards to encourage innovation in project design including variations in architectural products and styles, setbacks, driveways, rooflines, materials, colors, and landscape treatments. Enclaves of homes will vary in size and density to promote diversity and interesting and varied neighborhoods.

The Sycamore Flats development area has been incorporated into the Lytle Creek Ranch Specific Plan. This Specific Plan (i.e., the Lytle Creek Ranch Specific Plan) covers both areas and include policies, standards, and regulations that apply specifically to these areas.

Goal 2.1 Promote well planned design of residential land uses within the City.

Policies

- 2.1.2 Within multi-family developments, encourage the clustering of residential units which provide semi-private common areas, maximize views, and provide passive open space and recreation uses.
- 2.1.3 Meandering greenbelts shall be incorporated into subdivision design along trails, collector streets, secondary streets and major highways, protected environmental areas, or other features. Bicycle and pedestrian trails should be connected with similar features in neighboring projects so that upon completion newer neighborhoods will be linked at the pedestrian level.
- 2.1.4 No houses should face secondary and/or major highways as defined in the Circulation Element.
- 2.1.5 The City shall encourage parkways to be placed on the outside of the public sidewalk immediately adjoining the curb, to shade pedestrians and provide a canopy of trees to be either uniformly spaced or informally grouped, but in no event shall trees be less than 25 feet average distance apart.
- 2.1.6 Where a subdivision fronts on a secondary or major highway, the subdivision shall be buffered and turned inward so that residences are not exposed to the traffic, noise and visual intrusions of the automobile. Instead, the subdivision shall be surrounded by

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decorative walls, varied in planned and texture to avoid monotony. Both the setback area in front of the wall, the wall itself, and the parkways shall receive landscape treatment, including turf, trees, flowers, shrubs, and vines.

- 2.1.7 All new residential development shall be required to install six (6) foot block walls along the rear and street side of the property line.
- 2.1.8 A minimum of 50% of the required front yard in all residential areas shall be landscaped (i.e., grasses, shrubs, trees and other plant materials).

Consistency Analysis

Lytle Creek Ranch will include a variety of innovative product types, possibly including, but not limited to, patio homes, zero lot line units, "cluster homes," attached townhouse products, garden courts, motorcourts, "mansionettes," and alley-loaded designs. Furthermore, Lytle Creek Ranch promotes the clustering of residential units within multi-family developments that provide semi-private common areas, maximize views, and provide passive open space and recreation uses.

Greenbelts will be incorporated into subdivision design along trails, collector streets, and major highways, and along Lytle Creek. Bicycle and pedestrian trails will connect with similar features in nearby neighboring projects so that, upon completion, the neighborhoods in Lytle Creek Ranch will be linked with the existing off-site neighborhoods at the pedestrian level.

No houses within Lytle Creek Ranch will face any major arterials (i.e., Riverside Avenue) as defined in the Circulation Element.

The project will comply with the intent of the General Plan, which encourages parkways to be placed on the outside of the public sidewalk immediately adjoining the curb, to shade pedestrians and provide a canopy of trees. Parkway trees either will be uniformly spaced or informally grouped and will be spaced according to City standards.

Where a subdivision fronts on a secondary or major highway, the subdivision shall be buffered and turned inward so that residences are not exposed to the traffic, noise, and visual intrusions of the automobile. Instead, the subdivision shall be surrounded by decorative walls, varied in planned and texture to avoid monotony. Where there are no noise considerations, housing developments on collector and local streets may be constructed without walls. Both the setback area in front of the wall, the wall itself, and the parkways shall receive landscape treatment, including turf, trees, flowers, shrubs, and vines.

New single-family residential development will be required to install block walls along the rear and street side of the property line, except for alley loaded products or where other design considerations make constructing a wall impractical or undesirable.

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The project will require compliance with the condition that a minimum of 50% of the required front yard in all residential areas shall be landscaped (i.e., grasses, shrubs, turf, trees, and other plant materials).

Goal 3.1 Promote commercial and/or industrial development which is well designed, people-oriented, sensitive to the needs of the visitor or resident, and functionally efficient for its purpose.

Policies

- 3.1.1 All commercial and industrial projects shall follow a site plan in which buildings are juxtaposed at differing angles, rather than arrayed along rectangular axes.
- 3.1.2 Building facades shall incorporate varied planes and textures; natural rather than manufactured finishes; variety in window and door treatments.
- 3.1.3 Architecture shall be encouraged which disaggregates massive buildings into smaller parts with greater human scale.
- 3.1.4 Mature landscape planting shall be incorporated into commercial and industrial projects to define and emphasize entrances, inclusive of those areas along the front of a building facing a parking lot.
- 3.1.5 All major commercial developments shall incorporate theme elements intended to distinguish them from other development, foster individuality, and promote gathering opportunities. These elements to include: outdoor cafes, gateways, kiosks, flag courts, trellises and arbors, bell towers, theme towers, galleries, patios and plazas, water elements, booths, amphitheaters, outdoor markets, colonnades and arcades, and clerestories.
- 3.1.6 All commercial projects shall incorporate direct walkways which cross the parking lots, connecting the buildings with the streets and bus shelters.
- 3.1.7 Parking lots at the rear of a commercial development shall not be isolated from the fronts of buildings. Commercial developments shall provide either mid-building pedestrian access or fully treated rear entrances. Delivery areas shall be separated from pedestrian areas.
- 3.1.8 Rather than relating only to the parking lots, commercial projects should also include internal corridors or passages which are not jeopardized by automobile noise and congestion. These should be designed with the type of visual and social elements which can draw the pedestrian from building to building, patio to courtyard.

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- 3.1.9 Where pedestrian crossings are developed, curbs shall be pinched to shorten the crossing distance required, whenever feasible. Additional pedestrian protections, including bollards and defensible space landscape treatment shall be required.
- 3.1.10 Pedestrian walkways, including, but not necessarily limited to, those directly under building canopies, shall be enhanced by one or more of the following techniques: interlocking or textured paving, turf block walls, theme plantings, trees projecting through canopies, bollards and kiosks, pavilions or gazebos, trellises and arbors planted with flowering vines.
- 3.1.11 Bus shelters shall be incorporated in all new commercial and industrial projects, and in all residential, institutional or other developments fronting major highways as defined in Chapter V, Circulation. Bus shelters may also be required in rehabilitation projects affecting existing commercial and industrial projects.
- 3.1.13 Outdoor storage areas shall be fully screened from the public view with a combination of block walls and landscaping.

Consistency Analysis

Efforts will be made for commercial and industrial buildings in Lytle Creek Ranch to arrange the buildings to minimize the appearance of long, unbroken, rectangular axes, where feasible, without compromising the efficiency of the site design and layout. Landscaping will be used to visually "break up" long, straight axes. For smaller buildings, this may include clustering them together around a small plaza, green space, or other focal point.

Commercial and industrial building facades will incorporate varied planes, colors, and textures to promote interest.

To the extent feasible, large commercial buildings should be visually broken up into smaller components by changes in color, texture, rooflines, window and door spacing, or massing. Landscaping and vertical trees will also help to break up building massing.

The commercial and industrial development within Lytle Creek Ranch will incorporate enhanced landscape planting to define and emphasize entrances, including areas situated along the front of retail buildings, facing a parking lot.

All major commercial developments will incorporate theme elements intended to distinguish them from other developments, foster individuality, and promote gathering opportunities. Such elements may include, but are not limited to, outdoor cafes, gateways, kiosks, flag courts, trellises and arbors, bell towers, theme towers, galleries, patios and plazas, water elements, booths, outdoor markets, colonnades, arcades, and clerestories.

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Larger commercial projects of ten acres or larger in size will incorporate direct walkways, where feasible, that cross the parking lot(s) and connect the buildings with the adjacent streets. Smaller commercial projects will include sidewalks and walkways, where feasible.

Parking lots at the rear of a commercial development shall not be isolated from the fronts of buildings. Commercial developments will provide either mid-building pedestrian access or fully treated rear entrances. Delivery areas will be separated from pedestrian areas.

Rather than relating only to the parking lots, commercial projects should also include internal corridors or passages which are not jeopardized by automobile noise and congestion. These should be designed with the type of visual and social elements that can draw the pedestrian from building to building, patio to courtyard.

Where pedestrian crossings are provided, curbs may be pinched to shorten the crossing distance required. Additional pedestrian protections, including bollards and defensible space landscape treatment may be required to ensure pedestrian safety.

Pedestrian walkways including, but not necessarily limited to, those directly under building canopies, shall be enhanced by one or more of the following techniques: interlocking or textured paving, aggregate or colored concrete, broom finished concrete or other decorative finish, turf block walls, theme plantings, trees plantings, canopies, bollards and kiosks, benches, seat walls, pavilions or gazebos, and/or trellises and arbors planted with flowering vines.

To help foster use of alternative modes of transportation, bus shelters will be incorporated into new commercial and industrial projects that have direct access to Riverside Avenue. Additional bus stops will be provided along Riverside Avenue, adjacent to the residential portions of the project, as permitted by the City of Rialto and the local transit authority (Omnitrans).

Outdoor storage areas within Lytle Creek Ranch will be screened from the public view with a combination of block walls and landscaping.

Goal 3.3: Minimize the visual impact of vehicles on the landscape and community design of parking lots.

Policies

3.3.1 The City shall require commercial developments to minimize the visual impacts associated with parking lots through:

- Depression of parking lot grade, wherever feasible, to reduce the visual impact of automobiles when seen from the street;
- Development of screen walls and landscaped buffers at sufficient height to conceal car grillwork and nuisance headlights into the street;

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- Parking lot design which breaks up parking areas with landscaped belts, thereby reducing the massive and unbroken appearance of paved surfaces; or
- Continuous connection of planters rather than isolated tree wells and planters separated by wide expanses of paving.

3.3.2 City standards shall require 10% of the off-street parking area to be landscaped and the planting of a minimum of one tree for every five parking stalls, whether the parking aisles are single or double loaded; however, this standard may be increased through project conditions of approval to address size, canopy, or other characteristics which make parking lots more inviting.

3.3.3 The City shall require one landscaped finger with two parking lot trees at each finger for every ten lineal spaces on the perimeter of a parking lot.

3.3.4 Parking lot design shall incorporate trees planted to provide substantial shade. Parking lot trees shall have a minimum box size of a 24" box and canopy to provide substantial coverage of paved areas. The periphery of parking lots shall be densely planted with trees and shrub hedges; more importantly, special consideration shall be given wherever.

- the periphery represents a change from one type of land use to another;
- the property in question faces or backs to a freeway;
- adjoining properties are of a different architectural style, character, or massing;
- landscape treatments are necessary to ensure the privacy of residents.

3.3.5 The City shall require a five foot wide minimum clear planting space for all planting areas. Narrower planters are difficult to properly maintain or irrigate and often die, are trampled, or covered by the front bumpers of cars.

3.3.6 The City shall encourage the inclusion of pedestrian amenities including walkways, bus benches, and other features; textured paving along pedestrian walkways and under building canopies.

Consistency Analysis

Lytle Creek Ranch will comply with City requirements that commercial developments minimize the visual impacts associated with parking lots by using a variety of techniques, including some or all of the techniques identified below:

- Depression of parking lot grade, wherever feasible, to reduce the visual impact of automobiles when seen from the street;

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- Development of screen walls and landscaped buffers at sufficient height to conceal car grillwork and nuisance headlights into the street;
- Parking lot design which breaks up parking areas with landscaped belts, thereby reducing the massive and unbroken appearance of paved surfaces; or
- Continuous connection of planters rather than isolated tree wells and planters separated by wide expanses of paving.

Lytle Creek Ranch will comply, at a minimum, with City standards that require 10% of the off-street parking area to be landscaped and the planting of a minimum of one tree for every five parking stalls, whether the parking aisles are single or double loaded. In addition, for commercial developments, one landscaped finger with two parking lot trees at each finger will be provided for every ten lineal spaces on the perimeter of a parking lot, except where an alternative landscape treatment is determined to be acceptable during Plan Review.

Where feasible, parking lot design shall incorporate trees planted to provide substantial shade. Parking lot trees shall have a minimum box size of a 24" box and canopy to provide substantial coverage of paved areas. Where visible from adjacent public streets, the periphery of parking lots shall be planted with trees and shrub hedges. Special consideration to landscaping shall be given wherever.

- the periphery represents a change from one type of land use to another;
- the property in question faces or backs to a freeway;
- adjoining properties are of a different architectural style, character, or massing;
- landscape treatments are necessary to ensure the privacy of residents.

In conformance with City standards, a five foot wide minimum clear planting space will be provided for planting areas in commercial developments, except where tree wells are provided. Areas less than five feet in width (excluding tree wells) will be paved.

The City shall encourage the inclusion of pedestrian amenities including walkways, bus benches, and other features; textured paving along pedestrian walkways and under building canopies.

LANDSCAPE TREATMENT

Goal 5.1 Promote the "greening" of Rialto.

Policies

- 5.1.3 The City shall insist that all new development incorporate street tree plantings dense enough to shade and beautify residential and commercial areas.
- 5.1.5 Landscape materials shall be installed prior to completion of the first building phase for the entire project, including vacant land for the following projects: new specific plan areas, future development carried out under existing specific plans, and new commercial

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and industrial projects, regardless of the size of individual parcels within the development plan.

Consistency Analysis

Lytle Creek Ranch requires all new development to be landscaped. Street trees will be planted along all public and private streets where adequate right-of-way exists.

Landscape materials in Lytle Creek Ranch will be installed concurrently with each phase of development as each development is constructed.

Goal 5.2 The City shall develop a uniform streetscape program which emphasizes major and minor portals into the City.

Policies

5.2.1 Major entries to the City of Rialto shall be designated at the following high traffic volume locations: Riverside Avenue at the I-15 and I-10 freeways, Cedar Avenue at the I-10 Freeway, Foothill Boulevard at Pepper and Maple Avenues, Riverside Avenue entering Rialto from the south, and at Valley Boulevard at the eastern and western boundaries of the planning Area, as shown on Figure VIII-2.

The City shall establish unified entry treatments at the major entries to the City, thereby setting the tone for visitors and residents alike. The design of each of these entries shall consist of one or more of the elements described below.

- Low rise monument signs surrounded by groundcover, shrubs, and trees, similar to that monumentation found at Rialto City Park or in the Central Business District Redevelopment Project Area, consisting of precast concrete signs with embossed letters and natural river rock pilasters with concrete caps;
- Enriched, textured and /or interlocking paving at intersections, similar to that found in the Central Business District Redevelopment Project Area;
- Prohibition of pole signs and billboards within 500 feet of the entry;
- Sensitive lighting treatments;
- An emphasis on landscape treatments familiar to the area: river rock, rough hewn wood, plantings and trees.
- Undergrounding of utility lines.

5.2.3 The City shall encourage the development of unified entry statements for new residential, commercial, and industrial projects incorporating textured paving, coordinated monument signs and landscape treatments.

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- 5.2.5 Along the major thoroughfares within the City, trees should be formally massed to promote a rhythmic, ceremonial appearance and conform with the City's Street Tree Plan. Street trees shall be placed along the public rights-of-way no farther than 30 feet apart, have a minimum size of 24" box, and be selected from Table 1.
- 5.2.6 The median along Riverside Avenue in the Central Business District Redevelopment Project Area offers an attractive amenity to the median wherever possible along Riverside Avenue, with special attention given to that segment of the roadway in the northern area of the City development and the Country Club residential area.
- 5.2.8 Along residential streets, trees may either be formally massed to produce a steady rhythm, or grouped informally to create an informal, naturally wooded street appearance. In any event, the total number of trees plotted should not be less than one for every 25 feet of lineal street frontage.
- 5.2.9 The City of Rialto recognizes the value of alleys and their importance to the circulation, aesthetic, and land use goals of the General Plan. New projects shall incorporate any improvements necessary to upgrade alleys behind the project area to current standards; and the City shall encourage projects which do not abandon the alleys, but rather seek to refine their appearance and function with landscape treatment, textured paving, rear l treatments, parking and loading.
- 5.2.11 The City shall continue to upgrade landscape treatments of all annexations to uniform City Standards.
- 5.2.13 The City shall work with developers/builders within Rialto and its Sphere of Influence to ensure new landscape treatments are installed per the City's Landscaping Maintenance specifications.

Consistency Analysis

Lytle Creek Ranch will construct an entry feature into the City of Rialto within the Sierra Avenue or Riverside Avenue rights-of-way near the I-15 Freeway. This City entry feature will be constructed adjacent to Planning Area 33 and will include a representation of the Rialto Bridge.

Lytle Creek Ranch will include entry statements for new residential projects that incorporate textured paving, coordinated monument signs, and landscape treatments.

Along the community's major thoroughfares, trees will be massed to create scenic streetscapes. Street trees shall be placed along the public rights-of-way no farther than 30 feet apart, have a minimum size of 24" box, and be selected from the plant palette contained in this Specific Plan.

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Lytle Creek Ranch proposes to pay its fair share toward roadway improvements to Riverside Avenue that would include a landscaped median.

Street trees will be planted along all residential streets in Lytle Creek Ranch. The spacing of trees planted will comply with either City standards or the standards set forth in this Specific Plan, as applicable.

The City of Rialto recognizes the value of alleys and their importance to the circulation, aesthetic, and land use goals of the General Plan. Several of the new residential developments within Lytle Creek Ranch are expected to be alley-loaded products. Where provided, alleyways should be designed with rear façade treatments and landscaping, as feasible.

Lytle Creek Ranch will ensure that all areas to be annexed into the City of Rialto are maintained to uniform City standards. In addition, new landscape treatments within Lytle Creek Ranch shall be designed and installed per the City's Landscaping Maintenance specifications.

Goal 5.3 Ensure that the design of all freeways, their interchanges, and grade, separations are an aesthetically pleasing asset to the city of Rialto.

Policies

- 5.3.2 Arterials which cross over or under freeways shall be provided with planted medians sufficiently wide to include minor entry signs and landscape treatment. All medians shall be fully landscaped and treated with brick, tile, turf block, stamped concrete, pavers, or other elements.
- 5.3.5 The City shall establish loan, grant, or other programs to provide landscape treatment of residential lots immediately backing to proposed freeway routes and railways.
- 5.3.7 Landscape treatments near freeway off- and on- ramps should be designed to announce the drivers' entry into Rialto. Landscape design should incorporate the dedicated City tree which shall be determined.

Consistency Analysis

In conformance with the City's General Plan, arterials that cross under freeways shall be provided with planted medians sufficiently wide to include minor entry signs and landscape treatment. All medians shall be fully landscaped and treated with brick, tile, turf block, stamped concrete, pavers, or other elements.

Lytle Creek Ranch will provide a landscape treatment between the I-15 Freeway and any residential use. This landscape treatment shall not be required to exceed eight feet in width.

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A landscaped entry feature with signage announcing arrival into the City of Rialto will be constructed adjacent to Planning Area 33 within either the Sierra Avenue or Riverside Avenue right-of-way.

Goal 6.1 Lighting features within the City shall be aesthetically pleasing, while being functionally useful.

Policies

6.1.1 Street lighting in neighborhoods should be consistent.

Consistency Analysis

Street lighting within each neighborhood (Neighborhoods I, II, III, and IV) shall be internally consistent, although lighting between neighborhoods may differ in order to provide visual interest and a “sense of place.”

Goal 6.3 New streets shall be developed to assist rather than alienate pedestrians.

Policies

6.3.1 In residential areas, straight streets shall be avoided, and curvilinear street shall be used, thereby contributing to the character of the streetscape while discouraging speeding, increasing the safety of these streets.

6.3.2 Landscape treatments shall incorporate street trees along all streets, of species which provide sufficient canopy to shade the street and promote a pedestrian scale.

6.3.3 The City shall pursue undergrounding of utilities in existing areas and require that utilities in existing areas and require that utilities be undergrounded on all major new development.

6.3.6 Walled projects (including gated residential communities) shall be designed to provide an interesting streetscape, through the following:

- Walls shall be varied in plane and texture, utilizing different, but complimentary, types of materials and colors, in addition to the use of vines (the latter will act as a deterrent from graffiti problems);
- Landscaped greenbelts, vine pockets, and other landscape techniques shall be employed;
- Curvilinear wall alignments and meandering sidewalks shall be encouraged along project peripheries;
- And variation in setbacks and front wall planes behind the established minimum setback line shall be encouraged. Variation in front of the minimum setback line shall be discouraged.

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- 6.3.7 Screen or perimeter walls shall incorporate shrub massings, vine pockets or informal tree massing to minimize the vertical scale of the wall.
- 6.3.9 Enriched, varied textured paving treatments shall be used at all project entries, wherever pedestrian crossings, plazas, or gatherings areas are proposed, and as an accent feature to break up the monotonous appearance of concrete walkways.
- 6.3.12 Bus shelters and other outdoor use areas shall be shaded from the sun. Each project shall incorporate at least one bus shelter, taxi stop, bicycle racks, or similar pedestrian use area.

Consistency Analysis

Although many of the streets within Lytle Creek Ranch will be curvilinear, straight streets are permitted within individual Planning Areas. Long expanses of straight streets shall be avoided. The landscape design for Lytle Creek Ranch provides for street trees along all streets of all classifications. Where space allows, street trees will include tree species that provide sufficient canopy to shade the street and promote a pedestrian scale.

As permitted by the utility companies, utilities within Lytle Creek Ranch shall be placed underground.

Lytle Creek Ranch is designed to minimize the use of walls and fences. Where used, walls shall be designed to provide an interesting streetscape, through varying wall planes, textures, colors, and materials; providing vine pockets and other landscaped techniques; and occasionally varying setbacks and front wall planes. Walls will be installed in residential areas that abut Riverside Avenue for noise and safety reasons.

Perimeter walls shall incorporate shrub massings, vine pockets, and/or informal tree massing to minimize the vertical scale of the wall.

If permitted by the City of Rialto, enriched, varied textured paving treatments may be used at the various project entries along Riverside Avenue, opposite N. Live Oak Avenue, Redwood Avenue, N. Alder Avenue and N. Locust Avenue. Enhanced paving may also be used at other key entry and focal points within the project.

Where provided, bus shelters and other outdoor use areas shall be shaded from the sun. The project shall provide at least one of the following: one bus shelter, bicycle racks, or similar pedestrian uses in Neighborhood III.

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Goal 6.5 Encourage the undergrounding of utility wires to protect scenery, enhance the appearance of major boulevards, and promote neighborhood character.

Policies

6.5.1 The City shall require the undergrounding of all utilities in Lytle Creek Ranch through its standard list of conditions. Where above-ground installations are not required, the City shall encourage electrical vaults to be placed underground. Where the installations must be aboveground, the City shall require these to be landscaped and concealed by a low decorative wall.

Consistency Analysis

Lytle Creek Ranch shall install all utilities underground, as permitted by the appropriate utility company or agency.

Goal 7.2 Ensure the protection of new development from watercourses, flood control channels and other waterways, while retaining an aesthetic appearance.

Policies

7.2.3 The City of Rialto shall require that whenever possible, watercourses shall be combined with pedestrian amenities, such as riding and hiking trails, scenic corridors, linear parks, greenbelts, pedestrian bridges, and other landscape features. The developers of such proposed projects should consider not only the landscape and cross sections of such facilities, but also demonstrate how these facilities can be interconnected with other elements of the City's trail and street systems. In addition, dense landscape treatments shall be used to promote the "greening of Rialto."

Consistency Analysis

The Lytle Creek Ranch project includes improvements to the flood control levee system along Lytle Creek Wash. These improvements will ensure the protection of new development from flooding associated with Lytle Creek during major storm events. In addition, a system of on-site drainageways will minimize the potential for flooding on the project site.

The project proposes a landscaped "Grand Paseo" in Neighborhood III that will function for both recreational and stormwater purposes. Likewise, the ~~golf course~~ open space in Neighborhood II will handle stormwater flows, while accommodating ~~golfing as a~~ recreational uses. The intent is to provide "Green" stormwater swales and drainageways throughout Lytle Creek Ranch in lieu of using pipes for stormwater flows.

1.6 CULTURAL AND HISTORIC RESOURCES ELEMENT

HISTORIC ARCHAEOLOGICAL RESOURCES

Goal 3.1 All significant historic archeological resources within Rialto shall be surveyed recorded and, where feasible, protected.

Policies

3.1.3 Documentation of all historical archeological surveys conducted within the City of Rialto shall be provided to the Rialto City Planning Department, with copies to the Rialto Historical Society.

Consistency Analysis

The Lytle Creek Ranch project will comply with the requirements of Policy 3.1.3 on page IX-4 of the City of Rialto General Plan. Copies of the document will be provided to the Rialto City Planning Department and the Rialto Historical Society.

1.7 CONSERVATION ELEMENT

Goal 1.1 Conserve, protect and enhance the natural resources in Rialto to ensure their optimal use and support to the benefit of all present and future citizens of Rialto.

Goal 2.1 Protect and enhance Rialto's surface waters and groundwater basins.

Goal 2.2 Conserve scarce water resources.

Consistency Analysis

Lytle Creek Ranch will incorporate sustainable building techniques and design strategies intended to help conserve, protect, and enhance natural resources in the community.

By creating a system of natural bioswales and incorporating a variety of Best Management Practices, Lytle Creek Ranch will help to protect and enhance Rialto's surface waters and groundwater basins.

The project will help to conserve scarce water resources by incorporating irrigation timers and automatic sprinklers, drip irrigation (where feasible), low flush toilets, low water use shower heads, and other water conservation measures as feasible.

AIR QUALITY

Goal 5.1 To achieve conformance with the AQMP by adopting a comprehensive plan for implementation, so that all general developments projects approved are consistent with the AQMP.

Policies

- 5.1.1 Require that all developments within the City with more than 100 employees develop a rideshare programs.
- 5.1.2 Require all developments to comply with the AQMP, particularly regarding Transportation Demand Management (TDM) programs. A TDM plan for new developments shall include but not be limited to design considerations to encourage ridesharing, transit use, park and ride facilities, as well as bicycle and pedestrian circulation.
- 5.1.3 Incorporate phasing policies and requirements in development plans to achieve concurrent provision of infrastructure, particularly transportation facilities, to serve development.
- 5.1.4 Locate and design new development in a manner that will minimize direct and indirect emission of air contaminants. To this end, participate with SANBAG in jointly formulating appropriate standards for regulating the location and protection of sensitive receptors (i.e., schools, day care facilities, and hospitals) from excessive and hazardous emissions.

Consistency Analysis

Any and all developments within Lytle Creek Ranch that employ more than 100 employees shall be required to develop a rideshare program.

All developments within Lytle Creek Ranch shall be required to comply with the AQMP, particularly regarding Transportation Demand Management (TDM) programs. A TDM plan for new developments shall include but not be limited to design considerations to encourage ridesharing, transit use, and bicycle and pedestrian circulation.

The Lytle Creek Ranch project is designed to provide infrastructure concurrently with development. Roadways will be constructed to ensure adequate safety and comply with City circulation requirements.

The two potential school sites identified within the Lytle Creek Ranch Specific Plan area have been located to minimize these sensitive receptors from excessive and hazardous emissions. The

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proposed school sites are surrounded by parks and residential uses. No heavy industry or manufacturing uses are planned near the school sites.

Goal 5.4 Promote the expansion of bus, rail and other forms of transit, within the region.

Policies

5.4.6 Develop standards and guidelines for support facilities to incorporate into development plans for increased bicycle and pedestrian routes to link appropriate activity centers to nearby residential development.

Consistency Analysis

Lytle Creek Ranch is designed to promote bicycle traffic and pedestrian movement throughout the community. The "Grand Paseo" in Neighborhood III will contain a minimum eight foot wide meandering multi-purpose trail that will accommodate both pedestrian and bicycle traffic. In addition, most of the larger streets within the community will contain on-street bike lines. There will also be a comprehensive system of sidewalks that will provide linkages between planning areas and the parks, schools, and open space on-site.

Goal 5.9 Reduce emissions through reduced energy consumption.

Policies

5.9.5 Require all new development to meet or exceed Title 24 building standards for energy efficiency.

Consistency Analysis

All construction will meet or exceed Year 2010 Title 24 building standards for energy efficiency, as applicable. It is the intent of the developer to promote energy efficiency within the structures located within Lytle Creek Ranch.

BIOLOGICAL RESOURCES

Goal 6.1 Conserve and enhance Rialto's biological resources, facilitating development in a manner which reflect the characteristics, sensitivities and constraints of these resources.

Policies

6.1.1 Designate those areas along Lytle Creek which may contain rare or endangered species as "Biological Resource Management Areas."

6.1.2 Require that all proposed development in these "Biological Resource Management Areas" be subject to a biological study, to be prepared by a qualified professional, to

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determine whether there will be any impact to rare, threatened or endangered species, and identify mitigation measures where appropriate.

- 6.1.5 Pursue voluntary open space, wildlife corridors, or conservation easements to protect sensitive species or their habitats.

Consistency Analysis

A comprehensive biological resources report was prepared by PCR, a qualified firm specializing in preparing biological and environmental studies, for the entire Lytle Creek Ranch project site as part of the required environmental process. In addition, focused studies were undertaken to identify potential impacts to rare, threatened, or endangered species. As a result of these studies, a minimum of ~~829-2769~~ acres on-site, including portions of Lytle Creek, will be preserved in permanent open space.

ENERGY

Goal 7.1 Conserve scarce energy resources.

Policies

- 7.1.1 Require the incorporation of energy conservation features in the design of all new construction and site development as required by state law.

Consistency Analysis

The project incorporates energy conservation features into the project design in excess of that required by California state law.

1.8 NOISE ELEMENT

Goal To protect public health and welfare by eliminating existing noise problems and by preventing significant degradation of the future acoustic environment.

Objective 1.0 Incorporate noise considerations into land use planning decisions.

Policies

Policy 1.b: The City shall require an environmental and noise impact evaluation for all projects as part of the design review process to determine if unacceptable noise levels will be created or experienced. Should noise abatement be necessary, the City shall require the implementation of mitigation measures based on a detailed technical study prepared by a qualified acoustical engineer (i.e., a Registered Professional Engineer in the State of California with a minimum of three years experience in acoustics).

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Policy 1.c The City shall not approve projects that do not comply with the adopted standards.

Objective 2.0 Establish measures to reduce noise impacts from traffic noise sources.

Policies

Policy 2.a The City shall require the construction of barriers to mitigation sound emissions where necessary or feasible.

Consistency Analysis

Concurrently with preparation of this Specific Plan, both an environmental and noise impact evaluation were prepared for the Lytle Creek Ranch project. The project will incorporate all applicable noise mitigation techniques as identified in the Project EIR.

Lytle Creek Ranch shall comply with the adopted standards for noise mitigation.

The EIR will identify measures, as needed to reduce noise impacts from traffic noise sources.

The City shall require the construction of barriers to mitigation sound emissions as identified in the EIR for Lytle Creek Ranch.

1.9 SAFETY ELEMENT

GEOLOGIC HAZARDS

Goal 2.1 Minimize hazards to public health, safety, and welfare resulting from geotechnical hazards.

Policies

2.1.1 The City shall require geotechnical investigations by a certified engineering geologist and registered civil engineer for all grading and construction proposed within any area which may be subject to severe seismic hazards.

2.1.3 The City shall require construction to be in conformance with the Uniform Building Code, specifically Chapter 23 as it provides for earthquake-resistant design, and Chapter 70 as it provides for excavation and grading.

Goal 2.2 Encourage urbanization only in those areas without significant risk to life and property.

Policies

2.2.1 Development within Alquist-Priolo Special Studies Zones will be subject to the restrictions and requirements of the Special Studies Zones Act.

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Consistency Analysis

Lytle Creek Ranch will comply with all applicable restrictions and requirements of the Special Studies Zones Act as it applies to Alquist-Priolo Special Studies Zones.

FLOODING

Goal 3.2 Minimize the adverse effects of urbanization upon drainage and flood control facilities.

Policies

- 3.2.1 The City shall require the implementation of adequate erosion control measures for development projects to minimize sedimentation damage to drainage facilities.
- 3.2.3 The City shall maintain its open space and shall require developers to provide adequate open space pursuant to the standards established in the Parks and Recreation Element of the General Plan and the City's zoning ordinance as a measure to minimize impermeable surfaces throughout the City.
- 3.2.4 The City shall require water retention devices in new development in order to minimize peak flows to the surface drainage system.

Consistency Analysis

The Lytle Creek Ranch project incorporates Best Management Practices and erosion control measures to minimize sedimentation damage to drainage facilities. Planning and design for water quality protection employs three basic strategies in the following order of relative effectiveness: 1) reduce or eliminate post-project runoff; 2) control sources of pollutants, and 3) treat contaminated stormwater runoff before discharging it to natural water bodies. These principles are consistent with the typical permit and local program requirements for projects that require a consideration of a combination of source control BMPs (that reduce or eliminate runoff and control pollutant sources) and treatment control BMPs with specific quantitative standards.

Lytle Creek Ranch provides adequate open space pursuant to the standards established in the Parks and Recreation Element of the General Plan and the City's zoning ordinance as a measure to minimize impermeable surfaces throughout the city.

As required by the General Plan, Lytle Creek Ranch provides for water retention devices concurrently with development of new construction in order to minimize peak flows to the surface drainage system. Most of these devices will be constructed within the "Grand Paseo" in Neighborhood III, or ~~the golf course~~ in Neighborhood II.

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FIRE

Goal 4.1 Fire prevention regulations and standards to minimize potential fire hazards and fire losses.

Policies

4.1.4 Require that all site plans, subdivision plans, and building plans be reviewed by the Fire Department to ensure compliance with appropriate fire regulations.

Consistency Analysis

All site plans, subdivision plans, and building plans for Lytle Creek Ranch will be reviewed by the City's Fire Department to ensure compliance with appropriate fire regulations.

LAW ENFORCEMENT

Goal 7.1 To provide a safe and secure environment for the City's residents, workers and visitors.

Policies

7.1.1. The City shall require new development and improvements to employ defensible space concepts into site design and building specifications (i.e., lighting of sidewalks and parking areas, resident surveillance sight lines, and the use of burglary-resistant hardware and fixtures in buildings.)

Consistency Analysis

Lytle Creek Ranch complies with Policy 7.1.1, which requires that new developments employ defensible space concepts into site design and building specifications.

1.10 HOUSING ELEMENT

Goal 2.0 Provide adequate residential sites through appropriate land use, zoning and Specific Plan designations to accommodate the City's share of Regional Housing needs.

Policies

2.1 Implement land use policies which provide for a diversity of housing types and range ranges that will enable the City to encourage consistency with the 1998-2005 RHNA.

2.4 Promote the phase and orderly development of new neighborhoods consistent with the provision of infrastructure improvements.

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Consistency Analysis

Development within Lytle Creek Ranch will be phased to ensure orderly growth. This Specific Plan requires that infrastructure improvements be provided concurrently with planned development.

Goal 3.0 Assist in the provision of adequate housing that is affordable to lower and moderate income households.

Policies

- 3.1 Support the development of rental units with three or more bedrooms to provide affordable housing that adequately accommodates larger families, thereby reducing overcrowding and overpayment.

- 3.5 Encourage the construction of apartment complexes with strong on-site management to ensure that housing is well-maintained.

Consistency Analysis

Lytle Creek Ranch incorporates several planning areas that are expected to develop with rental units. A percentage of these units may contain three or more bedrooms. The exact number of units with three or more bedrooms, if any, will be determined by the builder of each complex. Lytle Creek Ranch encourages the construction of apartment complexes with strong on-site management to ensure that housing is well-maintained.